
Traffic Impact & Access Study

487 Park Avenue (Route 12)

***Proposed Mixed-Use Development
Worcester, Massachusetts***

Prepared for
Gjinko Reality, LLC.

April 2024

Prepared by



GREEN INTERNATIONAL AFFILIATES, INC.
TRANSPORTATION | STRUCTURAL | WATER RESOURCES | CIVIL/SITE



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Green International Affiliates, Inc.
100 Ames Pond Drive, Suite 200
Tewksbury, MA 01876
978-923-0400

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1.0 INTRODUCTION AND EXECUTIVE SUMMARY

This report describes the potential traffic impacts associated with the proposed six-story mixed-use development located at 487 Park Avenue (Route 12) in Worcester, Massachusetts. Access to the proposed development will be provided from Park Avenue (Route 12) and Shirley Avenue. The mixed-use development proposes 40 residential units, a 1,000 square foot (SF) coffee shop, and 1,800 SF automotive repair shop. The total proposed area of the building is 10,050 SF. This report describes the potential traffic impacts on the adjacent roadways and nearby intersections as a result of the development project. Intersection capacity analyses were completed at the study intersections for the existing, future No-Build, and future Build conditions.

The evaluation and analyses completed as part of the traffic impact study include field review, traffic data collection including traffic volumes, crash data, trip generation calculation, traffic safety and operations of the roadway/site access interface. The guidelines from the Institute of Transportation Engineers (ITE) Trip Generation Manual were used for completing this traffic impact study. The report contains descriptions of existing characteristics of the abutting roadway network, current traffic conditions, estimated traffic impacts and access/egress characteristics of the proposed mixed-use development. The project location is shown in Figure 1.

1.1 Future Conditions

The future year analysis horizon year 2031 was chosen based on the current Massachusetts Department of Transportation (MassDOT) analysis guidelines. The evaluation of the future conditions involved comparing the future No-Build and Build conditions.

The proposed development project is estimated to generate 111 total trips (52 entering, 59 exiting) during the AM peak hour and 52 total trips (27 entering, 25 exiting) during the PM peak hour.

1.2 Conclusions

The analysis indicated the following:

- The required sight distance is available at the proposed Park Avenue (Route 12) site driveway. Traffic can enter and exit the proposed site driveway without sight distance creating a safety issue. At the proposed site driveway on Shirley Street, the minimum sight distance is provided on the northbound approach and a clear line of sight to and from Park Avenue (Route 12) is provided on the southbound approach.
- The proposed development is not expected to have a significant impact on the roadway network. Levels of service at the study intersections are expected to be similar under both No-Build and Build conditions.

1.3 Recommendations

- Evaluate existing vegetation that may obstruct lines of sight exiting the site driveways. Any proposed landscaping should be set back from the edge of the roadway so as not to impede sight distance;
- Install a stop sign and stop line at the proposed site driveway approaches to Route 12 (Park Avenue) and Shirley Street;
- Reconstruct sidewalks along the site frontage and incorporate connections to the internal pedestrian walkways;

- Improve the crosswalk across Shirley Street at Park Avenue (Route 12) by restriping and reconstructing ADA-compliant curb ramps;
- Reconstruct sidewalk in front of the bus stop adjacent to the site to provide an ADA-compliant landing pad;
- Coordinate with the City of Worcester to optimize signal timings at Park Avenue (Route 12) at Maywood Street intersection prior to the opening of the proposed development;
- Implement the recommendations listed in the TDM section.

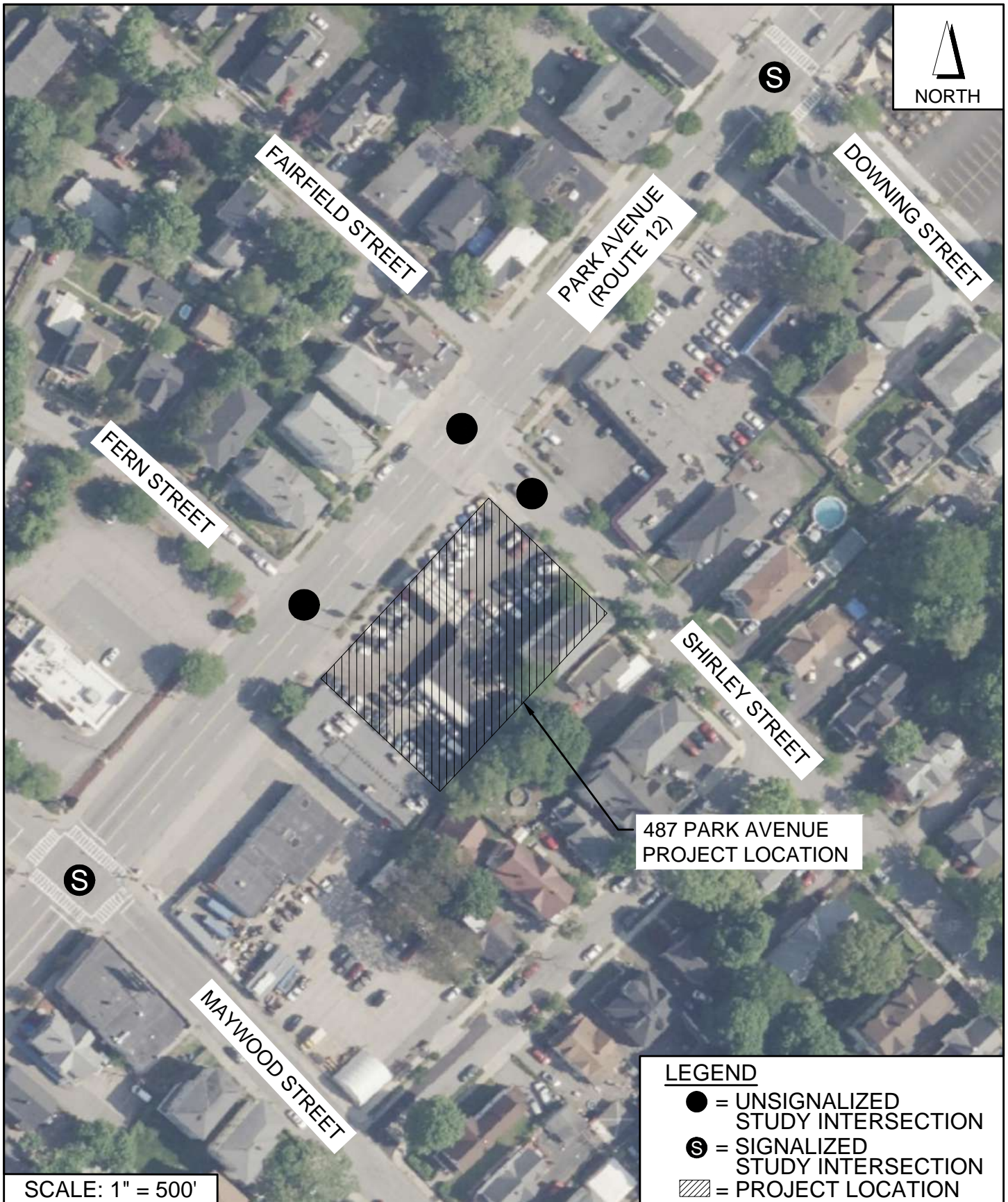


Figure 1
Project Location
487 Park Avenue (Route 12) TIAS
Worcester, MA

2.0 EXISTING TRAFFIC CONDITIONS

The following sections describe the existing transportation system in terms of physical and operational characteristics. The selection of the study area considered the location and type of development project. The servicing roadways including Park Avenue (Route 12) and the adjacent intersections to the east and west of the site, which are anticipated to be most impacted by the proposed mixed-use facility.

2.1 Study Roadway Network

The study focused on Park Avenue (Route 12) between Maywood Street to the west and Downing Street to the east, with an emphasis on the following five (5) study intersections:

- Park Avenue (Route 12) at Maywood Street
- Park Avenue (Route 12) at the Proposed Site Driveway
- Park Avenue (Route 12) at Shirley Street
- Shirley Street at the Proposed Site Driveway
- Park Avenue (Route 12) at Downing Street

As part of this study, a field reconnaissance was conducted to verify the physical and geometric layout of the study intersections and roadways and to observe traffic operations in the study area. A description of the study roadways serving the project site is as follows:

2.1.1 Park Avenue (Route 12)

Park Avenue (Route 12) is owned and maintained by the City of Worcester. Route 12 (Park Avenue) is classified as an urban principal arterial and follows an east-west alignment. Park Avenue (Route 12) is approximately 52-foot-wide consisting of two 11-foot-wide northbound travel lanes, and two 11-foot-wide southbound travel lanes. There is a double yellow center line, and the travel lanes are separated with dashed white pavement markings. There are no marked shoulders. There is on-street parking on the west side of the roadway, and no parking allowed on the east side of the roadway. There are sidewalks that vary between five to thirteen feet wide on both sides of the roadway that are in fair to poor condition. Where the sidewalk terminates at most of the side street intersections there are no curb ramps with detectable warning panels. There are segments of five-foot-wide grass buffers separating the sidewalks from the roadway. There are trees and other vegetation planted in the grass buffer areas. The pavement is in fair to poor conditions with potholes, vertical, and horizontal cracking. The pavement markings are in fair to poor condition. The posted speed limit in each direction is 30 miles per hour (MPH). The surrounding land use is residential and commercial.



2.1.2 Maywood Street

Maywood Street is owned and maintained by the City of Worcester. Maywood Street is classified as a local roadway and follows a north-south alignment. Maywood Street is approximately 32-feet-wide, consisting of 16-foot-wide travel lanes and no marked shoulders. There is on-street parking allowed on both sides of the roadway. There are eight-foot-wide sidewalks on both sides of the roadway that are in poor condition. The pavement is in fair condition with areas of vertical and horizontal cracking. The pavement markings are in fair condition. There is no posted speed limit along Maywood Street, except for a 20 MPH curve ahead warning south of the Park Avenue (Route 12) at Maywood Street intersection. On local roadways with no posted speed limit, the city-wide speed limit of 30 MPH applies. The surrounding land use is mainly residential with some commercial properties.



Maywood Street, facing south

2.1.3 Shirley Street

Shirley Street is owned and maintained by the City of Worcester. Shirley Street is classified as a local roadway and follows a north-south alignment. Shirley Street is approximately 30-feet-wide with no marked travel lanes or shoulders. The pavement is in good to fair condition with minimal vertical and horizontal cracking. There is on-street resident only parking allowed on both sides of the roadway. There are five-to-six-foot-wide sidewalks on both sides of the roadway. The sidewalks are in good condition, and there is a three-foot-wide grass buffer separating the sidewalks from the roadway. There is no posted speed limit along Shirley Street. On local roadways with no posted speed limit, the city-wide speed limit of 30 MPH applies. The surround land use is mainly residential with some commercial properties.



Shirley Street, facing south

2.1.4 Downing Street

Downing Street is owned and maintained by the City of Worcester. Downing street is classified as a local roadway, and follows a north-south alignment. Downing Street is approximately 30-feet-wide with no marked travel lanes or shoulders. The pavement is in good to fair condition with minimal vertical or horizontal cracking. There is on-street parking allowed on both sides of the roadway. There are five-to-six-foot-wide sidewalks on both sides of the roadway. The sidewalks are in good condition, and there is a three-foot-wide grass buffer separating the sidewalk from the roadway. There is no posted speed limit along Downing Street. On local



Downing Street, facing south

roadways with no posted speed limit, the city-wide speed limit of 30 MPH applies. The surrounding land use is mainly residential with some commercial properties.

2.1.5 Park Avenue (Route 12) at Maywood Street

The intersection of Park Avenue (Route 12) at Maywood Street forms a four-legged signalized intersection. Park Avenue (Route 12) forms the east and west legs, and Maywood Street forms the north and south legs of the intersection. There are no protected signal phases for any of the intersection turning movements. The Park Avenue (Route 12) approach signals run concurrently, and the Maywood Street approach signals run concurrently. The Park Avenue (Route 12) approaches consist of two travel lanes, a shared through/left-turn lane and a shared through/right-turn lane. The Maywood Street approaches have a single travel lane to accommodate left, through, and right turning movements. There are striped crosswalks across each intersection leg with push button activated pedestrian signals. There are no detectable warning panels on the curb ramps. There are sidewalks along each intersection leg that are in fair condition. The intersection pavement markings are in fair condition. The pavement is in fair condition with potholes, vertical and horizontal cracking. The surrounding land use is commercial.



Park Avenue (Route 12) at Maywood Street

2.1.6 Park Avenue (Route 12) at Proposed Site Driveway

The intersection of Park Avenue (Route 12) at the 487 Park Avenue driveway and Fern Street forms an unsignalized intersection. The 487 Park Avenue driveway provides access to “Olsi Auto Sales & Service”, which is an automotive car dealership and repair shop. The driveway curb cut to Park Avenue (Route 12) is approximately 35-foot-wide. Park Avenue (Route 12) forms the east and west legs, and the 487 Park Avenue driveway forms the south leg of the intersection. Fern Street forms the north leg of the intersection. The Park Avenue (Route 12) approaches are uncontrolled and run freely. The 487 Park Avenue Driveway is STOP controlled with no STOP sign or stopbar. Fern Street is a one-way street and does not permit exiting traffic to Park Avenue (Route 12). There are five-foot-wide sidewalks on both sides of Park Avenue (Route 12). The pavement is in poor condition with areas cracking and potholes. The pavement markings are in fair condition. The surrounding land use is commercial and residential.



487 Park Avenue Driveway at Park Avenue (Route 12)

2.1.7 Park Avenue (Route 12) at Shirley Street

The intersection of Park Avenue (Route 12) at Shirley Street forms a three-legged unsignalized intersection. Park Avenue (Route 12) forms the east and west legs, and Shirley Street forms the south leg of the intersection. Park Avenue (Route 12) approaches are uncontrolled and run freely. Shirley Street is STOP controlled with a STOP sign and no stopbar. There are detectable warning panels at the Shirley Street curb ramps, but no marked crosswalk. The pavement is in poor condition with vertical and horizontal cracking. The pavement markings are in poor condition. There are sidewalks along each intersection approach that are in fair condition. The surrounding land use is residential and commercial.



Park Avenue (Route 12) at Shirley Street

2.1.8 Shirley Street at Proposed Site Driveway

The intersection of Shirley Street at 487 Park Avenue and 467 Park Avenue driveways forms an unsignalized intersection. Shirley Street forms the north and south legs of the intersection. The 487 Park Avenue driveway forms the west leg, and the 467 Park Avenue driveway forms the east leg. The 487 Park Avenue driveway is approximately 50-feet south of the Shirley Street and Park Avenue (Route 12) intersection. The 487 Park Avenue driveway curb cut to Shirley Street is approximately 30-feet-wide. The 467 Park Avenue driveway curb cut to Shirley Street is approximately 22-feet-wide. The Shirley Street approaches are uncontrolled and run freely. The Park Avenue driveways are STOP controlled with no STOP sign or stopbars. There are sidewalks on both sides of the roadway that are in fair condition. There are no pavement markings at the intersection, and the pavement is in fair condition. The surrounding land use is residential and commercial.



487 Park Avenue driveway at Shirley Street

2.1.9 Park Avenue (Route 12) at Downing Street

The intersection of Park Avenue (Route 12) at Downing Street forms a three-legged signalized intersection. Park Avenue (Route 12) forms the east and west legs, and Downing Street forms the south leg. There are no protected signal phases for any of the intersection turning movements. The Park Avenue (Route 12) approach signal runs concurrently. The Downing Street approach has an exclusive phase. The parking lot for 466 Park Avenue across from Downing Street is not signal controlled. The Park Avenue (Route 12) approaches consist of two travel lanes, a shared through/left-turn lane and a shared through/right-turn lane. Downing Street approach has a single travel lane to accommodate left, through, and right turning movements. There are striped crosswalks across the east leg of Park Avenue (Route 12), and across the Downing Street approach. There are push button activated pedestrian signals for the crosswalk across Park Avenue (Route 12), and no protected pedestrian signals for the Downing Street crosswalk. There are detectable warning panels on each crosswalk curb ramp. There are sidewalks along each intersection leg that are in fair condition. The intersection pavement markings are in fair to poor condition. The pavement is in fair to poor condition with potholes, vertical, and horizontal cracking. The surrounding land use is commercial and residential.



Park Avenue (Route 12) at Downing Street

2.2 Traffic Volumes

A traffic counting program was conducted as part of this study to analyze existing and future traffic operational conditions. Traffic count data at the study intersections was obtained from Precision Data Industries (PDI). The count program included turning movement count (TMC) data collected from 7-9 AM and 4-6 PM on Wednesday, March 20th, 2024.

In addition, a 48-hour automatic traffic recorder (ATR) count was conducted on Park Avenue (Route 12) in front of the site, from Tuesday, March 19th, 2024 through Wednesday, March 20th, 2024. The ATR data is summarized in Table 1. The TMC data is illustrated in Figure 2 for the weekday AM and weekday PM peak hours.

The measured average daily traffic (ADT) on a weekday on Park Avenue (Route 12) in front of the site was 20,778 vehicles per day. During the weekday AM peak hour, approximately 67% of traffic travels eastbound, and 33% of traffic travels westbound. During the weekday PM peak hour, approximately 39% of traffic travels eastbound and 61% of traffic travels westbound.

Table 1 – Summary of Park Avenue (Route 12) Traffic Volumes 2024

Location	85 th Percentile Speed	Average Weekday Volume	Weekday AM Peak Hour				Weekday PM Peak Hour			
			Time	Volume	K-Factor	Dir. Dist.	Time	Volume	K-Factor	Dir. Dist.
Park Avenue (Route 12) west of Shirley Street	35	20,778	7:45 - 8:45	1,366	6.57%	67% EB / 33% WB	4:00 - 5:00	1,522	7.32%	42% EB / 58% WB
Abbreviations:										
vpd = vehicles per day					K-Factor = Percentage of daily traffic that occurs during the peak hour					
vph = vehicles per hour					Dir. Dist. = Directional Distribution					

The seasonal variation of traffic flow on roadways similar in function and/or in the general region served as the basis of seasonal adjustments. The peak hour volumes for the study intersections were collected in the month of March. The average traffic volumes for this month are generally higher than the typical month, thus no seasonal adjustments were made to the existing volumes. The traffic count data and the seasonal factor information are included in the Appendix. Figure 2 presents the intersection traffic volumes for the existing morning and afternoon peak hours.

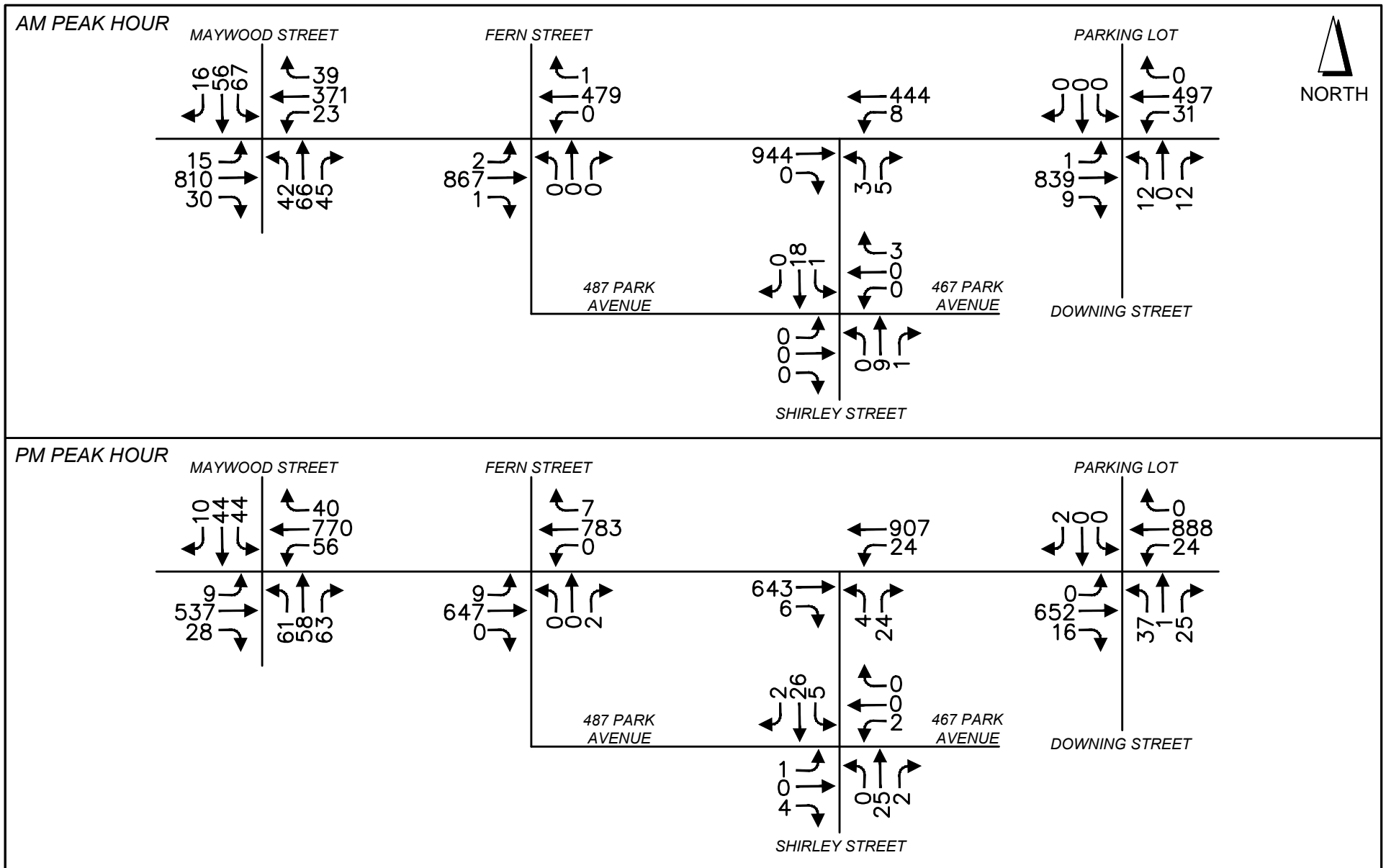


Figure 2
2024 Existing Traffic Volumes
487 Park Avenue (Route 12) TIAS
Worcester, MA

2.3 Crash Experience

Crash data were obtained from the MassDOT Crash IMPACT Portal for the years 2016-2020. During the five-year timeframe, there were 68 recorded crashes at the Park Avenue (Route 12) at Maywood Street intersection. The intersection averaged 13.8 crashes per year during the study timeframe. The crashes resulted in 63% property damage only, and 37% resulted in non-fatal injuries. The three most prevalent collision types were 35% angle, 26% rear-end, and 17% sideswipe in the same direction. Four of the crashes involved a collision with a pedestrian, and one of the crashes involved a collision with a cyclist. Of the five crashes involving pedestrians or bicyclists 60% resulted in a non-fatal injury. The pedestrians and bicyclists were recorded to be in a marked crosswalk at the time of the collision. Each pedestrian and bicyclist crash involved vehicles travelling northbound or southbound on Park Avenue (Route 12). The Park Avenue (Route 12) at Maywood Street intersection is designated as a Highway Safety Improvement Program (HSIP) cluster in 2018-2020.

In vicinity of the 487 Park Avenue (Route 12) driveway, there were eight crashes averaging 1.6 crashes per year over the five-year timeframe. The crashes resulted in 88% property damage only, and 12% non-fatal injuries. Of the eight crashes 38% were cited to be hit-and-run crashes, thus limited information is available for these crashes. The collision types were 50% sideswipe in the same direction, 38% rear-end, and 12% angle crashes. The 487 Park Avenue driveway at Park Avenue (Route 12) was not specifically cited as a location involved with a collision in the records.

During the five-year time frame, there were 10 recorded crashes at the Park Avenue (Route 12) and Shirley Street intersection, and the intersection averaged two crashes per year. The crashes resulted in 90% property damage only and 10% non-fatal injuries. The three most prevalent collision types were 40% sideswipe in the same direction, 30% rear-end, and 20% collisions with a cyclist. One collision with a cyclist involved a vehicle exiting Shirley Street westbound, and one collision with a cyclist involved a vehicle traveling northbound on Park Avenue (Route 12). The 487 Park Avenue driveway at Shirley Street was not specifically cited as a location involved with a collision in the records.

During the five-year time frame, there were 13 recorded crashes at the Park Avenue (Route 12) and Downing Street intersection, and the intersection averaged 2.6 crashes per year. The crashes resulted in 54% property damage only, and 46% non-fatal injuries. The collision types were 38% angle, 38% rear-end, 8% sideswipe in the same direction, 8% single vehicle, and 8% collision with a cyclist. The cyclist was recorded to be in a marked crosswalk at the time of the collision, and resulted in property damage only.

The MassDOT Intersection Crash Rate Worksheet was used to calculate the crash rate for the study intersections, which was tabulated in Table 4.1. The crash rate at the signalized Park Avenue (Route 12) at Maywood Street is approximately 1.59 crashes per million entering vehicles. The Park Avenue (Route 12) at Maywood Street crash rate is higher than the District 3 average signalized intersection crash rate. The remaining study intersections were calculated to have crash rates lower than the District 3 average crash rates. The crash rate calculations are included in the appendix.

Table 2 – Summary of Reported Crash Data

	Park Avenue (Route 12) at Maywood Street					487 Park Avenue (Route 12) Driveway					Park Avenue (Route 12) at Shirley Street					Park Avenue (Route 12) at Downing Street					
	2016	2017	2018	2019	2020	2016	2017	2018	2019	2020	2016	2017	2018	2019	2020	2016	2017	2018	2019	2020	
Severity																					
Property Damage	14	15	4	7	8	2		2	3		2	2	3		2	2	4				1
Injury	4	6	3	4	3				1						1	3	2				1
Collision Type																					
Angle	5	7	2	7	3	1							1		1	3					1
Rear End	7	5	1	2	3	1			2		2	1			2	2					1
Head-on		1			2																
Side Swipe, same direction	4	3	3		1			2	2		1		1		2		1				
Side Swipe, opposite direction		1																			
Single Vehicle	1	2													1						
Front to Rear			1	2																	
Cyclist					1						1				1						
Pedestrian	1	2			1											1					
Time of Day																					
6:01 AM – 10:00 AM	1	4	1	2	1				2						1	1	2				
10:01 AM – 4:00 PM	5	9	2	4	6	2		1	1		2	2	2		1	1					
4:01 PM – 7:00 PM	4	7	3	3	2										1	1	1				
7:01 PM – 6:00 AM	8	1	1	2	2			1	1				1			2	3				2
Roadway Conditions																					
Dry	16	16	5	9	8	2					2	1	2		3	5	4				1
Wet	2	4	2	2	2							1	1				2				1
Snow/Ice		1			1																
Season																					
Dec-Feb	6	6	3	2	5	1		1					1				3				
Mar-May	5	7		2	1				2			1	1		1	1	2				
June-Aug	5	1	1	3	4	1		1	2			1				2	1				1
Sept-Nov	2	7	3	4	1						2		1		2	2					1
Light Conditions																					
Daylight	7	15	5	8	8	2		1	2		2	2	2		3	2	2				
Dawn/Dusk	2		1	1													1				
Dark (Lighted)	9	6	1	2	3			1	2				1			3	3				2
Dark (Not Lighted)																					
Totals	18	21	7	11	11	2	0	2	4	0	2	2	3	0	3	5	6	0	0	2	
Annual Average Crashes	13.6					1.6					2					2.6					
Intersection Crash Rate	1.59					0.22					0.25					0.31					
MassDOT District 3 Average Crash Rate	0.89					0.61					0.61					0.89					
MassDOT Statewide Average Crash Rate	0.78					0.57					0.57					0.78					

2.4 Public Transportation Network

The presence of nearby public transit systems was identified to better understand the potential interaction among multiple modes of travel and the impact that commuters may have on the roadway network while driving to or from transit stations. Worcester, Massachusetts is serviced by the Worcester Regional Transit Authority (WRTA). WRTA services 37 communities in the Worcester area. WRTA offers two fixed bus routes that service the study area, Route 825 Park Avenue Connector and Route 7 Washington Heights Apartments. There are three bus stops in the study area that are serviced by the WRTA Route 825 Park Avenue Connector and Route 7 Washington Heights Apartments. There is a bus stop located on Park Avenue (Route 12) directly in front of the proposed development. There are bus stops at the Park Avenue (Route 12) at Maywood Street, and Park Avenue (Route 12) at Downing Street intersections. There are no ADA-compliant paths to the bus stops located directly in front of the proposed site. WRTA offers three demand response services for senior citizens and residents with disabilities. The demand responses service operates at the same times and days as the fixed bus route services. Since these services pick up patrons from a predetermined address, WRTA buses could potentially use the roadways in the study area, however there are no permanent bus stops.

3.0 FUTURE CONDITIONS

The impact of the proposed mixed-use development project on the roadway network within the study area was evaluated and the results are described in this section. This study used the year 2031 for the future analysis year, which represents a seven-year permitting and build-out timeframe from the present condition and is consistent with current MassDOT guidelines for traffic studies.

3.1 No-Build Traffic Volumes

The year 2031 No-Build traffic volume network was developed by identifying area-wide background traffic volume growth and known specific nearby development projects that could contribute to traffic flow on the 2031 study network.

3.1.1 Background Traffic Growth

The 2024 existing traffic volumes were projected seven years into the future, for a design year of 2031 by applying a background growth rate compounded annually. The background growth rate was determined by analyzing yearly trends of Average Annual Daily Traffic (AADT) data based on historical volumes found on MassDOT Transportation Data Management System. Research was conducted by identifying two permanent count stations near the study area with sufficient yearly AADT data, as shown below:

- No. 3333 (at I-190 north of Lincoln Street in Worcester, MA)
- No. 3894 (at I-290 eastbound in Worcester, MA)

The yearly growth trends of the AADT data from these count stations were compiled and averaged to result in a background growth rate of 1.5%, which was used to project the 2024 existing volumes seven years into the future design year of 2031.

Coordination with the City of Worcester revealed there are no planned future projects nearby that would impact the traffic volumes in the study area. As a result, any increase in traffic volumes within the project area is expected to be covered by the annual background growth rate. Detailed calculations of the background growth rate from the permanent count stations are included in the appendix.

3.1.2 No-Build Traffic Volumes

Based on the above noted research, the year 2031 No-Build peak hour traffic volume projections were developed by adding seven (7) years' background traffic growth of 1.5 percent annually to the existing traffic volumes in the study area. The projected year 2031 No-Build traffic volumes projected for the weekday morning and weekday afternoon and are shown in Figure 3.

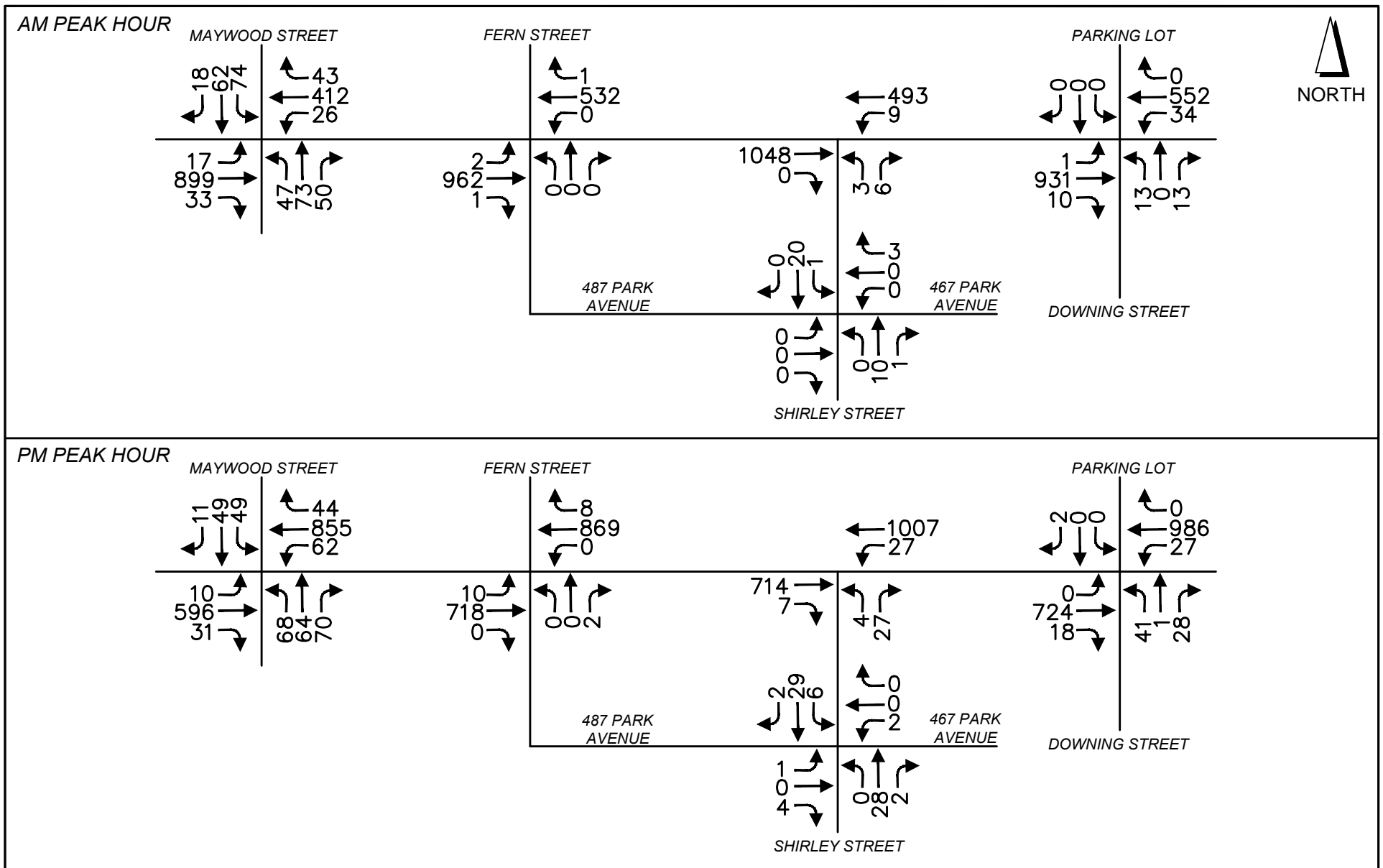


Figure 3
2031 No-Build Traffic Volume
487 Park Avenue (Route 12) TIAS
Worcester, MA

3.2 Proposed Project Description

The proposed mixed-use development includes 40 residential units, a 1,000 square foot (SF) coffee shop, and 1,800 SF automotive repair shop. The total proposed area of the building is 10,050 SF. Access to this proposed site will be provided from Park Avenue (Route 12) and Shirley Street. The existing site driveway on Shirley Street is proposed to be relocated to the southern limit of the site. The surrounding land use of the proposed development is a mix of commercial and residential.

3.3 Site Generated Traffic Volumes

In order to estimate the number of trips that could be potentially generated by the proposed development, statistics published by the Institute of Transportation Engineers (ITE) in Trip Generation Manual for similar land uses were examined. The ITE trip generation statistics represent compilations of data from studies/projects throughout the United States collected over the past 40+ years on trip generation characteristics for different types of land uses. The data has been compiled to provide transportation analysts with guidelines in forecasting daily and peak hour volumes for the specified use. The ITE report is based on observations of actual developments located in urban and suburban settings. Based on a review of the ITE database, Land Use Code (LUC) 221– Residential (Multi-Family), LUC 936– Services (Coffee Shop), LUC 943– Service (Auto Service Center) have been selected as most appropriate for this project.

Table 3 – Summary of Estimated Site Trip Generation

Land Use	Weekday						Daily
	AM Peak Hour			PM Peak Hour			
	Enter	Exit	Total	Enter	Exit	Total	
Residential: Multi-Family (LUC 221) 40 Dwelling Units	3	12	15	10	6	16	182
Services: Coffee Shop (LUC 936) 1,000 SF	47	46	93	16	16	32	-
Services: Auto Service Center (LUC 943) 1,800 SF	2	1	3	1	3	4	30
TOTAL	52	59	111	27	25	52	-

Sources: ITE Trip Generation, 10th Edition, 2017; LUC 814

As shown in Table 3, the proposed mixed-use development project is expected to generate during weekday morning peak hour approximately 111 new trips with 52 trips entering and 59 trips exiting the project site. During weekday afternoon peak hour, the project is estimated to generate approximately 52 new trips with 27 trips entering and 25 trips exiting.

The actual vehicle-trips generated by the proposed development are expected to be slightly lower than the totals shown in Table 3 because some trips may be shared between the different land uses. For example, some trips to the coffee shop will likely be by residents of the building or by patrons and employees of the auto service center. Additionally, some trips to and from the site are expected to be bicyclists, pedestrians, or transit users. No reductions in trips were applied for internal trips or trips by other modes of transportation to provide a conservative analysis of the proposed development’s potential impact.

3.3.1 Site Trip Distribution/Assignment

The trips estimated to be generated by the proposed development were assigned to the site driveways and study area roadways. The residential trip distribution was based on journey-to-work census data. The

commercial (coffee shop and automotive service center) trip distribution was based on existing travel patterns.

Figure 4 depicts the residential trip distribution, and Figure 5 depicts the commercial trip distributions during the AM and PM peak hours. Figure 6 shows the additional traffic generated by the residential component of the project during the weekday AM and PM peak hours. Figure 7 shows the additional traffic generated by the commercial component of the project during the weekday AM and PM peak hours.

3.3.2 Build Traffic Volumes

The vehicle-trips estimated to be generated by the proposed development were assigned to the study intersections and study area roadways using the trip distribution percentages to establish the future Build volumes. Figure 8 presents the Build volumes, including both the residential and commercial trip generation, for the AM and PM peak hours.

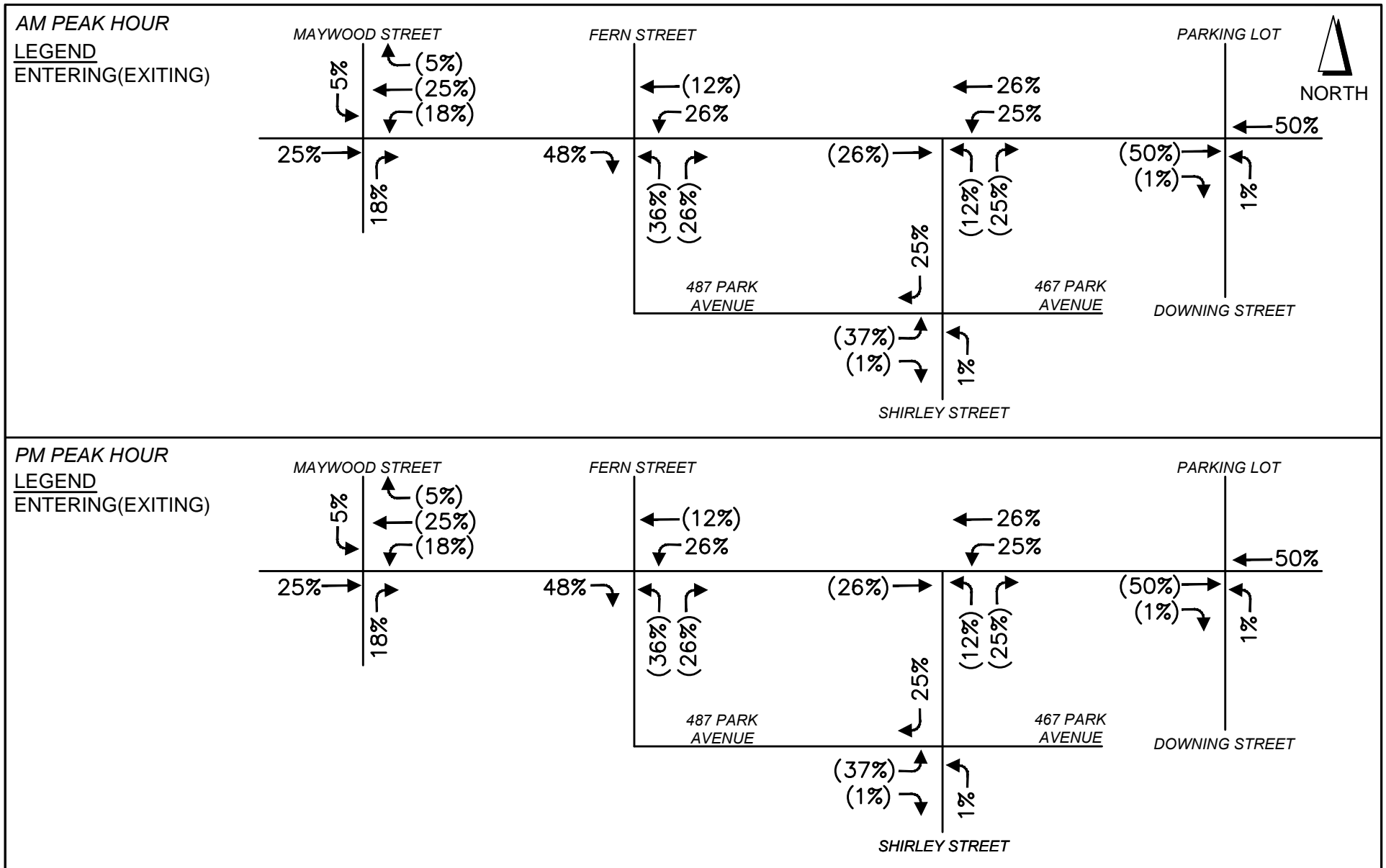


Figure 4
Estimated Residential Trip Distribution
487 Park Avenue (Route 12) TIAS
Worcester, MA

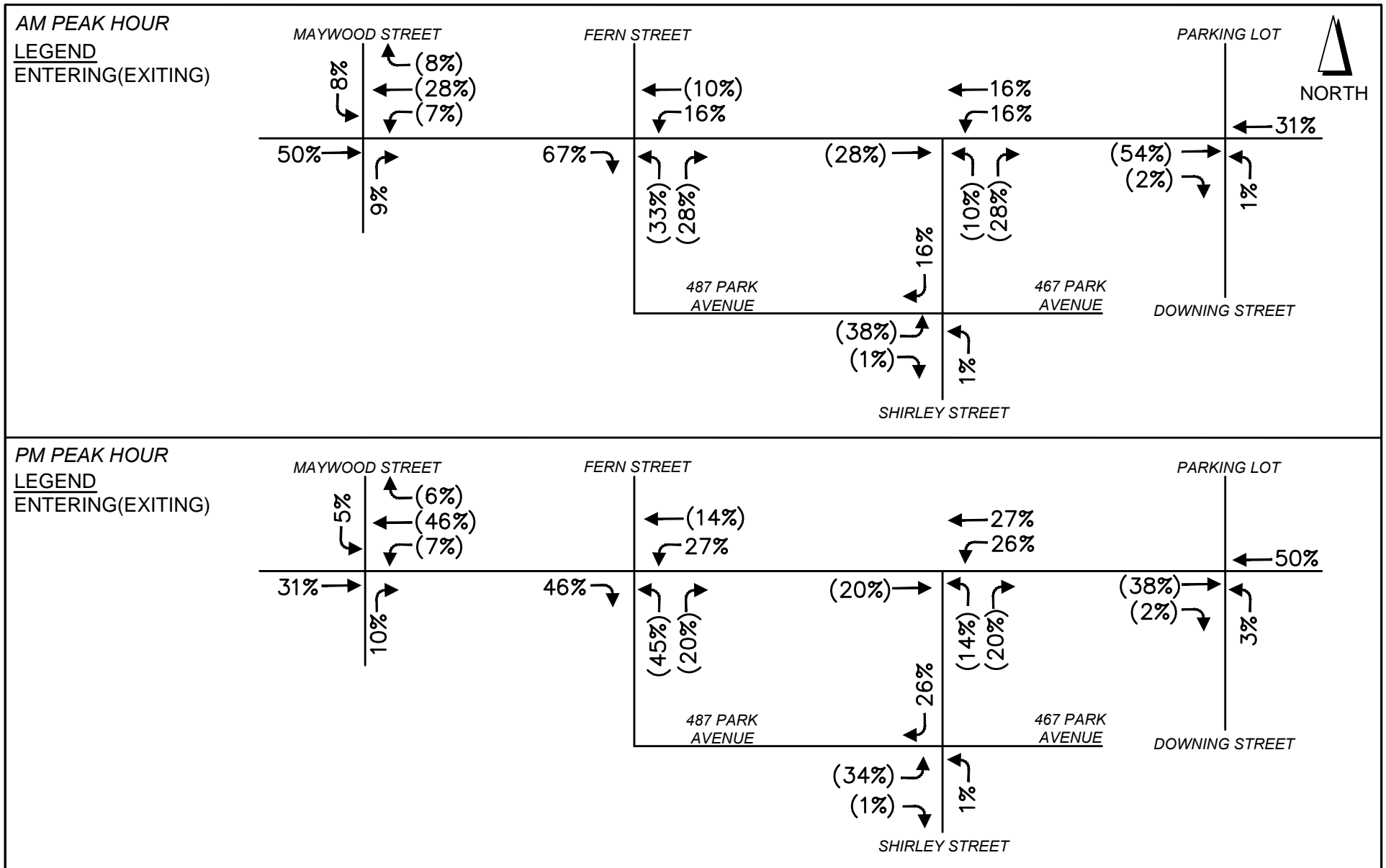


Figure 5
Estimated Commercial Trip Distribution
487 Park Avenue (Route 12) TIAS
Worcester, MA

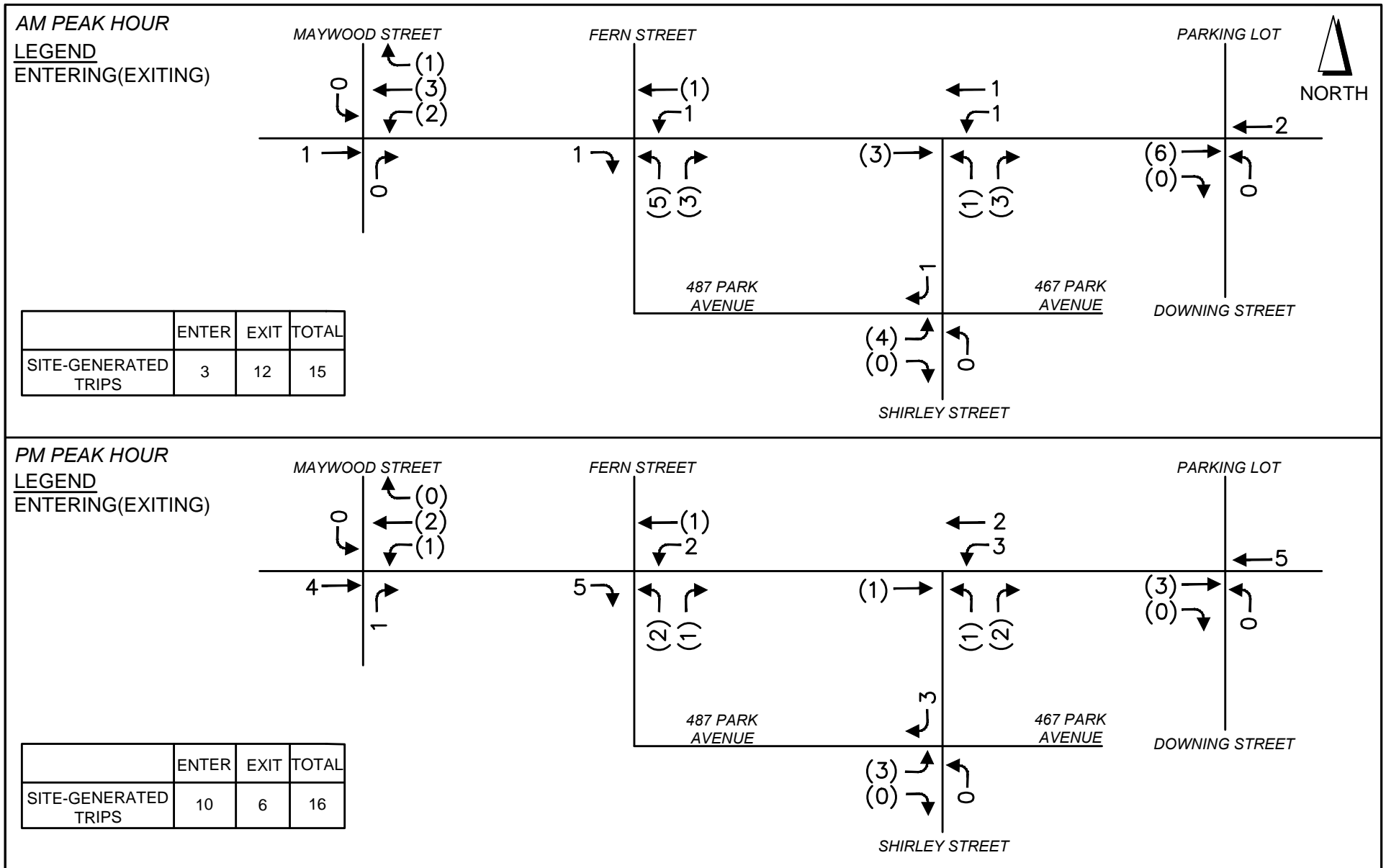


Figure 6
Estimated Residential Trip Generation
487 Park Avenue (Route 12) TIAS
Worcester, MA

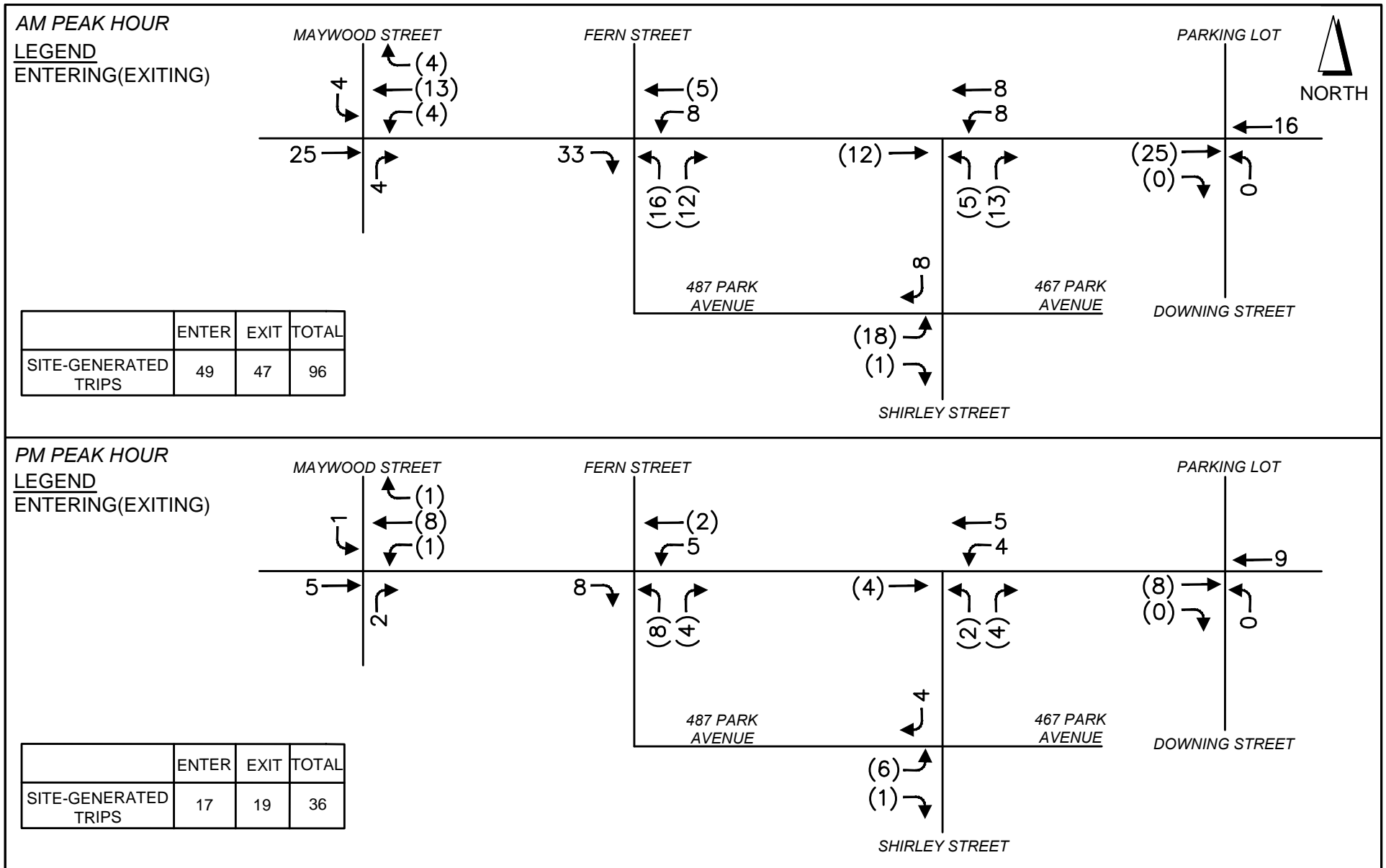


Figure 7
Estimated Commercial Trip Generation
487 Park Avenue (Route 12) TIAS
Worcester, MA

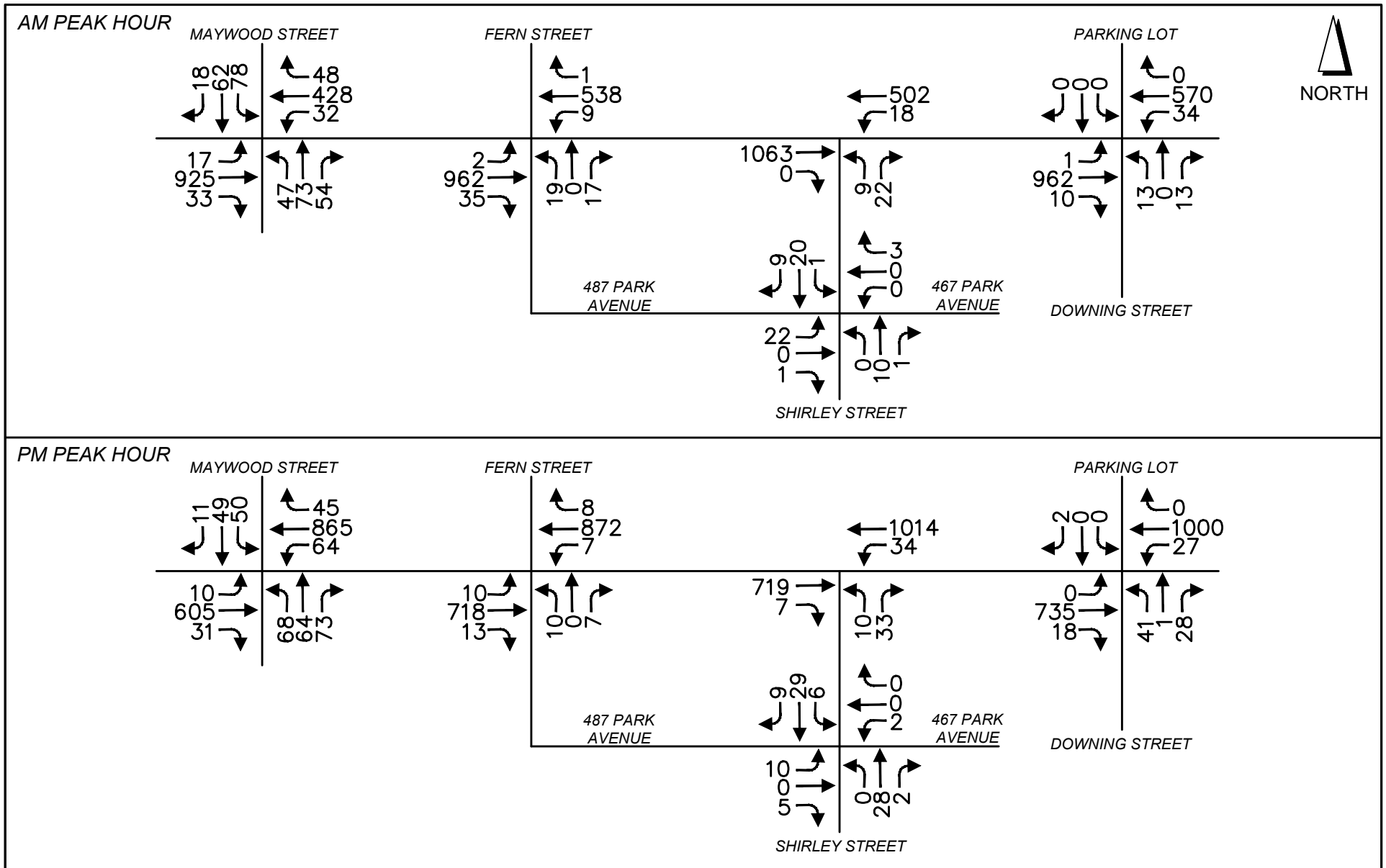


Figure 8
2031 Build Traffic Volumes
487 Park Avenue (Route 12) TIAS
Worcester, MA

4.0 OPERATIONAL ANALYSIS

Included in this section is an examination of the intersection capacity analyses for the study intersections and an analysis of available sight distances at the proposed site driveways.

4.1 Intersection Capacity Analysis

The study intersections were examined with regard to flow rates, capacity and delay characteristics to determine the Level of Service (LOS), using the methodology defined in the latest edition of the Highway Capacity Manual (HCM)¹ for the existing and future (No-Build and Build) traffic conditions. Level of Service is an indicator of operating conditions of a traffic facility such as a roadway or intersection while accommodating varying levels of traffic volumes. It is a qualitative measure that accounts for a number of operational factors including roadway geometry, speed, traffic composition, peak hour factors, travel delay, freedom to maneuver and driver expectation. When all of these measures are assessed, and a Level of Service is assigned to a roadway or intersection, it is equivalent to presenting an “index” to the operational qualities of the section under study. Level of Service is classified into six levels that are designated ‘A’ through ‘F’ based on the control delay ranges they fall under. These are presented in Table 4 for unsignalized and signalized intersections.

It should be noted that for unsignalized intersections, the LOS is not computed for the intersection as a whole. Instead, the level of service is determined by the computed or measured control delay for each individual critical movement (typically the side street movements).

Table 4 – Level of Service Criteria for Signalized and Unsignalized Intersections

LOS	Unsignalized Intersection (S)	Signalized Intersection (S)
A	≤10	≤10
B	>10 and ≤15	>10 and ≤20
C	>15 and ≤25	>20 and ≤35
D	>25 and ≤35	>35 and ≤55
E	>35 and ≤50	>55 and ≤80
F	>50 or v/c ≥1.00	>80 or v/c ≥1.00
<i>Abbreviations:</i> S = Seconds, v/c = Volume-to-Capacity Ratio, LOS = Level of Service		

The study intersections were evaluated using the Synchro 11 computer software HCM output. Using existing roadway features and intersection controls, traffic operations at the study intersections were evaluated for existing as well as predicted 2031 conditions. Analysis results are presented in Tables 5, and 6 for the weekday AM and weekday PM at the study intersections and proposed site driveway, respectively.

¹ Transportation Research Board, of the National Academies, Highway Capacity Manual 6th Edition, Washington, D.C., 2017.

Table 5 – Summary of Level of Service Analysis Period: Weekday AM Peak Hour

	2024 Existing Conditions				2031 No-Build Conditions				2031 Build Conditions			
	Delay (S)	LOS	V/C	95th Q (FT)	Delay (S)	LOS	V/C	95th Q (FT)	Delay (S)	LOS	V/C	95th Q (FT)
Park Avenue (Route 12) at Maywood Street												
Maywood Street NB LTR	41.7	D	0.65	113	40.2	D	0.62	152	40.7	D	0.63	156
Maywood Street SB LTR	55.2	E	0.78	152	52.1	D	0.75	170	57.4	E	0.80	180
Park Avenue (Route 12) EB LTTR	24.1	C	0.75	264	26.8	C	0.82	303	28.0	C	0.84	328
Park Avenue (Route 12) WB LTTR	18.9	B	0.49	122	18.8	B	0.48	135	19.9	B	0.54	147
<i>Overall Intersection</i>	<i>27.4</i>	<i>C</i>	<i>-</i>	<i>-</i>	<i>28.1</i>	<i>C</i>	<i>-</i>	<i>-</i>	<i>29.5</i>	<i>C</i>	<i>-</i>	<i>-</i>
487 Park Avenue at Park Avenue (Route 12) and Fern Street												
487 Park Avenue (Rte. 12) NB LTR	0	A	0	0	0	A	0	0	27.5	D	0.20	17.5
Park Avenue (Rte. 12) EB LTTR	0	A	0	0	0	A	0	0	0	A	0	0
Park Avenue (Rte. 12) WB LTTR	0	A	0	0	0	A	0	0	0.3	A	0.02	0
<i>Overall Intersection</i>	<i>0</i>	<i>A</i>	<i>-</i>	<i>-</i>	<i>0</i>	<i>A</i>	<i>-</i>	<i>-</i>	<i>0.7</i>	<i>A</i>	<i>-</i>	<i>-</i>
Park Avenue (Route 12) at Shirley Steet												
Shirley Street NB LTR	31.8	D	0.11	7.5	28.2	D	0.06	5	31.9	D	0.20	17.5
Park Avenue (Rte. 12) EB LTTR	0	A	0	0	0	A	0	0	0	A	0.01	0
Park Avenue (Rte. 12) WB LTTR	0.3	A	0.02	2.5	0.3	A	0.02	0	0.6	A	0.03	2.5
<i>Overall Intersection</i>	<i>0.4</i>	<i>A</i>	<i>-</i>	<i>-</i>	<i>0.3</i>	<i>A</i>	<i>-</i>	<i>-</i>	<i>0.8</i>	<i>A</i>	<i>-</i>	<i>-</i>
Shirley Steet at 487 Park Avenue and 467 Park Avenue												
Shirley Street NB LTR	0	A	0	0	0	A	0	0	0	A	0	0
Shirley Street SB LTR	0.8	A	0	0	0.3	A	0	0	2.2	A	0.01	0
487 Park Avenue EB LTR	0	A	0	0	0	A	0	0	8.9	A	0.03	2.5
467 Park Avenue WB LTR	8.4	A	0	0	8.4	A	0	0	8.4	A	0	0
<i>Overall Intersection</i>	<i>1</i>	<i>A</i>	<i>-</i>	<i>-</i>	<i>0.9</i>	<i>A</i>	<i>-</i>	<i>-</i>	<i>4.4</i>	<i>A</i>	<i>-</i>	<i>-</i>
Park Avenue (Route 12) at Downing Street and Parking Lot												
Downing Street NB LTR	32.5	C	0.24	17	33.5	C	0.22	28	33.5	C	0.22	28
Park Avenue (Rte. 12) EB LTTR	13.4	B	0.58	215	12.9	B	0.59	235	13.2	B	0.61	246
Park Avenue (Rte. 12) WB LTTR	12.2	B	0.46	133	11.0	B	0.42	136	11.1	B	0.44	142
<i>Overall Intersection</i>	<i>13.5</i>	<i>B</i>	<i>-</i>	<i>-</i>	<i>12.5</i>	<i>B</i>	<i>-</i>	<i>-</i>	<i>12.8</i>	<i>B</i>	<i>-</i>	<i>-</i>
EB = Eastbound	L = Left	S = Seconds	Delay = Average delay per vehicle (measured in seconds)									
WB = Westbound	T = Through	FT = Feet	50th Q = 50th percentile queue length (measured in feet), assumes 25 feet per vehicle									
NB = Northbound	R = Right	LOS = Level of Service	95th Q = 95th percentile queue length (measured in feet), assumes 25 feet per vehicle									
SB = Southbound	v/c = Volume-to-Capacity Ratio											

Table 6 – Summary of Level of Service Analysis Period: Weekday PM Peak Hour

	2024 Existing Conditions				2031 No-Build Conditions				2031 Build Conditions				2031 Build Conditions - Optimized			
	Delay (S)	LOS	V/C	95th Q (FT)	Delay (S)	LOS	V/C	95th Q (FT)	Delay (S)	LOS	V/C	95th Q (FT)	Delay (S)	LOS	V/C	95th Q (FT)
Park Avenue (Route 12) at Maywood Street																
Maywood Street NB LTR	48.9	D	0.75	196	46.8	D	0.73	191	47.4	D	0.74	195	47.5	D	0.73	200
Maywood Street SB LTR	40.7	D	0.57	116	39.8	D	0.55	106	40.3	D	0.56	113	40.8	D	0.55	107
Park Avenue (Route 12) EB LTR	19.7	B	0.56	165	19.6	B	0.55	180	19.8	B	0.56	183	19.4	B	0.54	186
Park Avenue (Route 12) WB LTR	41.6	D	0.95	358	63.0	E	1.04	406	73.2	E	1.07	418	56.7	E	1.02	422
Overall Intersection	34.8	C	-	-	45.5	D	-	-	50.7	D	-	-	42.3	D	-	-
487 Park Avenue at Park Avenue (Route 12) and Fern Street																
487 Park Avenue (Route 12) NB LTR	10.6	B	0.01	0	10.9	B	0	0	23.7	C	0.09	7.5	Not Applicable			
Park Avenue (Route 12) EB LTR	0.4	A	0	0	0.2	A	0	0	0.2	A	0.02	0				
Park Avenue (Route 12) WB LTR	0	A	0	0	0	A	0	0	0.2	A	0.01	0				
Overall Intersection	0.2	A	-	-	0.1	A	-	-	0.4	A	-	-				
Park Avenue (Route 12) at Shirley Street																
Shirley Street NB LTR	24.0	C	0.18	15	17.7	C	0.11	10	25.8	D	0.21	20	Not Applicable			
Park Avenue (Route 12) EB LTR	0	A	0	0	0	A	0	0	0	A	0	0				
Park Avenue (Route 12) WB LTR	0.7	A	0.04	2.5	0.6	A	0.04	2.5	0.8	A	0.04	2.5				
Overall Intersection	0.9	A	-	-	0.7	A	-	-	1.1	A	-	-				
Shirley Street at 487 Park Avenue and 467 Park Avenue																
Shirley Street NB LTR	0	A	0	0	0	A	0	0	0	A	0	0	Not Applicable			
Shirley Street SB LTR	0.9	A	0.01	0	1.2	A	0	0	1.0	A	0	0				
487 Park Avenue EB LTR	9.3	A	0.02	0	8.6	A	0.01	0	8.9	A	0.02	2.5				
467 Park Avenue WB LTR	8.8	A	0.01	0	9.0	A	0	0	9.1	A	0	0				
Overall Intersection	2.0	A	-	-	1.4	A	-	-	2.2	A	-	-				
Park Avenue (Route 12) at Downing Street and Parking Lot																
Downing Street NB LTR	31.8	C	0.31	18	31.8	C	0.31	56	31.8	C	0.31	56	Not Applicable			
Park Avenue (Route 12) EB LTR	12.6	B	0.44	163	13.1	B	0.48	182	13.1	B	0.49	185				
Park Avenue (Route 12) WB LTR	15.4	B	0.64	167	16.7	B	0.70	286	17.0	B	0.71	293				
Overall Intersection	15.0	B	-	-	15.8	B	-	-	16.0	B	-	-				
EB = Eastbound L = Left S = Seconds Delay = Average delay per vehicle (measured in seconds) WB = Westbound T = Through FT = Feet 50th Q = 50th percentile queue length (measured in feet), assumes 25 feet per vehicle NB = Northbound R = Right LOS = Level of Service 95th Q = 95th percentile queue length (measured in feet), assumes 25 feet per vehicle SB = Southbound v/c = Volume-to-Capacity Ratio																

The capacity analysis indicated the following:

AM Peak Hour

- Under the Build conditions, the Park Avenue (Route 12) site driveway delay is approximately 27.5 seconds or an acceptable LOS 'D.'
- The Shirley Street northbound approach delay is expected to increase by approximately 1.4 seconds compared to the No-Build AM conditions.
- At the site driveway and Shirley Street intersections, the Park Avenue (Route 12) approaches are expected to operate with minimal increase in delay.
- Operations at the signalized intersections of Park Avenue (Route 12) with Maywood Street and Downing Street are expected to be similar in both the No-Build and Build conditions.

PM Peak Hour

- Under the Build conditions, the Park Avenue (Route 12) site driveway delay is expected to be approximately 23.7 seconds or an acceptable LOS 'C.'
- The Shirley Street northbound approach delay is expected to increase by approximately 8.1 seconds compared to the No-Build PM conditions.
- At the site driveway and Shirley Street intersections, the Park Avenue (Route 12) approaches are expected to operate with minimal increase in delay.
- Operations at the signalized intersection of Park Avenue (Route 12) with Downing Street are expected to be similar in both the No-Build and Build conditions.
- At the signalized intersection of Park Avenue (Route 12) with Maywood Street, the westbound approach is expected to experience approximately 10 additional seconds of delay in the Build Condition when compared with the No-Build condition. To mitigate this increase in delay, another Build scenario was analyzed that included optimization of traffic signal timings at the intersection. With the optimized timings, all approaches to the intersection are expected to operate with similar or lower delays than those expected under the No-Build condition.

4.2 Sight Distance Analysis

Adequate sight distance is an important safety consideration at intersections and driveways. Sight distances were reviewed at the proposed site drive intersections on Park Avenue (Route 12) and Shirley Street. Stopping sight distance (SSD) is the distance required for an approaching driver (with an eye height of 3.5 feet) to perceive and stop in time to avoid a collision with an object 2 feet high in the roadway. Corner or intersection sight distance (ISD) is based upon the time required to perceive, react, and complete a desired exiting maneuver from a driveway once the driver decides to execute the maneuver. Adjustments for the grade of the roadway are applied to both SSD and ISD.

The minimum criteria are defined by the American Association of State and Highway and Transportation Officials (AASHTO)². SSD relates specifically to safety. As indicated by AASHTO, if the available ISD meets or exceeds the minimum SSD criteria, then there is adequate safe sight distance available for motorists to avoid collisions. A criterion for calculating minimum required sight distances can be established based on operating speed, the speed at or under which most motorists (85th-percentile) actually travel along a particular portion of roadway.

The ATR data collected on Park Avenue (Route 12) in vicinity of the site show that the 85th-percentile travel speed was approximately 36 MPH in the eastbound direction and 34 MPH in the westbound. The posted speed limit for this section of Park Avenue (Route 12) was 30 MPH in both directions.

The SSD and ISD were measured in the field and compared to minimum and desirable distances. Table 7 summarizes the results of the evaluation. The measured sight distances were also adjusted accordingly to account for any vegetation that could interfere with the accuracy of the measured values. As noted in Table 7, both the minimum SSD and ISD were met in both directions for the proposed Park Avenue (Route 12) site driveway based on measured 85th-percentile travel speeds and posted speed limit. As noted in Table 7, the minimum SSD and ISD were met in both directions for the proposed Shirley Street site driveway.

² American Association of State and Highway and Transportation Officials (AASHTO), [A Policy on Geometric Design of Highways and Streets, 7th Edition](#), Washington, D.C., 2018.

Table 7 – Summary of Sight Distance Analysis at Proposed Site Driveway

Location	Sight Distance				
	Available	Posted Speed Limit		85th %-ile Speed	
	Measured (ft)	Minimum Required (ft)	Desirable (ft)	Minimum Required (ft)	Desirable (ft)
Stopping Sight Distance					
Route 12 approaching from East	1500'	200'	-	250'	-
Route 12 approaching from West	1700'	200'	-	250'	-
Shirley Street approaching from North	30'*	200'	-	250'	-
Shirley Street approaching from South	500'	200'	-	250'	-
Intersection Sight Distance					
Route 12 approaching from East	1500'	200'	340'	-	-
Route 12 approaching from West	515'	200'	340'	-	-
Shirley Street approaching from North	30'*	200'	340'	-	-
Shirley Street approaching from South	250'	200'	340'	-	-

*Distance from site driveway to Park Avenue

As indicated in Table 7, the available SSD and ISD of the site driveway along Park Avenue (Route 12) meet the minimum required distance in both directions. There is a clear line of sight between the proposed site driveway on Shirley Street and the intersection of Shirley Street and Park Avenue (Route 12). Although the minimum SSD required for a speed of 30 MPH is not available on the approach to the Shirley Street driveway, the approaching southbound vehicles are anticipated to travel at lower speeds after slowing to complete turning maneuvers from Park Avenue (Route 12) onto Shirley Street.

5.0 TRANSPORTATION DEMAND MANAGEMENT

In an effort to encourage the use of alternative modes of transportation to single-occupant vehicles, the following Transportation Demand Management (TDM) measures are recommended as part of the Project:

- Assign a transportation coordinator to be designated for the development to coordinate the elements of the TDM programs;
- Information regarding the public transportation services, including maps, schedules, and fare information should be posted in central locations. This information should be readily available in the main residential area, coffee shop, and automotive shop;
- Include a “welcome packet” to the residents detailing the available public transportation services, bicycle and walking alternatives, and commuter option available;
- Implement a monthly parking space fee, also known as “unbundling” or “parking decoupling,” policy to residents;
- Provided secure, weather-protected bicycle parking for residents, employees and customers;
- Install electrical vehicle (EV) charging stations with parking reserved for EV charging stations;
- Provide subsidized transit passes to employees.

6.0 CONCLUSIONS AND RECOMMENDATIONS

In summary, the study has shown that the proposed mixed-use project can be accommodated by the servicing roadway system with the implementation of the recommendations outlined below. The safety of the proposed site driveways were reviewed for geometry and sight distances. The Park Avenue (Route 12) site driveway and was determined to provide sufficient sight distances in accordance with AASHTO criteria for visibility and decision making of drivers attempting to enter/exit main street traffic from the proposed driveway. At the proposed site driveway on Shirley Street, the minimum sight distance is provided on the northbound approach and a clear line of sight to and from Park Avenue (Route 12) is provided on the southbound approach.

The results of the operational analysis indicate that the estimated increase in traffic resulting from the proposed mixed-use project is expected to have a minor impact on overall traffic operations at the study intersections during the weekday morning and afternoon peak hours when the site would service its greatest daily volumes.

6.1 Conclusions

The analysis indicated the following:

- The required sight distance is available at the proposed Park Avenue (Route 12) site driveway. Traffic can enter and exit the proposed site driveway without sight distance creating a safety issue. At the proposed site driveway on Shirley Street, the minimum sight distance is provided on the northbound approach and a clear line of sight to and from Park Avenue (Route 12) is provided on the southbound approach.
- The proposed development is not expected to have a significant impact on the roadway network. Levels of service at the study intersections are expected to be similar under both No-Build and Build conditions.

6.2 Recommendations

- Evaluate existing vegetation that may obstruct lines of sight exiting the site driveways. Any proposed landscaping should be set back from the edge of the roadway so as not to impede sight distance;
- Install a stop sign and stop line at the proposed site driveway approaches to Route 12 (Park Avenue) and Shirley Street;
- Reconstruct sidewalks along the site frontage and incorporate connections to the internal pedestrian walkways;
- Improve the crosswalk across Shirley Street at Park Avenue (Route 12) by restriping and reconstructing ADA-compliant curb ramps;
- Reconstruct sidewalk in front of the bus stop adjacent to the site to provide an ADA-compliant landing pad;
- Coordinate with the City of Worcester to optimize signal timings at Park Avenue (Route 12) at Maywood Street intersection prior to the opening of the proposed development;
- Implement the recommendations listed in the TDM section.



PROPOSED SITE PLAN



PARK AVENUE GREEN LIVING MIXED-USE DEVELOPMENT

487 PARK AVENUE
WORCESTER, MA 01603

SHEET INDEX

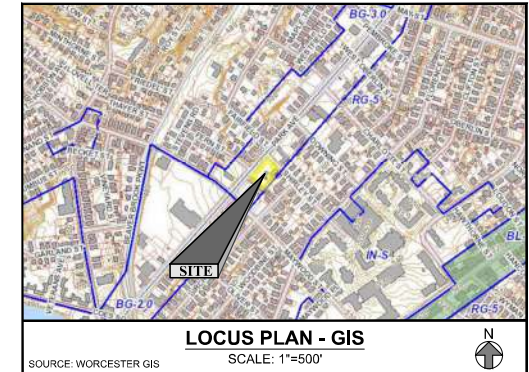
No.	Title	Issue Date
C001	Cover Sheet	April 11, 2024
C101	Site Demo & Sediment and Erosion Control Plan	April 11, 2024
C102	Layout Plan	April 11, 2024
C103	Grading & Drainage Plan	April 11, 2024
C104	Site Utilities Plan	April 11, 2024
C105	Landscaping Plan	April 11, 2024
C501	Site Details - 1	April 11, 2024
C502	Site Details - 2	April 11, 2024
C503	Site Details - 3	April 11, 2024
C504	Site Details - 4	April 11, 2024

REVISIONS/ISSUES

No.	Note	Date
1	Issued for Permitting	March 25, 2024
2	Revised Parking Calculations per DPRS Staff Review	April 11, 2024

REFERENCE PLAN INDEX

No.	Title	Issue Date
E1	Existing Conditions Plan (HS&T)	August 28, 2023



OWNER/APPLICANT

Gjinko Realty, LLC
487 Park Avenue
Worcester, MA 01610

ARCHITECT

AA Design Services, LLC
43 Monroe Avenue
Worcester, MA 01602



100 GROVE STREET | WORCESTER MA 01605
T 508-856-0321 | F 508-856-0357
gravesengineering.com



GENERAL NOTES

PARCEL DATA:
STREET ADDRESS: 487 PARK AVENUE, WORCESTER, MA 01610
AREA: 21,968 SQ. FT. (0.50 AC.)

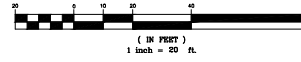
- NOTES:**
- THIS PLAN WAS PREPARED WITHOUT THE BENEFIT OF A TITLE REPORT AND IS SUBJECT TO ANY FINDINGS SUCH A REPORT MIGHT DISCLOSE.
 - LOCATION OF UNDERGROUND UTILITIES SHOWN HEREON ARE APPROXIMATE AND ARE BASED ON THE FIELD LOCATION OF VISIBLE STRUCTURES SUCH AS CATCH BASINS, MANHOLES, WATER GATES, ETC. IN ACCORDANCE WITH CHAPTER 82 SECTION 4D INCLUDING AMENDMENTS. ALL CONTRACTORS SHOULD NOTIFY IN WRITING ALL UTILITY COMPANIES AND GOVERNMENT AGENCIES PRIOR TO ANY EXCAVATION WORK AND CALL DIG-SAFE AT 811. THE CITY OF WORCESTER SHALL ALSO BE CONTACTED FOR UTILITY MARKOUTS.
 - PROPERTY LINE DATA AND TOPOGRAPHIC FEATURES ON THIS PLAN ARE BASED ON PLAN PREPARED BY HS&T GROUP, INC., DATED AUGUST 28, 2023.
 - THE SITE PROPERTY IS NOT LOCATED WITHIN ANY DESIGNATION FLOOD ZONES AS SHOWN ON FIRM MAP 250270084E, DATED JULY 4, 2011.
 - SITE SOILS PER USDA-NRCS WEB SOIL SURVEY ARE URBAN LAND (MAP UNIT 602). ON-SITE SOIL TESTING HAS NOT BEEN CONDUCTED.

REFERENCES:
DEED BOOK/PAGE: 49590/392 (WORCESTER REGISTRY OF DEEDS)
ASSESSORS REFERENCE: 08-006-0004A

SHEET KEY

FEATURES MARKED AS (⊕) SHALL BE PROTECTED
FEATURES MARKED AS (⊗) SHALL BE R&D
R&D = REMOVE AND DISPOSE (OFFSITE)
////// = R&D FEATURE

GRAPHIC SCALE

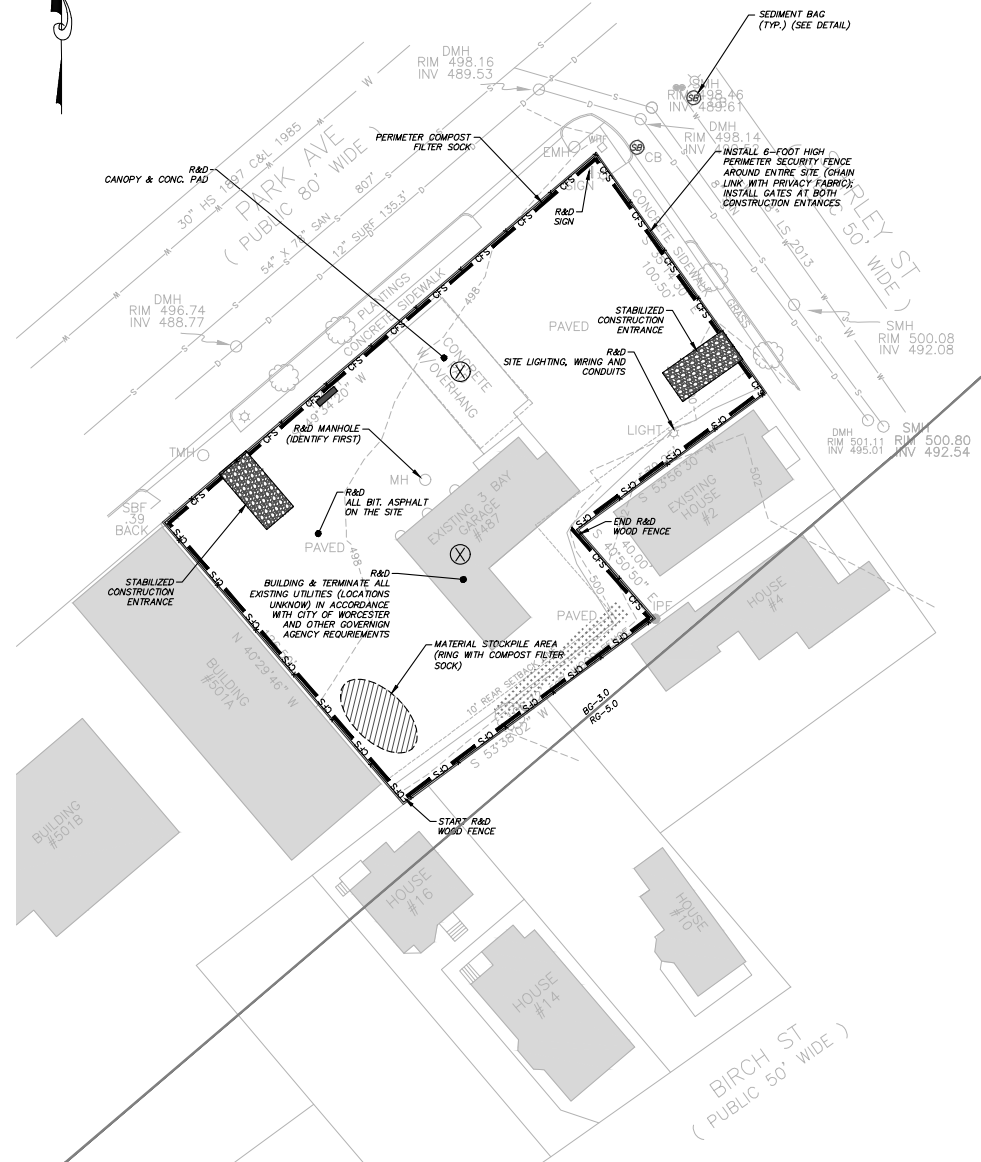


SHEET NOTES

- ALL EXISTING PAVEMENT ABUTTING NEW PAVEMENT SHALL BE NEATLY SAWCUT.
- ALL ITEMS NOTED TO BE REMOVED AND DISPOSED SHALL BE PROPERLY DISPOSED OFFSITE.
- STOCKPILE LOCATIONS (FILL MATERIALS, ETC.) SHALL BE COORDINATED WITH THE OWNER AND THE ENGINEER AND SHALL BE RINGED WITH SILT FENCE. ALL DISTURBED AREAS INCLUDING STOCKPILES, THAT WILL NOT BE RE-DISTURBED WITHIN 14 DAYS SHALL BE STABILIZED BY THE 14TH DAY AFTER THE LAST DISTURBANCE.
- THE CONTRACTOR SHALL EMPLOY ANY AND ALL EROSION AND SEDIMENT CONTROL MEASURES TO PREVENT A RELEASE OF SEDIMENT FROM THE SITE.
- SEDIMENT AND EROSION CONTROLS SHALL BE INSTALLED PRIOR TO THE START OF ANY SITE WORK.
- SEDIMENT TRACKED ONTO ROADWAYS ADJACENT TO THE SITE SHALL BE SWEEPED CLEAN AND REMOVED EACH DAY.
- PERIMETER SEDIMENT CONTROLS SHOWN ARE INTENDED TO THE MINIMUM REQUIRED; THE CONTRACTOR SHALL EMPLOY AND INSTALL ANY OTHER CONTROLS AS NECESSARY TO PREVENT SEDIMENT RELEASE FROM THE SITE INCLUDING AERIAL (DUST).

LEGEND

— D —	SEWER	574.6	SPOT ELEVATION (FINISHED GRADE)
— FM —	SEWER FORCE MAIN	+	SIGN
— W —	WATER	TP-#	TEST PIT - NUMBER
— OW —	OVERHEAD UTILITY	⊕	TRAFFIC CONTROL SIGNAL
— UST —	UNDERGROUND TELEPHONE	⊗	TREE LINE
— UE —	UNDERGROUND ELECTRIC	—	EOP EDGE OF PAVEMENT
— CATV —	UNDERGROUND CABLE & TELEVISION	D/S	DOWN SPOUT
— GAS or G —	UNDERGROUND GAS	CCB	CAPE COD BERM
— IR —	IRRIGATION WATER	CONC.	CONCRETE
— RD —	ROOF DRAIN	BIT.	BITUMINOUS
— SW —	STRAW WATTLE	FFE	FINISHED FLOOR ELEVATION
— HSF —	HAYBALES AND SILT FENCE	H.P.	HIGH POINT
— LOW —	LIMIT OF WORK	L.P.	LOW POINT
⊕	HANDICAP PARKING STALL	F.G.	FINISH GRADE
→	FLOW DIRECTION (PITCH)	GAS VALVE	GAS VALVE
⊕	WELL	WATER VALVE	WATER VALVE
⊕	MONITORING WELL	HYDRANT	HYDRANT
⊕	LIGHT	WATER SHUT-OFF	WATER SHUT-OFF
⊕	TREE, TRUNK DIAM., DECIDUOUS (DEC.) OR CONIFEROUS (CON.)	BOUND	BOUND
24" DEC.		FLARED END SECTION	FLARED END SECTION
⊕	STUMP	UTILITY POLE	UTILITY POLE
⊕	SEWER MANHOLE	UTILITY POLE & GUY WIRE	UTILITY POLE & GUY WIRE
⊕	DRAIN MANHOLE	▲	LIGHT FIXTURE
⊕	ELECTRIC MANHOLE		
⊕	TELEPHONE MANHOLE		
⊕	CATCH BASIN		
⊕	DOUBLE CATCH BASIN		
●	AREA DRAIN		



NO.	DATE	BY	DESCRIPTION
1	03/25/24	DPS	ISSUED FOR PERMITTING
2	04/11/24	DPS	REVISED PARKING CALCULATIONS



SITE DEMO, SEDIMENT & EROSION CONTROL PLAN
PARK AVENUE GREEN LIVING - MIXED-USE DEVELOPMENT
487 PARK AVENUE, WORCESTER, MA

PREPARED FOR: CUSCINO
DATE: 03/25/24
SCALE: 1"=20'
DES. BY: DPS
DRAW. BY: DPS
CHK. BY: MRA
PRJ. NO.: 23025

NO.	DATE	BY	DESCRIPTION
1	03/25/24	DFS	ISSUED FOR PERMITTING
2	04/11/24	DFS	REVISED PARKING CALCULATIONS



PROJECT: PARK AVENUE GREEN LIVING - MIXED-USE DEVELOPMENT
 487 PARK AVENUE, WORCESTER, MA 01603

DATE: 03/25/24
 SCALE: 1"=20'

PREPARED FOR:
 C/S (SUNCO)
 487 PARK AVENUE, WORCESTER, MA 01603

DES. BY: DFS
 CHK. BY: MFA
 PRJ. NO.: 23025

DRAWN BY: DFS

DATE: 03/25/24

PROJECT ZONING INFORMATION

CRITERIA	REQUIRED	PROPOSED
USE	AUTO SALES (EXIST.)	MIXED USE
ZONING DISTRICT	80-3.0	80-3.0
OVERLAY DISTRICT	NONE	NONE
MINIMUM LOT AREA	5,000 SQ.FT.(1)	21,968 SQ. FT.
MINIMUM FRONTAGE	40 FT./DU (2)	194.41 FT. (3)
FRONT SETBACK	NA	0 FT.
SIDE SETBACK	NA	0 FT.
REAR SETBACK	10 FT.(4)	44.1 FT.
MAXIMUM BLDG. STORIES	NA	6
MAXIMUM BLDG. HEIGHT	100 FT.	75 FT.
BLDG. LOT COVERAGE	N/A	32.9%
MAXIMUM F.A.R.	3.1	1.8:1
LANDSCAPE PARKING BUFFER	5 FT.	2 FT. (5)
LOT IMPERVIOUS COVERAGE	93%	88%
RECREATION AREA	10% OF LOT AREA	3,856 SQ.FT. (17.5%) (6)

ZONING INFORMATION SOURCE: THE ABOVE INFORMATION WAS OBTAINED FROM THE CITY OF WORCESTER ZONING ORDINANCE, AMENDED THROUGH MAY 9, 2023.

- FOOTNOTES:**
- (1) 5,000 SQ.FT. REQ'D FOR RESIDENTIAL USE, NA FOR NON-RESIDENTIAL USE.
 - (2) MAX. 200 FT. REQUIRED FOR RESIDENTIAL USE; NA FOR NON-RESIDENTIAL USE.
 - (3) RELIEF SOUGHT FOR OF 5.59 FEET OF FRONTAGE (SEE TABLE THIS SHEET).
 - (4) 10 FT. REQUIRED FOR BOTH RESIDENTIAL AND NON-RESIDENTIAL USE.
 - (5) RELIEF SOUGHT FOR REDUCTION OF LANDSCAPE PARKING BUFFER (SEE TABLE THIS SHEET).
 - (6) RECREATION AREA INCLUDES INTERIOR COMMON SPACES AND EXTERIOR AREAS OUTSIDE OF THE PARKING BUFFER.

SHEET NOTES

- 1) ALL DIMENSIONS ARE MEASURED FROM TOE OF CURB OR CENTER OF PAINTED LINE.
- 2) THE CONTRACTOR SHALL COORDINATE WITH PLANS BY OTHERS (ARCHITECTURAL, STRUCTURAL, MEP, ETC.)
- 3) PAVEMENT MARKINGS SHALL BE PAINTED WITH WHITE TRAFFIC PAINT CONFORMING TO SECTION M7.01.08 OF THE MASSDOT STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES, LATEST EDITION. LINE WIDTH SHALL BE 4 INCHES. HANDICAP SYMBOL MARKINGS SHALL COMPLY WITH 521 CMR WITH WHITE SYMBOL ON BLUE BACKGROUND.
- 4) ALL JOINTS OF EXISTING & PROPOSED BITUMINOUS PAVEMENT SHALL BE SEALED WITH HOT RUBBERIZED ASPHALT JOINT SEALANT AND SANDED.
- 5) THE SITE CONTRACTOR SHALL COORDINATE WITH MECHANICAL/HVAC PLANS FOR ADDITIONAL EXTERIOR EQUIPMENT PADS THAT MAY NOT BE SHOWN ON THESE DRAWINGS.
- 6) THE CONTRACTOR SHALL WORK WITHIN THE PARK AVENUE AND SHIRLEY STREET RIGHT-OF-WAYS WITH THE CITY OF WORCESTER DEPARTMENT OF PUBLIC WORKS AND PARKS WITH REGARD TO THIS PROJECT'S WORK AND THE PROPOSED/CONSTRUCTED STREET IMPROVEMENTS.

PARKING SCHEDULE

ITEM	CALCULATION	PARKING REQUIREMENT
RESIDENTIAL - MULTI-FAMILY DWELLING	40 DU x 2 SPACES/DU	80 SPACES
ARTICLE VII REDUCTION (SECTION 6.A.I.) DENSITY	20% UNIT DENSITY BONUS	-16 SPACES (1)
ARTICLE VII REDUCTION (SECTION 6.A.II.a)	25% PARKING REDUCTION	-16 SPACES (2)
ARTICLE VII REDUCTION (SECTION 6.A.II.c) PB SPECIAL PERMIT	50% OF ORIGINAL REQUIREMENT	-8 SPACES (3)
RESIDENTIAL PARKING SUBTOTAL	-	40 SPACES
AUTO SERVICE	2 BAYS x 1 SPACE/BAY	2 SPACES
BUSINESS - FOOD SERVICE	10 PERSONS x 0.5 SPACES/OCCUPANCY	5
TOTAL REQUIRED PARKING	-	47 SPACES
TOTAL PARKING PROVIDED	-	34 SPACES
PARKING SHORTFALL	-	13 SPACES (4)

- NOTES:**
- (1) **DENSITY BONUS CALCULATION:** 5% UNITS AT 60% AMI + 10% UNITS AT 80% AMI = 15% + 5% BONUS = 20%; 40 UNITS x 20% = 32 UNITS; 32 UNITS x 2 SPACES/UNIT = 64 SPACES REQUIRED; 80 SPACES - 64 SPACES = -16 SPACES.
 - (2) **TMP REDUCTION CALCULATION:** WITH SUBMITTAL OF A TRANSPORTATION MANAGEMENT PLAN (TMP), PARKING REQUIREMENTS ARE REDUCED BY 25% 64 SPACES x 25% = -16 SPACES. 64 SPACES - 16 SPACES = 48 SPACES REQUIRED..
 - (3) **PB SPECIAL PERMIT REDUCTION:** PB SPECIAL PERMIT ALLOWS A MAXIMUM 50% REDUCTION IN PARKING OF THE ORIGINAL REQUIREMENT BEFORE ANY ADJUSTMENTS: 80 SPACES x 50% = 40 SPACES REQUIRED; 48 SPACES - 40 SPACES = - 8 SPACES.
 - (4) SEE TABLE THIS SHEET FOR PARKING RELIEF SOUGHT.
 - (5) THE PROPOSED PARKING CONSISTS OF THE FOLLOWING:
 15 STANDARD SPACES (9'x18'); INCLUDES 8 EV OPERATIONAL
 17 COMPACT SPACES (8'x18'); 50% OF TOTAL PARKING
 2 VAN ACCESSIBLE HANDICAP SPACES (8'x18'); 2 EV OPERATIONAL
 10 EV OPERATIONAL SPACES (29% OF TOTAL PARKING)

SIGNAGE KEY

GEI ID	FEDERAL ID	SIGN SIZE		SIGN LAYOUT	QUANTITY
		WIDTH	HEIGHT		
S-1	R7-8	12"	18"		2
S-2	R7-8a	12"	6"		2

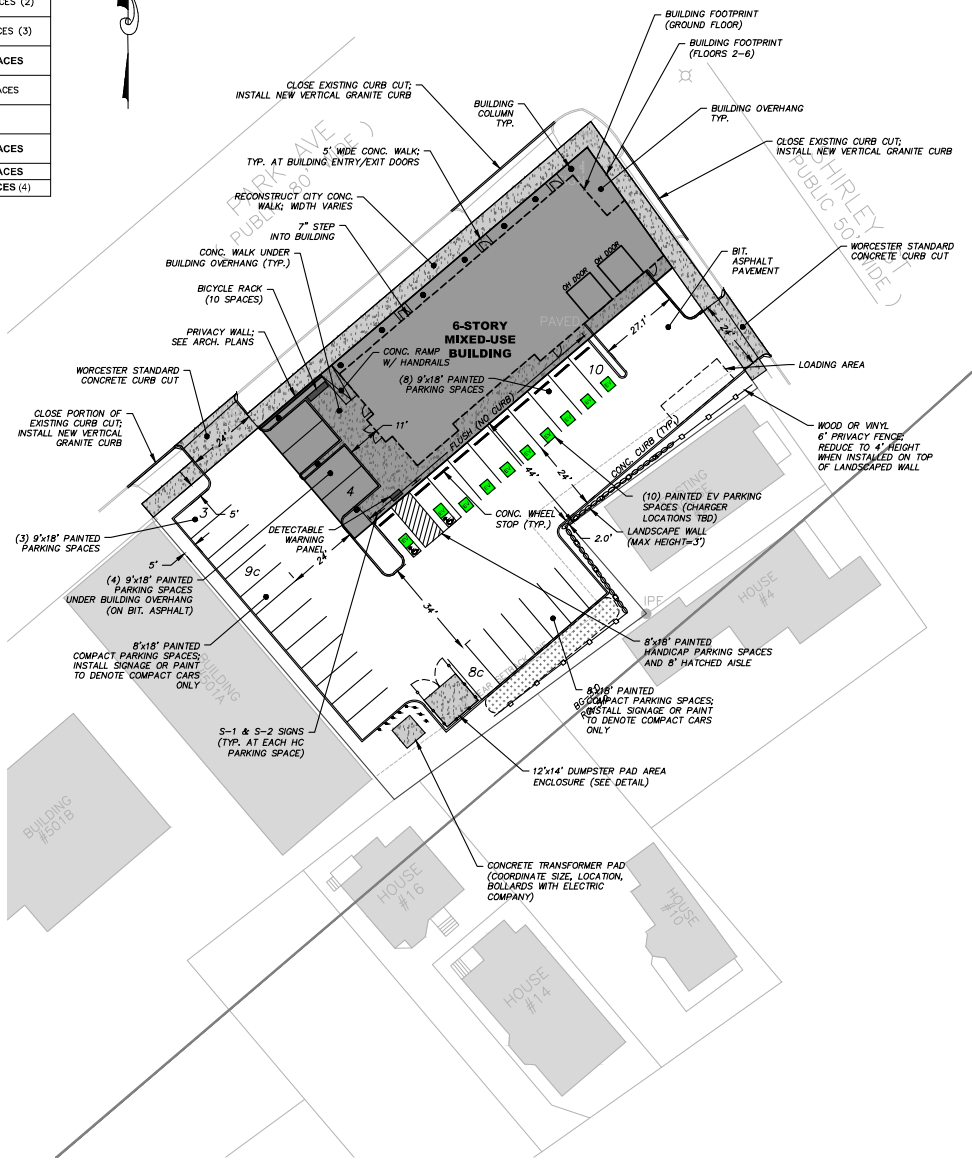
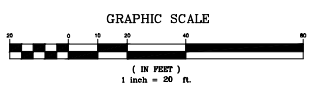
SNOW STORAGE NOTES

SNOW SHALL BE STORED WITHIN AREAS AS SHOWN ON THIS SHEET DESIGNATED BY THIS HATCHING

SNOW STORAGE LOCATIONS SHOWN REPRESENT THE MINIMUM NECESSARY AND THE OWNER SHALL EXPAND SUCH AREAS AS NEEDED. SNOW SHALL NOT BE PLACED SO AS TO INHIBIT VEHICLE SIGHT LINES FOR ENTERING AND EXITING THE SITE AND INTERNAL SITE CIRCULATION NOR PLACED TO INHIBIT SITE DRAINAGE. EXCESS SNOW SHALL BE HAULED OFF-SITE AS NECESSARY.

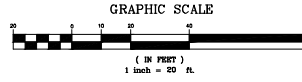
PROJECT RELIEF SOUGHT

- THE PROJECT SEEKS THE FOLLOWING RELIEF FROM THE CITY OF WORCESTER:
- ZONING BOARD OF APPEALS:**
- 1) **VARIANCE** TO ALLOW FOR THE EXISTING FRONTAGE OF 194.41 FEET TO REMAIN IN LIEU OF THE REQUIRED 200 FEET.
 - 2) **VARIANCE** TO REDUCE PARKING REQUIREMENTS BY 13 SPACES.
- PLANNING BOARD:**
- 1) **SPECIAL PERMIT** TO ALLOW FOR 50% OF THE PARKING SPACES TO BE COMPACT (ART. IV, SECTION 7, TABLE 4.4, NOTE 3).
 - 2) **SPECIAL PERMIT** TO REDUCE THE LANDSCAPE BUFFER FROM THE REQUIRED 5 FEET TO 2 FEET FOR A PORTION OF THE PROJECT (ART. IV, SECTION 7, TABLE 4.4, NOTE 5).



SHEET NOTES

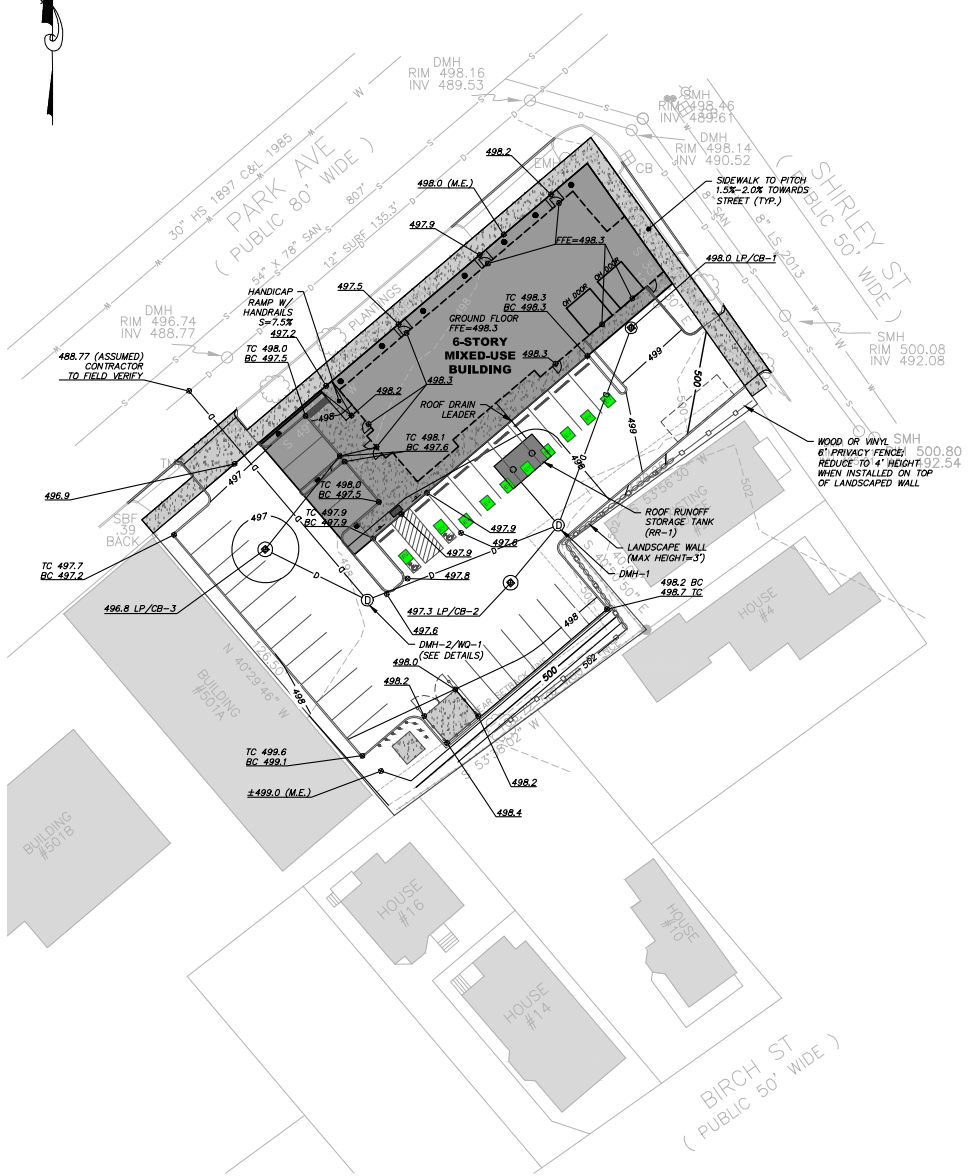
- 1) PROVIDE POSITIVE DRAINAGE IN ALL FINISH GRADED WORK AREAS INCLUDING AWAY FROM ALL BUILDINGS.
- 2) ADJUST ALL STRUCTURES TO PROPOSED FINISH GRADES WITHIN THE LIMIT OF WORK.
- 3) ALL DISTURBED AREAS OUTSIDE OF PAVEMENT SHALL BE RESTORED WITH 6 INCHES OF LOAM AND SEEDED OR LANDSCAPED IN ACCORDANCE WITH THE LANDSCAPING PLANS.
- 4) ALL FINISHED GRASSED SLOPES EQUAL TO OR GREATER THAN A 3:1 SLOPE SHALL BE STABILIZED WITH AN EROSION CONTROL MATTING SUCH AS SC-150 BY NORTH AMERICAN GREEN, CURLEX II BY AMERICAN EXCELSIOR, OR ENGINEER APPROVED EQUAL. COORDINATE MATTING LOCATIONS WITH THE LANDSCAPE PLANS AND GROUND COVER MATERIALS.
- 5) PROPOSED GRADES AS SHOWN SHALL BE CARRIED TO ALL PAVEMENT CORNERS HOWEVER GRADE ADJUSTMENTS SHALL BE MADE BY THE CONTRACTOR AS NECESSARY TO PROVIDE POSITIVE DRAINAGE FROM ALL CORNERS.
- 6) SPOT GRADE KEY:
 (ME) = MATCH EXISTING GRADE
 TC = TOP OF CURB FINISH GRADE
 BC = BOTTOM OF CURB FINISH GRADE
 FFE = BUILDING FINISH FLOOR ELEVATION
 CB = CATCH BASIN
 HP = HIGH POINT
 LP = LOW POINT
 TW = TOP OF WALL (FINISH GRADE)
 BW = BOTTOM OF WALL (FINISH GRADE)
- 7) PRIOR TO WORK, THE CONTRACTOR SHALL VERIFY THE DEPTH OF COVER OVER THE EXISTING UNDERGROUND UTILITIES IN THE AREA OF THE PROPOSED WORK. IF ADEQUATE COVER IS NOT PRESENT TO THE PROPOSED FINISH GRADES, THE CONTRACTOR SHALL CONTACT THE DESIGN ENGINEER IMMEDIATELY BEFORE BEGINNING WORK IN THIS AREA.
- 8) HANDICAP RAMPS AND ACCESSIBLE ROUTES SHALL BE CONSTRUCTED IN CONFORMANCE WITH 521 CMR "ARCHITECTURAL ACCESS BOARD" REGULATIONS. SHOULD ANY DISCREPANCIES BE DISCOVERED ON THESE DRAWINGS THAT MAY PREVENT FULL COMPLIANCE WITH SAID REGULATIONS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY BEFORE PROCEEDING WITH THE WORK.



PROPOSED STORM DRAIN ELEVATION TABLE								
STRUCTURE	RIM ELEV.	STRUCTURE ELEVATIONS		NOTES	PIPE DATA			
		PENETRATIONS	INVERT		FROM	TO	LENGTH (Ft.)	SCOPE (Ft.)
CB-1	498.0	8" DR18 OUT (DMH-1)	494.00		CB-1	DMH-1	69.7	1.50
DMH-1	497.7	8" DR18 IN (CB-1)	492.95					
		12" PVC IN (RR-1)	494.91					
		8" DR18 IN (CB-2)	492.95					
		12" HDPE OUT (DMH-2)	492.85		DMH-1	DMH-2	65.0	2.00
CB-2	497.3	8" DR18 OUT (DMH-1)	493.30		CB-2	DMH-1	23.0	1.50
DMH-2	497.3	8" DR18 IN (CB-3)	492.55	WO-1 (STIC 450)				
		12" HDPE IN (DMH-1)	491.49					
		12" DR18 OUT (EXISTING PARK AVE)	491.39		DMH-2	PARK AVE	92.7	2.83
CB-3	496.8	8" DR18 OUT (DMH-2)	493.13		CB-3	DMH-2	36.4	1.59
RR-1	497.8	10" PVC IN (ROOF LEADERS)	495.50					
		10" PVC OUT (DMH-1)	495.30		RR-1	DMH-1	19.5	2.00

DRAINAGE TABLE KEY:

- INV.: PIPE INVERT ELEVATION
- ID: INSIDE DIAMETER
- CB-#: CATCH BASIN-NUMBER
- DMH-#: DRAIN MANHOLE-NUMBER
- HDPE: HIGH DENSITY POLYETHYLENE DRAINAGE PIPE
- PVC: SCH40 PVC PIPE
- DI: DUCTILE IRON
- TBD: TO BE DETERMINED
- WO-1: STORMWATER WATER QUALITY UNIT, STORMCEPTOR 450I OR ENGINEER APPROVED EQUAL
- RR: ROOF RUNOFF STORMWATER TANK
- DR18: DR-18 PVC PIPE (BLUE BRUTE OR EQUAL)



NO.	DATE	BY	DESCRIPTION
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GRADING & DRAINAGE PLAN
 PARK AVENUE GREEN LIVING - MIXED-USE DEVELOPMENT
 487 PARK AVENUE, WORCESTER, MA

PREPARED FOR: CUSCUSKINO
 487 PARK AVENUE, WORCESTER, MA 01603
 DATE: 03/25/24
 SCALE: 1"=20'
 DRAWN BY: DPS
 CHECKED BY: MRA
 PERM. NO.: 23025

SHEET NOTES

- 1) PRIOR TO WORK, THE CONTRACTOR SHALL VERIFY THE DEPTH OF COVER OVER THE EXISTING UNDERGROUND UTILITIES IN THE AREA OF THE PROPOSED WORK. IF ADEQUATE COVER IS NOT PRESENT TO THE PROPOSED FINISH GRADES, THE CONTRACTOR SHALL CONTACT THE DESIGN ENGINEER IMMEDIATELY BEFORE BEGINNING WORK IN THIS AREA.
- 2) UTILITY MATERIALS, CONSTRUCTION, & TESTING SHALL COMPLY WITH THE WORCESTER DEPARTMENT OF PUBLIC WORKS SPECIFICATIONS, LATEST EDITION.
- 3) THE CONTRACTOR SHALL COORDINATE THE UTILITY WORK WITH THE PROPOSED BUILDING PLANS.
- 4) ALL UTILITY WORK SHALL BE COORDINATED BY THE CONTRACTOR WITH THE PROPER UTILITY COMPANIES.
- 5) THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION OF ALL EXISTING WATER AND SEWER SERVICE UTILITIES. IF CONFLICTS ARE FOUND THAT PROHIBIT THE INSTALLATION OF THE UTILITIES AS SHOWN ON THESE DRAWINGS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY BEFORE PROCEEDING WITH THE WORK.
- 6) NEW CATCH BASIN FRAMES AND GRATES SHALL BE CITY STANDARD (MODEL: EJIW 7288Z1 (3-FLANGE), 7288Z (4-FLANGE), OR LEBARON LF288 CAST IRON FRAME & EJIW 7288M OR LEBARON L28S1 CAST IRON GRATE.
- 7) EXISTING WATER AND SEWER UTILITIES SHALL REMAIN.
- 8) THE SITE CONTRACTOR SHALL COORDINATE WITH AND PROVIDE NECESSARY EXCAVATION FOR ELECTRICAL CONDUIT BETWEEN THE BUILDING AND SITE LIGHTING LOCATIONS.

LIGHTING NOTES

- 1) PHOTOMETRIC INFORMATION BASED ON COOPER LIGHTING SOLUTIONS GALN GALLEON MODEL GALN-SA4D-730-1-SL3 LED FIXTURE MOUNTED AT A HEIGHT OF 15 FEET AGL. COLOR TEMPERATURE = 3,000K.
- 2) THE CONTRACTOR SHALL COORDINATE WITH THE OWNER AND PROVIDE ALL NECESSARY WORK FOR UNDERGROUND ELECTRICAL WIRING AND CONDUITS FOR PROPOSED LIGHTING.
- 3) FC = FOOTCANDLE
- 4) PROPOSED BUILDING WALL PACK LIGHT FIXTURES ARE NOT SHOWN.
- 5) ALL LIGHT FIXTURES SHALL BE DARK SKY COMPLIANT.

Dimensional Details

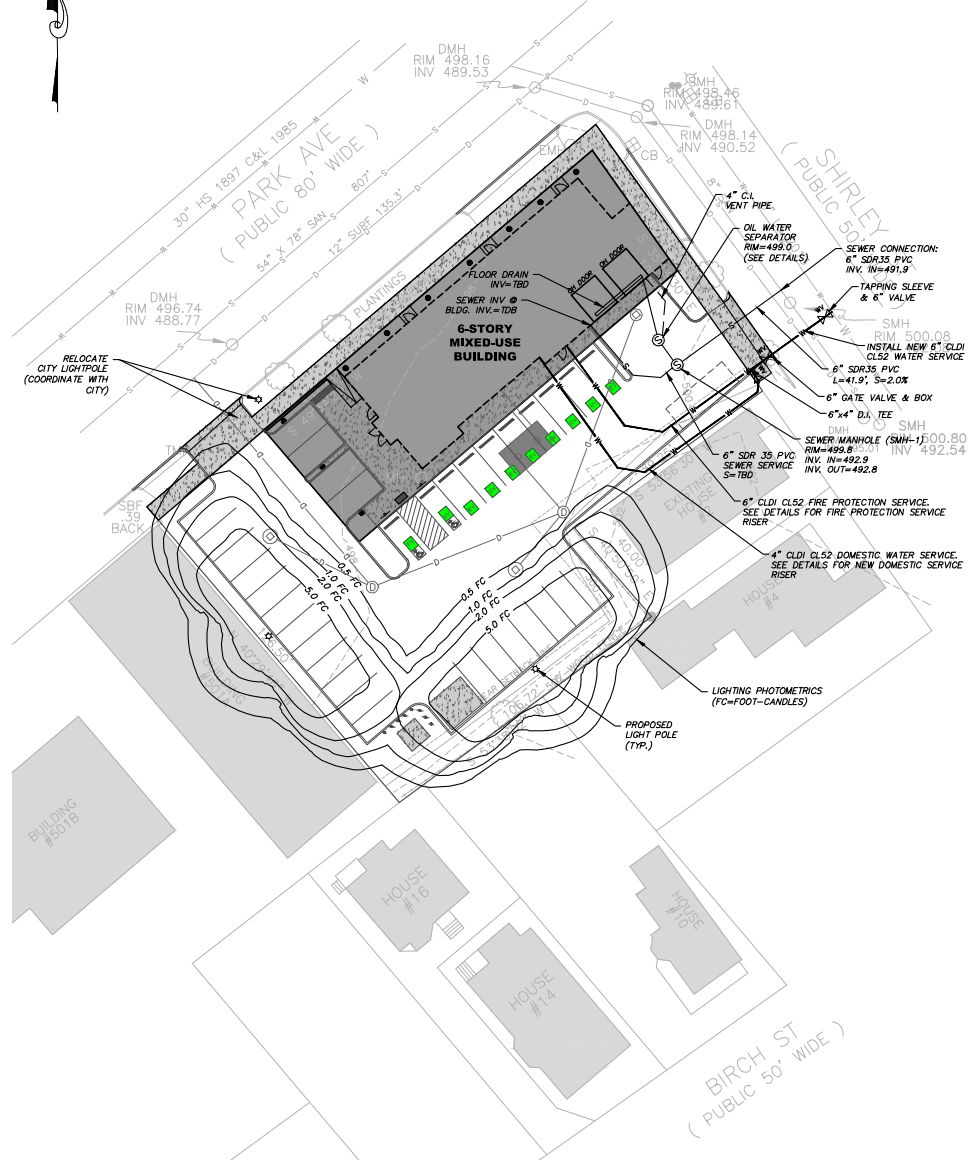
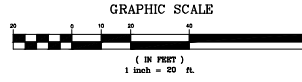
Model	Height	Beam Angle	Beam Diameter @ 15' Ht	Beam Diameter @ 30' Ht	Beam Diameter @ 45' Ht	Beam Diameter @ 60' Ht	Beam Diameter @ 75' Ht	Beam Diameter @ 90' Ht	Beam Diameter @ 105' Ht	Beam Diameter @ 120' Ht
GALN-SA4D-730-1-SL3	15'	120°	15'	30'	45'	60'	75'	90'	105'	120'

COOPER LIGHTING SOLUTIONS

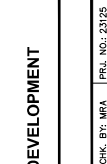
McGraw-Hill Construction Information Group
GALN-SA4D-730-1-SL3
GLEON Galleon



LIGHT POLE FIXTURE NOTES



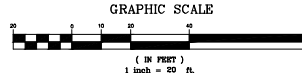
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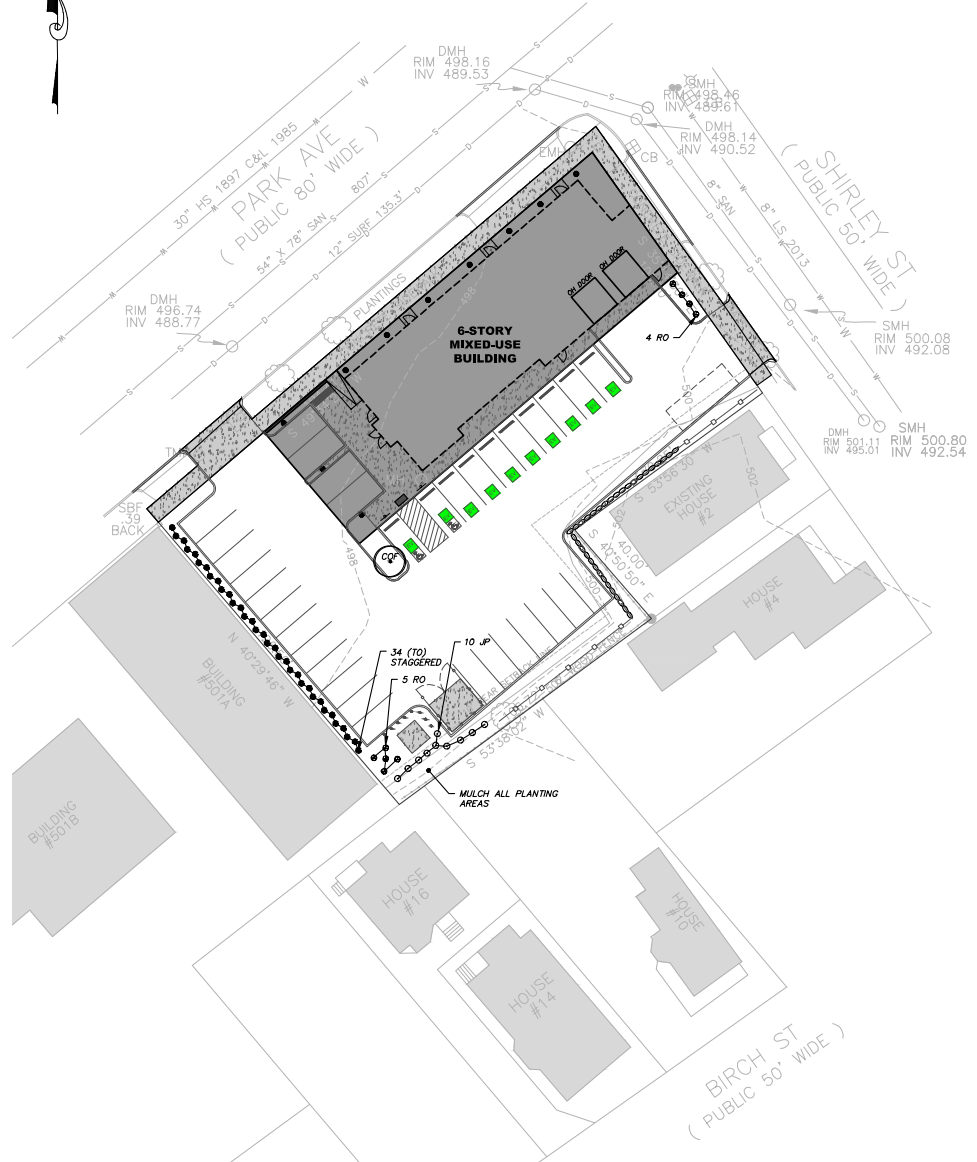
UTILITIES PLAN
PARK AVENUE GREEN LIVING - MIXED-USE DEVELOPMENT
 487 PARK AVENUE, WORCESTER, MA
 PREPARED FOR: OLSI (SINKO)
 DATE: 03/25/24
 SCALE: 1"=20'
 DES. BY: DPS
 DRW. BY: DPS
 CHK. BY: MRA
 PROJ. NO.: 23025

SHEET NOTES

- 1) ALL PLANTINGS SHALL BE ASIAN LONGHORN BEETLE AND EMERALD ASH BORER RESISTANT, NON-HOST SPECIES. SPECIES NOTED ON THIS PLAN MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- 2) ALL PLANTINGS SHALL BE WATERED AND FERTILIZED AS NECESSARY UNTIL ESTABLISHED. THE CONTRACTOR SHALL INSTALL A SLOW-RELEASE WATERING BAG AT EACH TREE PLANTING.
- 3) TREES SHALL BE SUPPORTED BY TEMPORARY STAKING AS NECESSARY UNTIL ESTABLISHED.



PLANTING LIST					
CODE	QTY.	BOTANICAL NAME	COMMON NAME	SIZE	NOTES
ORNAMENTAL TREES					
OP	1	CORNUS FLORIDA	FLOWERING DOGWOOD	3" CAL.	6" ABOVE GROUND
SHRUBS					
TO	34	T. OCCIDENTALIS SMARAGD	EMERALD GREEN ARBORVITAE	4'-5' HT.	PLANT WHERE SHOWN; 24"-36" O.C.
RO	9	RHODODENDRON 'ROBLEZA'	AUTUMN BONFIRE	2' HT.	3 GALLON
JP	10	JUNIPERUS HORIZONTALIS	BLUE RUG JUNIPER	2' HT.	2 GALLON

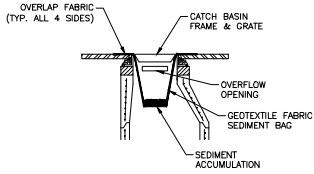


NO.	DATE	BY	DESCRIPTION
1	03/25/24	DPS	ISSUED FOR PERMITTING
2	04/11/24	DPS	REVISED PARKING CALCULATIONS



LANDSCAPE PLAN
PARK AVENUE GREEN LIVING - MIXED-USE DEVELOPMENT
 487 PARK AVENUE, WORCESTER, MA

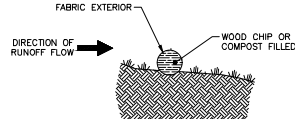
DES. BY: DPS
 DRW. BY: DPS
 CHK. BY: MRA
 PREPARED FOR: OL'SI/SINKO
 PROJECT NO.: 23125
 SCALE: 1"=20'
 DATE: 03/25/24



CATCH BASIN SEDIMENT BAG NTS

NOTES:

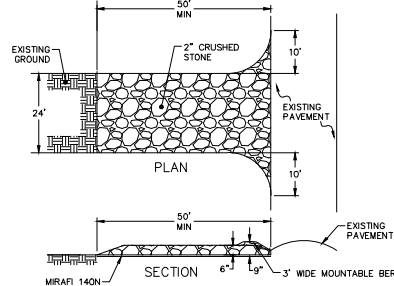
- 1) SEDIMENT BAG SHALL BE SILTSACK BY ACF ENVIRONMENTAL OR APPROVED EQUAL.
- 2) OIL ABSORPTION MEDIUM MAY ALSO BE PLACED IN BAG (OPTIONAL).
- 3) INSTALL AND MAINTAIN PER MANUFACTURER'S INSTRUCTIONS.
- 4) A SEDIMENT BAG SHALL BE PROVIDED FOR PROPOSED CATCH BASINS THAT ARE ACCEPTING STORMWATER RUNOFF PRIOR TO FINAL STABILIZATION.



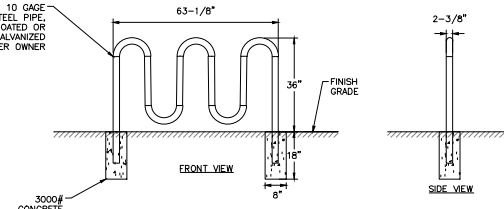
COMPOST FILTER SOCK NTS

NOTE:

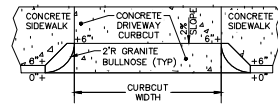
- 1) PROVIDE A 3' TO 6' LEVEL AREA BETWEEN THE SILT SOCK AND THE TOE OF ANY SLOPE TO PROVIDE AREA FOR SEDIMENT ACCUMULATION.



STABILIZED CONSTRUCTION ENTRANCE NTS



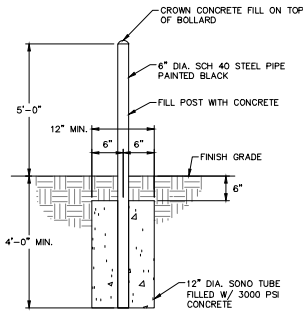
BIKE RACK NTS



WORCESTER STANDARD CURB CUT NTS

NOTES:

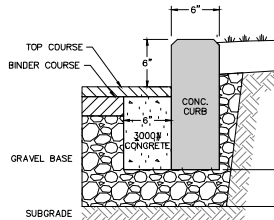
- 1) CONCRETE SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CONCRETE WALK DETAIL ON THESE PLANS (6" THICKNESS ACROSS CURB CUTS).



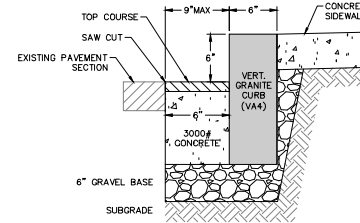
BOLLARD NTS

NOTE:

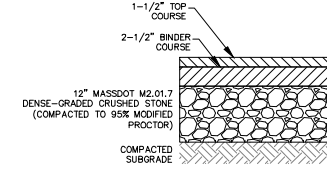
- 1) THIS DETAIL APPLIES TO BOLLARD INSTALLED AROUND THE PROPOSED TRANSFORMER AND COMPLIES WITH NGRID SPECIFICATIONS.



PRECAST CONCRETE CURB NTS



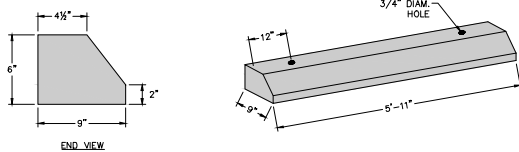
VERTICAL GRANITE CURB ALONG EXISTING ROADWAY NTS



PAVEMENT SECTION NTS

NOTES:

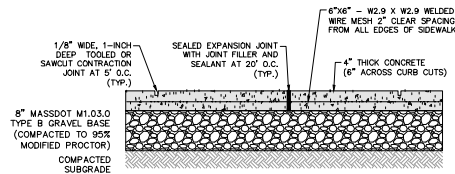
- 1) TAMP ALL ASPHALT EDGES THAT ABUT LAWN, LANDSCAPED OR OTHER SOFT SURFACE.
- 2) BINDER COURSE: - MASSDOT M3.11.03, TABLE A, "HMA INTERMEDIATE COURSE DENSE BINDER" OR SUPERPAVE INTERMEDIATE COURSE - 19.0MM (MIXTURE DESIGNATION SIC - 19.0).
- 3) TOP COURSE: - MASSDOT M3.11.03, TABLE A, "SURFACE COURSE STANDARD TOP" OR SUPERPAVE SURFACE COURSE - 12.5MM (MIXTURE DESIGNATION SSC - 12.5).



WHEEL STOP NTS

NOTES:

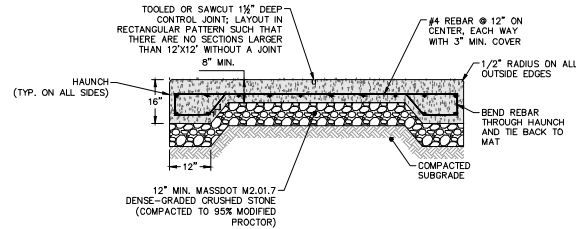
- 1) WHEEL STOPS SHALL BE CONSTRUCTED OF 4,000 PSI CONCRETE (28 DAYS) AND SECURED TO THE GROUND WITH STEEL PINS.
- 2) RUBBER OR OTHER MATERIAL WHEEL STOPS MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.



CONCRETE SIDEWALK NTS

NOTES:

- 1) CONCRETE MIX DESIGN SHALL BE FOR EXTERIOR USE; AIR-ENTRAINED, 4,000 PSI MIN. (28-DAY), 3/4" AGGREGATE.
- 2) WHEN THE WALKWAY ABUTS A STRUCTURE OR EXISTING HARD SURFACE (CONCRETE, ETC.), INSTALL AN EXPANSION JOINT ACROSS THE ENTIRETY OF THE ABUTTING FACE WITH PREFORMED FIBER JOINT FILLER AND FILL WITH 1" DEEP OF POLYURETHANE JOINT SEALANT.
- 3) WALKWAYS SHALL HAVE A LIGHT BROOM FINISH PERPENDICULAR TO THE DIRECTION OF FOOT TRAFFIC; THERE SHALL BE NO SMOOTHED TOOL JOINTS; BROOM FINISH SHALL EXTEND TO THE EDGES OF THE PAD AND ACROSS ANY JOINTS.



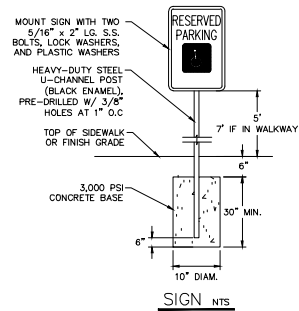
CONCRETE PAD-HEAVY DUTY NTS

NOTES:

- 1) CONCRETE MIX DESIGN SHALL BE FOR EXTERIOR USE; AIR-ENTRAINED, 4,000 PSI MIN. (28-DAY), 3/4" AGGREGATE.
- 2) OMIT THE HAUNCH WHEN ABUTTING ANOTHER HARD SURFACE SUCH AS EXISTING CONCRETE (BITUMINOUS IS NOT A HARD SURFACE).
- 3) WHEN THE PAD ABUTS A STRUCTURE OR EXISTING HARD SURFACE (CONCRETE, ETC.), INSTALL AN EXPANSION JOINT ACROSS THE ENTIRETY OF THE ABUTTING FACE WITH PREFORMED FIBER JOINT FILLER AND FILL WITH 1" DEEP OF POLYURETHANE JOINT SEALANT.
- 4) PAD SHALL HAVE A MEDIUM BROOM FINISH. THERE SHALL BE NO SMOOTHED TOOL JOINTS; BROOM FINISH SHALL EXTEND TO THE EDGES OF THE PAD AND ACROSS ANY JOINTS.

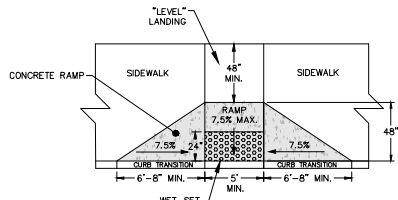
NO.	DATE	BY	DESCRIPTION
1	03/25/24	DFS	ISSUED FOR PERMITTING
2	04/11/24	DFS	REVISED PARKING CALCULATIONS





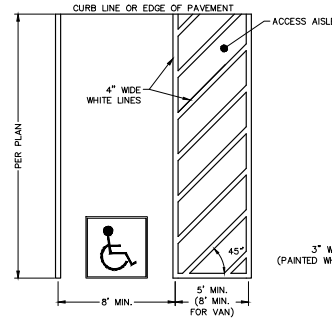
SIGN NTS

NOTE:
 1) THIS DETAIL APPLIES TO THE CONSTRUCTION OF ALL PROPOSED SIGNS. (SIGN TYPE AND DIMENSIONS MAY VARY FROM THOSE SHOWN). HANDICAP VAN ACCESSIBLE PLACARD SHALL BE MOUNTED DIRECTLY BELOW PRIMARY SIGN.



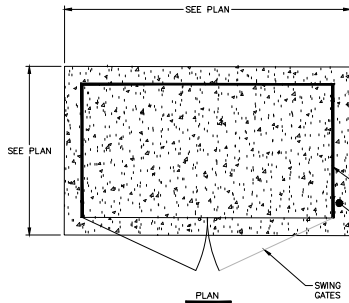
CONCRETE CURB CUT WHEELCHAIR RAMP NTS

NOTES:
 1) FOR SIDEWALKS PART OF AN ACCESSIBLE ROUTE, THE MAXIMUM ALLOWABLE RUNNING SLOPE SHALL BE 4.5% AND THE MAXIMUM ALLOWABLE CROSS SLOPE SHALL BE 1.5% (1.0% MIN.).
 2) "LEVEL LANDING" SHALL BE SLOPED NO MORE THAN 1.5% IN ANY DIRECTION (1.0% MIN. FOR CROSS SLOPE).
 3) A "LEVEL LANDING" 48" MIN. IN LENGTH MUST BE PROVIDED AT THE TOP OF EACH RAMP.

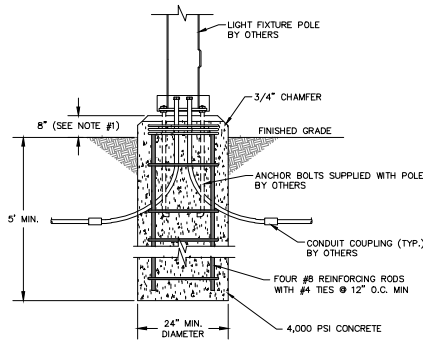


HANDICAP PARKING SPACE NTS

NOTE:
 1) PAVEMENT MARKINGS SHALL BE PAINTED WITH WHITE TRAFFIC PAINT CONFORMING TO SECTION M7.01.08 OF THE MASSDOT STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES, LATEST EDITION. LINE WIDTH SHALL BE 4 INCHES. HANDICAP SYMBOL MARKINGS SHALL COMPLY WITH 521 CMR WITH WHITE SYMBOL ON BLUE BACKGROUND.

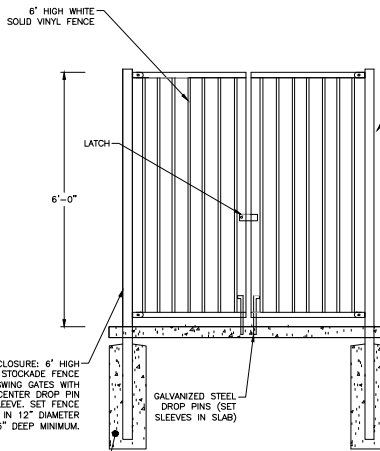


PLAN



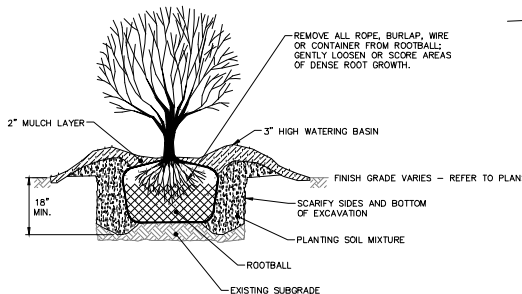
LIGHT POLE BASE NTS

NOTES:
 1) INCREASE EXPOSED HEIGHT TO 80" IN AND ADJACENT TO VEHICULAR TRAFFIC AREAS.
 2) CONTRACTOR SHALL COORDINATE WITH ELECTRICAL PLANS AND CONTRACTOR FOR BOLTS AND BOLT PATTERN FOR LIGHT POLE ANCHOR BOLTS.



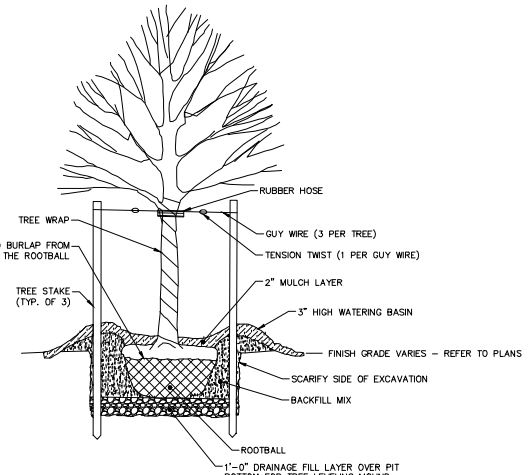
DUMPSTER ENCLOSURE NTS

LINE AND TERMINAL POSTS			
FENCE HEIGHT	"A" DIAM.	"B" DEPTH	"C" POST EMBEDMENT
6"	12"	48"	45"



SHRUB PLANTING NTS

NOTE:
 1) SET SHRUB ROOT CROWN FLUSH TO 2" HIGHER THAN FINISH GRADE.

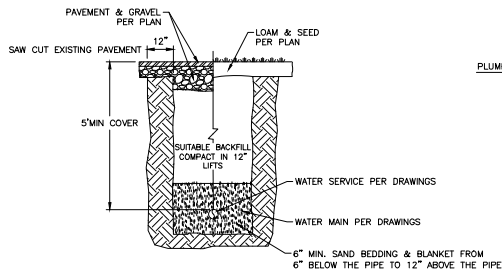


TREE PLANTING NTS

NOTES:
 1) SET TREE ROOT CROWN 2" ABOVE FINISH GRADE.
 2) DO NOT PLACE SOIL OVER ROOTBALL.

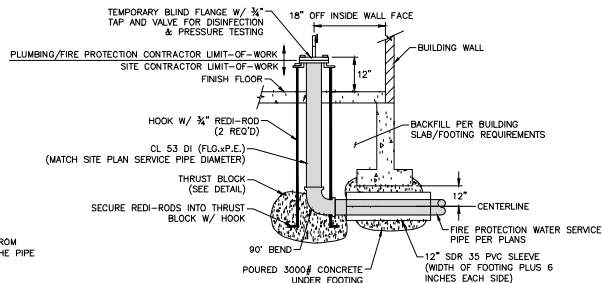
NO.	DATE	BY	DESCRIPTION
1	03/25/24	DFS	ISSUED FOR PERMITTING
2	04/11/24	DFS	REVISED PARKING CALCULATIONS



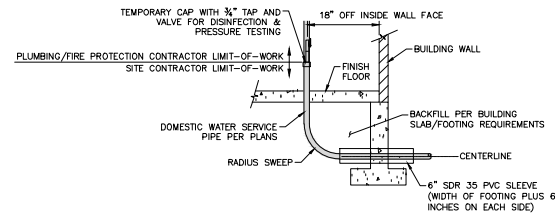


WATER SERVICE TRENCH SECTION NTS

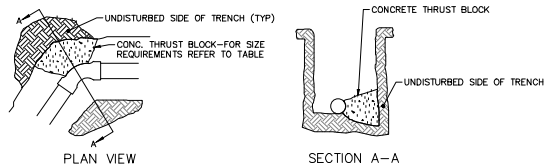
NOTE:
1) SUITABLE BACKFILL SHALL BE AN AASHTO CLASS III SOIL, EITHER TRENCH SPOILS OR AN IMPORTED SAND AND GRAVEL WITH FINES AND COMPACTED TO 90% OF THE PROCTOR DENSITY.



FIRE PROTECTION WATER SERVICE RISER NTS

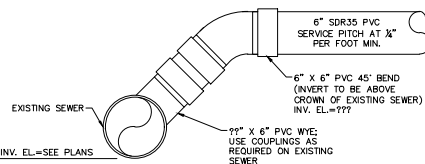


DOMESTIC WATER SERVICE RISER NTS

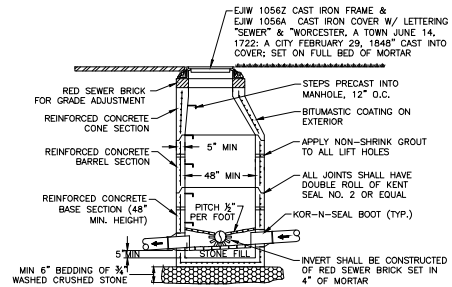


BEARING AREA AGAINST UNDISTURBED SOIL (SQ. FT.)					
PIPE SIZE	48\"/>				

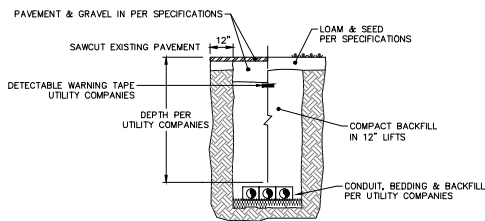
THRUST BLOCK DETAIL NTS



SEWER SERVICE CONNECTION DETAIL NTS

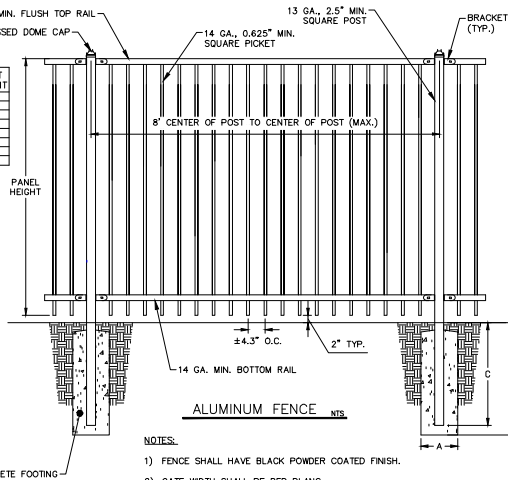


SEWER MANHOLE NTS



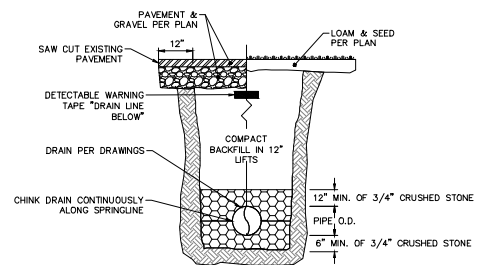
ELECTRICAL, CABLE & TELEPHONE SERVICE TRENCH SECTION NTS

LINE AND TERMINAL POSTS				
PANEL HEIGHT	TYPE POST	*A* DIAM.	*B* DEPTH	*C* POST EMBEDMENT
3'-4'	LINE	6"	26"	24"
	TERMINAL	10"	32"	30"
5'	LINE	8"	32"	30"
	TERMINAL	10"	36"	36"
6'-9"	LINE	12"	36"	36"
	TERMINAL	14"	36"	36"
10'-12'	LINE	18"	36"	36"
	TERMINAL	18"	36"	36"



NOTES:

- FENCE SHALL HAVE BLACK POWDER COATED FINISH.
- GATE WIDTH SHALL BE PER PLANS.
- GATE POSTS SHALL BE 8 GA., 4.0\"/>



HDPE/PVC DRAIN TRENCH SECTION NTS

NOTES:

- BACKFILL SHALL BE AN AASHTO CLASS III SOIL; EITHER TRENCH SPOILS OR AN IMPORTED SAND AND GRAVEL WITH FINES AND COMPACTED TO 90% OF THE PROCTOR DENSITY.
- BACKFILL OF HDPE PIPE SHALL CONFORM TO ASTM D2321 AND/OR MANUFACTURER'S SPECIFICATIONS.

NO.	DATE	BY	DESCRIPTION
2	04/11/24	DFS	REVISED PARKING CALCULATIONS
1	03/25/24	DFS	ISSUED FOR PERMITTING





TRAFFIC VOLUME DATA



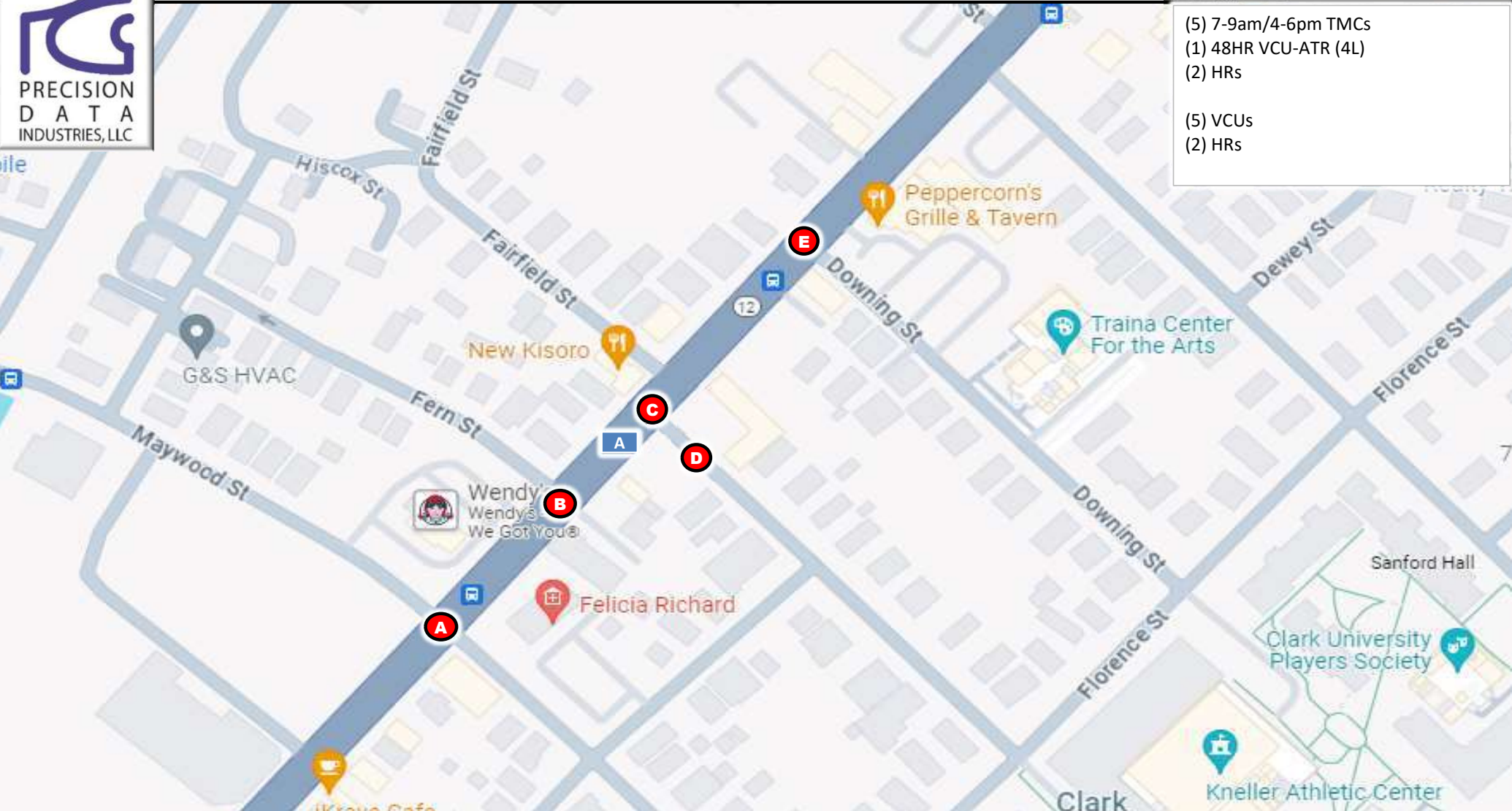


Location Map: 249915 Worcester, MA

Precision Data Industries, LLC 157 Washington Street, Suite 2, Hudson, MA 01749 ph: 508-875-0100 email: datarequests@pdillc.com

- (5) 7-9am/4-6pm TMCs
- (1) 48HR VCU-ATR (4L)
- (2) HRs

- (5) VCUs
- (2) HRs



Client: GIA	Engineer: A. Allen	Site Code: TBD	Date: Tue 3/19/24 - Wed 3/20/24	PDI Job # 249915	City, State: Worcester, MA
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Park Avenue (Route 9)
 west of Shirley Street
 City, State: Worcester, MA
 Client: GIA/A. Allen
 Site Code: TBD



PRECISION
 DATA
 INDUSTRIES, LLC
 157 Washington Street, Suite 2
 Hudson, MA 01749
 508-875-0100 datarequests@pdillc.com

PDI File #: 249915 ATR A

Count Date: Tuesday, March 19, 2024
 Direction: EB

AM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 AM	0	0	21	0	0	0	21
12:15 AM	0	0	29	0	0	0	29
12:30 AM	0	0	11	0	0	0	11
12:45 AM	0	0	23	0	1	0	24
1:00 AM	0	0	12	0	0	0	12
1:15 AM	0	0	13	0	0	0	13
1:30 AM	0	0	7	0	0	0	7
1:45 AM	0	0	8	0	0	0	8
2:00 AM	0	0	5	0	0	0	5
2:15 AM	0	0	8	0	0	1	9
2:30 AM	0	0	16	0	0	0	16
2:45 AM	0	0	19	0	0	0	19
3:00 AM	0	0	20	0	0	0	20
3:15 AM	0	0	8	0	0	0	8
3:30 AM	0	0	9	0	1	0	10
3:45 AM	0	0	16	0	0	0	16
4:00 AM	1	0	18	0	0	0	19
4:15 AM	0	0	15	0	1	1	17
4:30 AM	0	0	17	0	0	0	17
4:45 AM	0	0	30	0	1	0	31
5:00 AM	0	0	38	0	2	0	40
5:15 AM	0	0	34	0	1	0	35
5:30 AM	0	0	52	0	1	2	55
5:45 AM	0	0	61	0	0	1	62
6:00 AM	0	0	54	1	1	1	57
6:15 AM	0	0	85	1	0	1	87
6:30 AM	0	0	141	3	0	0	144
6:45 AM	0	0	158	6	3	0	167
7:00 AM	0	0	137	2	2	0	141
7:15 AM	0	0	189	5	6	0	200
7:30 AM	0	0	223	3	4	0	230
7:45 AM	0	1	216	2	2	1	222
8:00 AM	0	0	224	3	0	0	227
8:15 AM	0	0	249	3	4	0	256
8:30 AM	0	0	185	5	3	0	193
8:45 AM	0	0	162	0	1	1	164
9:00 AM	0	0	160	4	0	1	165
9:15 AM	0	0	160	8	3	1	172
9:30 AM	0	0	155	1	2	0	158
9:45 AM	0	0	147	0	3	1	151
10:00 AM	0	0	145	1	5	1	152
10:15 AM	0	0	125	0	4	0	129
10:30 AM	0	0	148	2	6	1	157
10:45 AM	0	0	153	0	3	0	156
11:00 AM	0	0	119	1	3	1	124
11:15 AM	0	0	153	0	5	1	159
11:30 AM	0	0	162	0	1	1	164
11:45 AM	0	1	157	2	4	1	165

PM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 PM	0	0	162	1	6	0	169
12:15 PM	0	0	172	2	2	1	177
12:30 PM	0	0	177	1	2	2	182
12:45 PM	0	0	162	0	4	0	166
1:00 PM	0	0	153	3	6	1	163
1:15 PM	0	0	148	2	1	1	152
1:30 PM	0	0	190	0	3	0	193
1:45 PM	0	0	184	1	1	0	186
2:00 PM	0	0	183	5	1	0	189
2:15 PM	0	0	157	3	1	1	162
2:30 PM	0	0	188	3	2	2	195
2:45 PM	0	1	155	1	0	1	158
3:00 PM	0	0	146	4	3	2	155
3:15 PM	0	0	155	2	2	0	159
3:30 PM	0	0	172	1	3	0	176
3:45 PM	0	0	154	2	0	1	157
4:00 PM	0	0	165	1	1	0	167
4:15 PM	1	0	174	1	0	1	177
4:30 PM	0	1	152	4	0	0	157
4:45 PM	0	0	184	1	2	1	188
5:00 PM	0	0	165	1	1	1	168
5:15 PM	0	1	139	1	2	0	143
5:30 PM	0	0	179	2	2	1	184
5:45 PM	0	0	158	1	0	0	159
6:00 PM	0	0	150	1	0	0	151
6:15 PM	0	1	125	0	1	0	127
6:30 PM	0	0	151	1	1	0	153
6:45 PM	0	0	153	1	1	0	155
7:00 PM	0	0	127	0	1	0	128
7:15 PM	0	0	89	1	1	0	91
7:30 PM	0	0	114	1	0	0	115
7:45 PM	0	0	122	2	0	0	124
8:00 PM	0	0	101	0	0	1	102
8:15 PM	0	0	129	1	1	0	131
8:30 PM	0	0	119	1	0	0	120
8:45 PM	0	0	77	2	0	0	79
9:00 PM	0	1	109	0	1	0	111
9:15 PM	0	0	89	0	0	0	89
9:30 PM	0	0	80	0	0	0	80
9:45 PM	0	0	75	0	0	0	75
10:00 PM	0	0	67	0	1	1	69
10:15 PM	0	0	67	0	0	0	67
10:30 PM	0	0	46	0	0	1	47
10:45 PM	0	0	38	0	0	0	38
11:00 PM	0	0	31	0	0	0	31
11:15 PM	0	0	35	0	0	0	35
11:30 PM	0	0	29	0	0	0	29
11:45 PM	0	0	35	0	0	0	35

AM Total	1	2	4297	53	73	18	4444
Percentage	0.02%	0.05%	96.69%	1.19%	1.64%	0.41%	
AM Peak	3:15 AM	7:00 AM	7:30 AM	8:30 AM	9:45 AM	5:30 AM	7:30 AM
Volume	1	1	912	17	18	5	935

PM Total	1	5	6132	54	53	19	6264
Percentage	0.02%	0.08%	97.89%	0.86%	0.85%	0.30%	
PM Peak	3:30 PM	4:30 PM	1:30 PM	1:45 PM	12:00 PM	2:15 PM	1:45 PM
Volume	1	2	714	12	14	6	732

Day Total	2	7	10429	107	126	37	10708
Percentage	0.02%	0.07%	97.39%	1.00%	1.18%	0.35%	

Park Avenue (Route 9)
 west of Shirley Street
 City, State: Worcester, MA
 Client: GIA/A. Allen
 Site Code: TBD



PRECISION
 DATA
 INDUSTRIES, LLC

157 Washington Street, Suite 2
 Hudson, MA 01749
 508-875-0100 datarequests@pdillc.com

PDI File #: 249915 ATR A

Count Date: **Wednesday, March 20, 2024**
 Direction: **EB**

AM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 AM	0	0	22	0	0	0	22
12:15 AM	0	0	24	1	1	0	26
12:30 AM	0	0	12	1	0	0	13
12:45 AM	0	0	26	0	0	0	26
1:00 AM	0	0	13	0	0	0	13
1:15 AM	0	0	9	0	0	0	9
1:30 AM	0	0	8	0	0	0	8
1:45 AM	0	0	10	0	0	1	11
2:00 AM	0	0	6	0	0	0	6
2:15 AM	0	0	13	0	0	0	13
2:30 AM	0	0	8	0	0	0	8
2:45 AM	0	0	23	0	0	0	23
3:00 AM	1	0	13	0	0	0	14
3:15 AM	0	0	10	0	0	0	10
3:30 AM	0	0	6	0	0	1	7
3:45 AM	0	0	16	0	0	0	16
4:00 AM	0	0	22	0	0	0	22
4:15 AM	0	0	9	0	1	0	10
4:30 AM	0	0	25	0	2	0	27
4:45 AM	0	0	27	0	0	1	28
5:00 AM	0	0	40	0	0	1	41
5:15 AM	0	0	40	1	0	0	41
5:30 AM	0	0	69	1	1	0	71
5:45 AM	0	0	55	1	0	0	56
6:00 AM	0	0	70	0	0	0	70
6:15 AM	0	0	89	0	0	0	89
6:30 AM	0	0	119	5	0	0	124
6:45 AM	0	0	156	4	1	1	162
7:00 AM	0	0	167	4	2	0	173
7:15 AM	0	0	185	3	2	1	191
7:30 AM	0	0	216	3	0	0	219
7:45 AM	0	0	258	2	2	1	263
8:00 AM	0	0	219	4	1	2	226
8:15 AM	0	0	237	2	4	1	244
8:30 AM	0	0	183	4	0	1	188
8:45 AM	0	0	199	2	7	0	208
9:00 AM	0	0	159	6	3	0	168
9:15 AM	0	0	138	5	7	1	151
9:30 AM	0	0	148	2	2	1	153
9:45 AM	0	0	156	0	8	0	164
10:00 AM	0	0	142	2	0	1	145
10:15 AM	0	0	141	2	6	0	149
10:30 AM	0	0	136	1	2	3	142
10:45 AM	0	0	136	1	1	0	138
11:00 AM	0	0	139	1	3	2	145
11:15 AM	0	0	154	2	4	0	160
11:30 AM	0	0	166	1	1	0	168
11:45 AM	0	0	140	3	3	1	147

PM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 PM	0	0	158	2	2	0	162
12:15 PM	0	0	174	1	5	0	180
12:30 PM	0	0	172	3	1	0	176
12:45 PM	0	0	166	0	0	0	166
1:00 PM	0	0	170	2	3	0	175
1:15 PM	0	0	180	0	1	0	181
1:30 PM	0	1	180	1	2	0	184
1:45 PM	0	0	168	1	3	0	172
2:00 PM	1	0	156	5	1	1	164
2:15 PM	0	0	168	3	0	1	172
2:30 PM	1	0	191	1	2	0	195
2:45 PM	0	0	161	1	2	1	165
3:00 PM	0	1	152	2	1	0	156
3:15 PM	0	0	161	4	3	0	168
3:30 PM	0	0	188	2	1	0	191
3:45 PM	0	0	157	2	3	1	163
4:00 PM	0	0	157	1	1	0	159
4:15 PM	0	0	145	1	2	0	148
4:30 PM	0	0	140	1	1	0	142
4:45 PM	0	0	144	0	3	0	147
5:00 PM	0	0	166	1	1	0	168
5:15 PM	0	0	156	2	0	0	158
5:30 PM	0	0	154	2	0	0	156
5:45 PM	0	0	144	0	1	0	145
6:00 PM	0	0	124	1	2	0	127
6:15 PM	0	0	131	1	0	1	133
6:30 PM	0	0	130	2	1	1	134
6:45 PM	0	0	150	1	0	0	151
7:00 PM	0	0	114	1	1	0	116
7:15 PM	0	0	94	1	0	0	95
7:30 PM	0	0	111	1	0	0	112
7:45 PM	0	0	95	0	0	0	95
8:00 PM	0	0	101	0	1	1	103
8:15 PM	0	0	105	0	0	1	106
8:30 PM	0	0	100	1	0	0	101
8:45 PM	0	0	93	0	0	0	93
9:00 PM	0	0	87	0	0	0	87
9:15 PM	0	0	87	0	0	0	87
9:30 PM	0	0	67	0	0	0	67
9:45 PM	1	0	68	0	0	0	69
10:00 PM	0	0	70	0	2	1	73
10:15 PM	0	0	67	0	1	0	68
10:30 PM	0	0	58	0	1	0	59
10:45 PM	0	0	46	0	0	0	46
11:00 PM	0	0	47	0	0	0	47
11:15 PM	0	0	40	0	0	0	40
11:30 PM	0	0	36	0	0	0	36
11:45 PM	0	0	42	0	0	0	42

AM Total	1	0	4359	64	64	20	4508
Percentage	0.02%	0.00%	96.69%	1.42%	1.42%	0.44%	
AM Peak	2:15 AM	12:00 AM	7:30 AM	8:30 AM	9:00 AM	7:45 AM	7:30 AM
Volume	1	0	930	17	20	5	952

PM Total	3	2	5971	47	48	9	6080
Percentage	0.05%	0.03%	98.21%	0.77%	0.79%	0.15%	
PM Peak	1:45 PM	12:45 PM	1:00 PM	1:30 PM	12:15 PM	2:00 PM	1:00 PM
Volume	2	1	698	10	9	3	712

Day Total	4	2	10330	111	112	29	10588
Percentage	0.04%	0.02%	97.56%	1.05%	1.06%	0.27%	

Park Avenue (Route 9)
west of Shirley Street
City, State: Worcester, MA
Client: GIA/A. Allen
Site Code: TBD



PRECISION
D A T A
INDUSTRIES, LLC

157 Washington Street, Suite 2
Hudson, MA 01749
508-875-0100 datarequests@pdillc.com

PDI File #: 249915 ATR A

Count Date: Tuesday, March 19, 2024
Direction: WB

AM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 AM	0	0	26	0	1	0	27
12:15 AM	0	0	26	0	0	0	26
12:30 AM	0	0	24	0	1	0	25
12:45 AM	0	0	12	0	1	0	13
1:00 AM	0	0	11	0	0	1	12
1:15 AM	0	0	7	0	0	0	7
1:30 AM	0	0	4	1	0	0	5
1:45 AM	0	0	18	0	0	0	18
2:00 AM	0	0	6	0	0	0	6
2:15 AM	0	0	11	0	0	0	11
2:30 AM	0	0	10	0	0	0	10
2:45 AM	0	0	7	0	0	0	7
3:00 AM	0	0	12	0	0	0	12
3:15 AM	0	0	2	0	0	1	3
3:30 AM	0	0	6	0	0	0	6
3:45 AM	0	0	4	0	0	0	4
4:00 AM	0	0	12	0	1	1	14
4:15 AM	0	0	5	0	0	0	5
4:30 AM	0	0	18	0	0	0	18
4:45 AM	0	0	11	0	1	0	12
5:00 AM	0	0	30	0	1	2	33
5:15 AM	0	0	22	0	0	0	22
5:30 AM	0	0	28	0	3	1	32
5:45 AM	0	0	35	1	0	0	36
6:00 AM	0	0	37	1	0	0	38
6:15 AM	0	0	43	4	0	1	48
6:30 AM	0	0	33	3	1	0	37
6:45 AM	0	0	67	2	2	1	72
7:00 AM	0	0	93	3	2	0	98
7:15 AM	0	0	97	1	1	0	99
7:30 AM	0	0	88	6	4	0	98
7:45 AM	0	0	121	1	6	0	128
8:00 AM	0	0	92	2	0	2	96
8:15 AM	0	0	110	1	0	1	112
8:30 AM	0	0	117	2	4	1	124
8:45 AM	0	0	133	2	3	0	138
9:00 AM	0	0	116	2	6	1	125
9:15 AM	0	0	130	0	3	3	136
9:30 AM	0	0	113	1	5	2	121
9:45 AM	0	0	99	3	5	0	107
10:00 AM	0	0	95	0	3	1	99
10:15 AM	0	0	93	0	0	0	93
10:30 AM	0	0	124	1	4	1	130
10:45 AM	0	0	131	0	3	1	135
11:00 AM	0	0	119	2	2	2	125
11:15 AM	0	0	126	1	1	0	128
11:30 AM	0	2	168	1	3	2	176
11:45 AM	0	0	158	1	3	4	166

PM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 PM	0	0	182	0	5	0	187
12:15 PM	0	0	164	1	5	1	171
12:30 PM	0	0	160	0	7	0	167
12:45 PM	0	1	155	0	1	0	157
1:00 PM	0	1	175	2	1	0	179
1:15 PM	0	0	168	1	6	1	176
1:30 PM	0	0	155	2	1	0	158
1:45 PM	1	0	176	2	4	2	185
2:00 PM	0	0	175	5	4	0	184
2:15 PM	0	0	191	3	1	1	196
2:30 PM	0	0	192	2	5	0	199
2:45 PM	0	0	195	3	1	1	200
3:00 PM	1	0	173	2	2	0	178
3:15 PM	0	0	192	1	1	0	194
3:30 PM	0	0	203	3	2	0	208
3:45 PM	1	0	192	4	1	0	198
4:00 PM	0	0	224	2	0	1	227
4:15 PM	0	0	209	3	0	0	212
4:30 PM	0	0	210	1	1	0	212
4:45 PM	0	0	199	0	1	1	201
5:00 PM	0	0	248	1	2	0	251
5:15 PM	1	2	219	1	5	1	229
5:30 PM	0	0	187	1	3	0	191
5:45 PM	0	0	187	0	0	0	187
6:00 PM	0	0	220	1	1	0	222
6:15 PM	0	0	193	1	1	0	195
6:30 PM	0	0	190	0	2	0	192
6:45 PM	1	1	138	0	3	1	144
7:00 PM	0	0	148	2	1	0	151
7:15 PM	0	0	135	1	0	0	136
7:30 PM	0	0	138	0	1	0	139
7:45 PM	0	0	128	2	0	0	130
8:00 PM	0	0	101	0	0	0	101
8:15 PM	0	0	146	1	0	0	147
8:30 PM	0	0	110	1	0	0	111
8:45 PM	0	1	109	0	0	0	110
9:00 PM	0	0	99	0	0	0	99
9:15 PM	0	0	86	0	1	0	87
9:30 PM	0	0	86	0	0	0	86
9:45 PM	0	0	67	0	0	0	67
10:00 PM	0	0	72	0	0	0	72
10:15 PM	0	0	55	0	1	0	56
10:30 PM	0	1	52	0	0	0	53
10:45 PM	0	0	63	0	0	0	63
11:00 PM	0	0	47	0	0	0	47
11:15 PM	0	0	48	0	0	0	48
11:30 PM	0	0	44	0	0	0	44
11:45 PM	0	0	26	0	0	0	26

AM Total	0	2	2850	42	70	29	2993
Percentage	0.00%	0.07%	95.22%	1.40%	2.34%	0.97%	
AM Peak	12:00 AM	10:45 AM	11:00 AM	6:15 AM	9:00 AM	11:00 AM	11:00 AM
Volume	0	2	571	12	19	8	595

PM Total	5	7	7032	49	70	10	7173
Percentage	0.07%	0.10%	98.03%	0.68%	0.98%	0.14%	
PM Peak	3:00 PM	12:15 PM	4:30 PM	2:00 PM	12:00 PM	1:00 PM	4:30 PM
Volume	2	2	876	13	18	3	893

Day Total	5	9	9882	91	140	39	10166
Percentage	0.05%	0.09%	97.21%	0.90%	1.38%	0.38%	

Park Avenue (Route 9)
west of Shirley Street
City, State: Worcester, MA
Client: GIA/A. Allen
Site Code: TBD



PRECISION
D A T A
INDUSTRIES, LLC

157 Washington Street, Suite 2
Hudson, MA 01749
508-875-0100 datarequests@pdillc.com

PDI File #: 249915 ATR A

Count Date: **Wednesday, March 20, 2024**
Direction: **WB**

AM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 AM	0	0	32	1	1	1	35
12:15 AM	0	0	24	2	0	0	26
12:30 AM	0	0	18	0	0	0	18
12:45 AM	0	0	17	0	0	0	17
1:00 AM	0	0	16	0	0	0	16
1:15 AM	0	0	7	0	1	0	8
1:30 AM	0	0	6	0	0	0	6
1:45 AM	0	0	12	0	0	0	12
2:00 AM	0	0	14	0	0	0	14
2:15 AM	0	0	4	0	0	0	4
2:30 AM	0	0	8	0	0	1	9
2:45 AM	0	0	14	0	0	0	14
3:00 AM	0	0	2	0	0	0	2
3:15 AM	0	0	4	0	0	0	4
3:30 AM	0	0	7	0	1	0	8
3:45 AM	0	0	11	0	0	0	11
4:00 AM	0	0	11	0	0	0	11
4:15 AM	0	0	7	0	2	1	10
4:30 AM	0	0	14	0	0	1	15
4:45 AM	0	0	10	0	0	1	11
5:00 AM	0	0	23	0	0	0	23
5:15 AM	0	0	19	0	0	1	20
5:30 AM	0	0	21	0	0	0	21
5:45 AM	0	0	33	2	0	0	35
6:00 AM	0	0	28	0	1	0	29
6:15 AM	0	0	41	4	1	1	47
6:30 AM	0	0	47	3	1	0	51
6:45 AM	0	0	72	3	0	0	75
7:00 AM	0	0	84	4	2	0	90
7:15 AM	0	0	88	2	5	4	99
7:30 AM	0	0	93	6	2	1	102
7:45 AM	0	0	89	3	2	0	94
8:00 AM	0	0	118	0	2	1	121
8:15 AM	0	0	117	2	0	1	120
8:30 AM	0	0	112	4	2	0	118
8:45 AM	0	0	140	2	2	0	144
9:00 AM	0	0	132	1	2	1	136
9:15 AM	0	0	111	0	6	1	118
9:30 AM	0	0	114	1	7	0	122
9:45 AM	0	0	130	1	6	1	138
10:00 AM	0	0	111	2	4	1	118
10:15 AM	0	0	121	0	2	0	123
10:30 AM	1	0	125	3	2	1	132
10:45 AM	0	0	123	3	6	0	132
11:00 AM	0	0	155	2	4	1	162
11:15 AM	0	0	161	1	3	0	165
11:30 AM	0	0	163	0	1	2	166
11:45 AM	0	0	166	1	1	1	169

PM	Bicycles	Motorcycle	Cars & Light Goods	Buses	Single Unit Heavy	Multi Unit Heavy	Total
12:00 PM	0	0	156	1	2	2	161
12:15 PM	0	0	179	3	1	0	183
12:30 PM	0	0	162	1	1	3	167
12:45 PM	0	0	157	0	0	1	158
1:00 PM	0	0	160	1	5	0	166
1:15 PM	0	0	169	4	2	0	175
1:30 PM	0	0	171	3	5	0	179
1:45 PM	0	0	173	2	0	0	175
2:00 PM	0	0	198	3	0	0	201
2:15 PM	0	1	213	4	2	1	221
2:30 PM	0	0	190	5	0	0	195
2:45 PM	1	0	215	1	3	0	220
3:00 PM	0	0	198	1	4	0	203
3:15 PM	0	0	183	0	2	0	185
3:30 PM	0	0	187	3	0	0	190
3:45 PM	0	0	186	3	2	0	191
4:00 PM	0	0	242	4	1	0	247
4:15 PM	0	0	223	2	4	1	230
4:30 PM	0	0	196	0	1	0	197
4:45 PM	0	0	230	1	1	0	232
5:00 PM	0	0	184	1	1	0	186
5:15 PM	0	0	209	1	0	1	211
5:30 PM	0	0	191	0	1	1	193
5:45 PM	0	0	195	0	0	0	195
6:00 PM	0	0	183	0	0	0	183
6:15 PM	0	0	146	2	0	0	148
6:30 PM	0	0	190	0	0	1	191
6:45 PM	0	0	140	1	0	0	141
7:00 PM	0	0	140	2	1	0	143
7:15 PM	0	0	133	1	0	0	134
7:30 PM	0	0	104	0	0	0	104
7:45 PM	0	0	109	1	1	0	111
8:00 PM	0	0	119	0	1	0	120
8:15 PM	0	0	96	2	0	0	98
8:30 PM	0	0	104	2	0	0	106
8:45 PM	0	0	109	1	1	0	111
9:00 PM	0	0	85	0	0	0	85
9:15 PM	0	0	105	0	0	0	105
9:30 PM	0	0	74	0	1	0	75
9:45 PM	0	0	56	0	0	0	56
10:00 PM	0	0	72	0	1	0	73
10:15 PM	0	0	68	0	0	1	69
10:30 PM	0	0	40	0	0	1	41
10:45 PM	0	0	59	0	0	0	59
11:00 PM	0	0	44	0	0	0	44
11:15 PM	0	0	58	0	0	1	59
11:30 PM	0	0	30	0	0	0	30
11:45 PM	0	0	25	0	0	0	25

AM Total	1	0	2975	53	69	23	3121
Percentage	0.03%	0.00%	95.32%	1.70%	2.21%	0.74%	
AM Peak	9:45 AM	12:00 AM	11:00 AM	6:45 AM	9:15 AM	7:15 AM	11:00 AM
Volume	1	0	645	15	23	6	662

PM Total	1	1	6856	56	44	14	6972
Percentage	0.01%	0.01%	98.34%	0.80%	0.63%	0.20%	
PM Peak	2:00 PM	1:30 PM	4:00 PM	1:45 PM	12:45 PM	12:00 PM	4:00 PM
Volume	1	1	891	14	12	6	906

Day Total	2	1	9831	109	113	37	10093
Percentage	0.02%	0.01%	97.40%	1.08%	1.12%	0.37%	

Park Avenue (Route 9)
west of Shirley Street
City, State: Worcester, MA
Client: GIA/A. Allen
Site Code: TBD



PRECISION
D A T A
INDUSTRIES, LLC
157 Washington Street, Suite 2
Hudson, MA 01749
508-875-0100 datarequests@pdillc.com

PDI File # 249915 ATR A

Direction: EB

Weekly Report

Day Date	Tuesday 03/19/24		Wednesday 03/20/24												Week Ave			
	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM		
12:00	21	169	22	162	0	0	0	0	0	0	0	0	0	0	22	166		
12:15	29	177	26	180	0	0	0	0	0	0	0	0	0	0	28	179		
12:30	11	182	13	176	0	0	0	0	0	0	0	0	0	0	12	179		
12:45	24	166	26	166	0	0	0	0	0	0	0	0	0	0	25	166		
1:00	12	163	13	175	0	0	0	0	0	0	0	0	0	0	13	169		
1:15	13	152	9	181	0	0	0	0	0	0	0	0	0	0	11	167		
1:30	7	193	8	184	0	0	0	0	0	0	0	0	0	0	8	189		
1:45	8	186	11	172	0	0	0	0	0	0	0	0	0	0	10	179		
2:00	5	189	6	164	0	0	0	0	0	0	0	0	0	0	6	177		
2:15	9	162	13	172	0	0	0	0	0	0	0	0	0	0	11	167		
2:30	16	195	8	195	0	0	0	0	0	0	0	0	0	0	12	195		
2:45	19	158	23	165	0	0	0	0	0	0	0	0	0	0	21	162		
3:00	20	155	14	156	0	0	0	0	0	0	0	0	0	0	17	156		
3:15	8	159	10	168	0	0	0	0	0	0	0	0	0	0	9	164		
3:30	10	176	7	191	0	0	0	0	0	0	0	0	0	0	9	184		
3:45	16	157	16	163	0	0	0	0	0	0	0	0	0	0	16	160		
4:00	19	167	22	159	0	0	0	0	0	0	0	0	0	0	21	163		
4:15	17	177	10	148	0	0	0	0	0	0	0	0	0	0	14	163		
4:30	17	157	27	142	0	0	0	0	0	0	0	0	0	0	22	150		
4:45	31	188	28	147	0	0	0	0	0	0	0	0	0	0	30	168		
5:00	40	168	41	168	0	0	0	0	0	0	0	0	0	0	41	168		
5:15	35	143	41	158	0	0	0	0	0	0	0	0	0	0	38	151		
5:30	55	184	71	156	0	0	0	0	0	0	0	0	0	0	63	170		
5:45	62	159	56	145	0	0	0	0	0	0	0	0	0	0	59	152		
6:00	57	151	70	127	0	0	0	0	0	0	0	0	0	0	64	139		
6:15	87	127	89	133	0	0	0	0	0	0	0	0	0	0	88	130		
6:30	144	153	124	134	0	0	0	0	0	0	0	0	0	0	134	144		
6:45	167	155	162	151	0	0	0	0	0	0	0	0	0	0	165	153		
7:00	141	128	173	116	0	0	0	0	0	0	0	0	0	0	157	122		
7:15	200	91	191	95	0	0	0	0	0	0	0	0	0	0	196	93		
7:30	230	115	219	112	0	0	0	0	0	0	0	0	0	0	225	114		
7:45	222	124	263	95	0	0	0	0	0	0	0	0	0	0	243	110		
8:00	227	102	226	103	0	0	0	0	0	0	0	0	0	0	227	103		
8:15	256	131	244	106	0	0	0	0	0	0	0	0	0	0	250	119		
8:30	193	120	188	101	0	0	0	0	0	0	0	0	0	0	191	111		
8:45	164	79	208	93	0	0	0	0	0	0	0	0	0	0	186	86		
9:00	165	111	168	87	0	0	0	0	0	0	0	0	0	0	167	99		
9:15	172	89	151	87	0	0	0	0	0	0	0	0	0	0	162	88		
9:30	158	80	153	67	0	0	0	0	0	0	0	0	0	0	156	74		
9:45	151	75	164	69	0	0	0	0	0	0	0	0	0	0	158	72		
10:00	152	69	145	73	0	0	0	0	0	0	0	0	0	0	149	71		
10:15	129	67	149	68	0	0	0	0	0	0	0	0	0	0	139	68		
10:30	157	47	142	59	0	0	0	0	0	0	0	0	0	0	150	53		
10:45	156	38	138	46	0	0	0	0	0	0	0	0	0	0	147	42		
11:00	124	31	145	47	0	0	0	0	0	0	0	0	0	0	135	39		
11:15	159	35	160	40	0	0	0	0	0	0	0	0	0	0	160	38		
11:30	164	29	168	36	0	0	0	0	0	0	0	0	0	0	166	33		
11:45	165	35	147	42	0	0	0	0	0	0	0	0	0	0	156	39		
Total	4444	6264	4508	6080	0	0	0	0	0	0	0	0	0	0	4476	6172		
Day Total	10708		10588		0		0		0		0		0		10648			
Peak HR	7:30 AM	1:45 PM	7:30 AM	1:00 PM													7:30 AM	1:45 PM
Volume	935	732	952	712													944	718

Park Avenue (Route 9)
west of Shirley Street
City, State: Worcester, MA
Client: GIA/A. Allen
Site Code: TBD



PRECISION
D A T A
INDUSTRIES, LLC
157 Washington Street, Suite 2
Hudson, MA 01749
508-875-0100 datarequests@pdillc.com

PDI File # 249915 ATR A

Direction: WB

Weekly Report

Day Date	Tuesday 03/19/24		Wednesday 03/20/24												Week Ave	
	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM
12:00	27	187	35	161	0	0	0	0	0	0	0	0	0	0	31	174
12:15	26	171	26	183	0	0	0	0	0	0	0	0	0	0	26	177
12:30	25	167	18	167	0	0	0	0	0	0	0	0	0	0	22	167
12:45	13	157	17	158	0	0	0	0	0	0	0	0	0	0	15	158
1:00	12	179	16	166	0	0	0	0	0	0	0	0	0	0	14	173
1:15	7	176	8	175	0	0	0	0	0	0	0	0	0	0	8	176
1:30	5	158	6	179	0	0	0	0	0	0	0	0	0	0	6	169
1:45	18	185	12	175	0	0	0	0	0	0	0	0	0	0	15	180
2:00	6	184	14	201	0	0	0	0	0	0	0	0	0	0	10	193
2:15	11	196	4	221	0	0	0	0	0	0	0	0	0	0	8	209
2:30	10	199	9	195	0	0	0	0	0	0	0	0	0	0	10	197
2:45	7	200	14	220	0	0	0	0	0	0	0	0	0	0	11	210
3:00	12	178	2	203	0	0	0	0	0	0	0	0	0	0	7	191
3:15	3	194	4	185	0	0	0	0	0	0	0	0	0	0	4	190
3:30	6	208	8	190	0	0	0	0	0	0	0	0	0	0	7	199
3:45	4	198	11	191	0	0	0	0	0	0	0	0	0	0	8	195
4:00	14	227	11	247	0	0	0	0	0	0	0	0	0	0	13	237
4:15	5	212	10	230	0	0	0	0	0	0	0	0	0	0	8	221
4:30	18	212	15	197	0	0	0	0	0	0	0	0	0	0	17	205
4:45	12	201	11	232	0	0	0	0	0	0	0	0	0	0	12	217
5:00	33	251	23	186	0	0	0	0	0	0	0	0	0	0	28	219
5:15	22	229	20	211	0	0	0	0	0	0	0	0	0	0	21	220
5:30	32	191	21	193	0	0	0	0	0	0	0	0	0	0	27	192
5:45	36	187	35	195	0	0	0	0	0	0	0	0	0	0	36	191
6:00	38	222	29	183	0	0	0	0	0	0	0	0	0	0	34	203
6:15	48	195	47	148	0	0	0	0	0	0	0	0	0	0	48	172
6:30	37	192	51	191	0	0	0	0	0	0	0	0	0	0	44	192
6:45	72	144	75	141	0	0	0	0	0	0	0	0	0	0	74	143
7:00	98	151	90	143	0	0	0	0	0	0	0	0	0	0	94	147
7:15	99	136	99	134	0	0	0	0	0	0	0	0	0	0	99	135
7:30	98	139	102	104	0	0	0	0	0	0	0	0	0	0	100	122
7:45	128	130	94	111	0	0	0	0	0	0	0	0	0	0	111	121
8:00	96	101	121	120	0	0	0	0	0	0	0	0	0	0	109	111
8:15	112	147	120	98	0	0	0	0	0	0	0	0	0	0	116	123
8:30	124	111	118	106	0	0	0	0	0	0	0	0	0	0	121	109
8:45	138	110	144	111	0	0	0	0	0	0	0	0	0	0	141	111
9:00	125	99	136	85	0	0	0	0	0	0	0	0	0	0	131	92
9:15	136	87	118	105	0	0	0	0	0	0	0	0	0	0	127	96
9:30	121	86	122	75	0	0	0	0	0	0	0	0	0	0	122	81
9:45	107	67	138	56	0	0	0	0	0	0	0	0	0	0	123	62
10:00	99	72	118	73	0	0	0	0	0	0	0	0	0	0	109	73
10:15	93	56	123	69	0	0	0	0	0	0	0	0	0	0	108	63
10:30	130	53	132	41	0	0	0	0	0	0	0	0	0	0	131	47
10:45	135	63	132	59	0	0	0	0	0	0	0	0	0	0	134	61
11:00	125	47	162	44	0	0	0	0	0	0	0	0	0	0	144	46
11:15	128	48	165	59	0	0	0	0	0	0	0	0	0	0	147	54
11:30	176	44	166	30	0	0	0	0	0	0	0	0	0	0	171	37
11:45	166	26	169	25	0	0	0	0	0	0	0	0	0	0	168	26
Total	2993	7173	3121	6972	0	0	0	0	0	0	0	0	0	0	3057	7073
Day Total	10166		10093		0		0		0		0		0		10130	
Peak HR	11:00 AM	4:30 PM	11:00 AM	4:00 PM											11:00 AM	4:00 PM
Volume	595	893	662	906											629	879

Park Avenue (Route 9)
west of Shirley Street
City, State: Worcester, MA
Client: GIA/A. Allen
Site Code: TBD



PDI File #: 249915 ATR A (speed)

Count Date
Tuesday, March 19, 2024

Speed (60-minute)

EB																
Start Time:	1 to 14	15 to 19	20 to 24	25 to 29	30 to 34	35 to 39	40 to 44	45 to 49	50 to 54	55 to 59	60 to 64	65 to 69	70+	Total	85th %ile	Ave Speed
12:00 AM	0	0	5	23	42	13	4	2	0	0	0	0	0	89	36.0	31.4
1:00 AM	0	2	3	12	12	12	1	0	0	0	0	0	0	42	37.9	31.0
2:00 AM	0	0	1	10	11	17	8	2	1	0	0	0	0	50	40.0	35.0
3:00 AM	1	2	4	12	16	13	5	2	1	0	0	0	0	56	39.0	32.1
4:00 AM	0	0	2	8	18	31	16	8	0	0	0	0	0	83	42.0	36.3
5:00 AM	0	3	5	28	63	66	27	5	1	0	0	0	1	199	41.0	34.6
6:00 AM	0	5	8	69	166	96	47	9	4	1	0	0	0	405	39.4	33.7
7:00 AM	0	3	39	143	286	158	43	9	1	0	0	0	0	682	37.0	32.3
8:00 AM	0	5	34	154	310	137	43	5	2	0	0	0	0	690	37.0	32.1
9:00 AM	1	6	43	172	243	92	22	5	1	1	0	0	0	586	35.3	30.9
10:00 AM	1	4	33	152	233	91	27	8	1	0	0	0	0	550	36.0	31.5
11:00 AM	2	3	37	149	222	98	29	4	1	0	0	0	0	545	36.0	31.3
12:00 PM	0	6	30	162	287	129	20	6	3	2	0	0	0	645	36.0	31.8
1:00 PM	4	5	62	206	242	99	16	4	0	0	0	0	0	638	35.0	30.3
2:00 PM	6	12	53	198	226	110	29	4	0	1	0	0	0	639	36.0	30.6
3:00 PM	5	7	33	168	227	103	25	5	1	0	0	0	0	574	36.0	31.0
4:00 PM	6	7	40	162	238	98	29	5	0	0	0	0	0	585	36.0	31.0
5:00 PM	1	9	56	149	263	89	26	2	1	1	0	0	0	597	36.0	30.8
6:00 PM	0	5	45	147	208	102	27	6	1	0	0	0	0	541	36.0	31.3
7:00 PM	2	2	30	139	152	70	21	3	0	0	0	0	0	419	36.0	30.8
8:00 PM	2	3	38	115	150	54	17	2	2	0	0	0	0	383	35.0	30.6
9:00 PM	3	6	18	97	141	62	12	3	0	2	2	0	0	346	36.0	31.2
10:00 PM	2	4	12	60	90	39	12	4	0	0	0	0	0	223	36.0	31.2
11:00 PM	0	1	9	26	55	32	4	1	1	0	0	0	0	129	36.0	32.2
Total	36	100	640	2561	3901	1811	510	104	22	8	2	0	1	9696	36.0	31.5
Percent	0.37%	1.03%	6.60%	26.41%	40.23%	18.68%	5.26%	1.07%	0.23%	0.08%	0.02%	0.00%	0.01%			

AM Peak	11:00 AM	9:00 AM	9:00 AM	9:00 AM	8:00 AM	7:00 AM	6:00 AM	6:00 AM	6:00 AM	6:00 AM				5:00 AM	8:00 AM
Volume	2	6	43	172	310	158	47	9	4	1	0	0		1	690

PM Peak	2:00 PM	2:00 PM	1:00 PM	1:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	9:00 PM				12:00 PM
Volume	6	12	62	206	287	129	29	6	3	2	2	0	0		645

15th Percentile:	26.0 MPH	Average Speed:	31.5 MPH	Posted Speed Limit:	35 MPH
50th Percentile:	31.0 MPH	10 MPH Pace:	27 to 36 MPH	Number of Vehicles > 35 MPH:	1917
85th Percentile:	36.0 MPH	Number in Pace:	6811	Percent of Vehicles > 35 MPH:	19.8%
95th Percentile:	41.0 MPH	Percent in Pace:	70.2%		

Park Avenue (Route 9)
west of Shirley Street
City, State: Worcester, MA
Client: GIA/A. Allen
Site Code: TBD



PDI File #: 249915 ATR A (speed)

Count Date
Tuesday, March 19, 2024

Speed (60-minute)

WB																
Start Time:	1 to 14	15 to 19	20 to 24	25 to 29	30 to 34	35 to 39	40 to 44	45 to 49	50 to 54	55 to 59	60 to 64	65 to 69	70+	Total	85th %ile	Ave Speed
12:00 AM	1	1	12	35	30	10	2	1	0	0	0	0	0	92	34.0	29.3
1:00 AM	0	3	5	12	16	3	1	0	0	0	0	0	0	40	33.0	28.6
2:00 AM	1	0	10	5	9	9	2	0	0	0	0	0	0	36	35.8	29.9
3:00 AM	0	0	0	8	8	5	2	0	0	0	0	0	0	23	38.0	32.3
4:00 AM	0	0	3	12	17	13	3	0	0	0	0	0	0	48	37.0	32.1
5:00 AM	0	1	6	27	52	23	2	5	1	0	0	0	1	118	37.0	32.3
6:00 AM	1	5	15	60	61	37	7	3	2	0	0	0	0	191	36.0	30.8
7:00 AM	2	5	58	127	114	64	14	3	2	0	0	0	0	389	36.8	30.0
8:00 AM	1	11	44	133	127	60	13	3	1	0	1	0	0	394	36.0	30.1
9:00 AM	3	14	64	127	145	50	10	3	2	0	0	0	0	418	35.0	29.4
10:00 AM	1	16	70	132	140	48	11	1	0	0	1	0	0	420	34.0	29.1
11:00 AM	4	24	107	124	134	46	5	3	2	0	1	0	0	450	34.0	28.2
12:00 PM	7	32	120	164	110	47	5	0	1	0	0	0	0	486	33.0	27.2
1:00 PM	8	27	112	175	106	54	10	3	0	0	0	0	0	495	34.0	27.6
2:00 PM	16	49	113	152	118	30	1	2	1	0	0	0	0	482	32.0	26.3
3:00 PM	16	41	135	151	125	34	2	2	0	0	0	0	0	506	33.0	26.5
4:00 PM	10	41	134	170	112	40	7	2	0	0	0	0	0	516	33.0	26.7
5:00 PM	6	51	147	198	122	43	7	0	0	0	0	0	0	574	33.0	26.7
6:00 PM	12	35	128	148	97	33	2	5	0	1	0	0	0	461	32.0	26.6
7:00 PM	2	29	116	159	100	29	4	0	0	0	0	0	0	439	32.0	26.8
8:00 PM	2	13	88	139	112	29	4	0	0	0	0	0	0	387	33.0	27.9
9:00 PM	0	12	52	121	74	35	9	1	0	0	0	0	0	304	34.0	28.6
10:00 PM	1	8	22	91	73	26	4	0	2	1	0	0	0	228	34.0	29.4
11:00 PM	0	5	29	43	53	22	7	0	0	0	0	0	0	159	35.0	29.5
Total	94	423	1590	2513	2055	790	134	37	14	2	3	0	1	7656	34.0	28.0
Percent	1.23%	5.53%	20.77%	32.82%	26.84%	10.32%	1.75%	0.48%	0.18%	0.03%	0.04%	0.00%	0.01%			

AM Peak	11:00 AM	11:00 AM	11:00 AM	8:00 AM	9:00 AM	7:00 AM	7:00 AM	5:00 AM	6:00 AM		8:00 AM		5:00 AM	11:00 AM
Volume	4	24	107	133	145	64	14	5	2	0	1	0	1	450

PM Peak	2:00 PM	5:00 PM	5:00 PM	5:00 PM	3:00 PM	1:00 PM	1:00 PM	6:00 PM	10:00 PM	6:00 PM				5:00 PM
Volume	16	51	147	198	125	54	10	5	2	1	0	0	0	574

15th Percentile:	22.0 MPH	Average Speed:	28.0 MPH	Posted Speed Limit:	35 MPH
50th Percentile:	28.0 MPH	10 MPH Pace:	24 to 33 MPH	Number of Vehicles > 35 MPH:	734
85th Percentile:	34.0 MPH	Number in Pace:	4804	Percent of Vehicles > 35 MPH:	9.6%
95th Percentile:	37.0 MPH	Percent in Pace:	62.7%		

Park Avenue (Route 9)
west of Shirley Street
City, State: Worcester, MA
Client: GIA/A. Allen
Site Code: TBD



PDI File #: 249915 ATR A (speed)

Count Date
Tuesday, March 19, 2024

Speed (60-minute)

Combined EB and WB

Start Time:	1 to 14	15 to 19	20 to 24	25 to 29	30 to 34	35 to 39	40 to 44	45 to 49	50 to 54	55 to 59	60 to 64	65 to 69	70+	Total	85th %ile	Ave Speed
12:00 AM	1	1	17	58	72	23	6	3	0	0	0	0	0	181	35.0	30.3
1:00 AM	0	5	8	24	28	15	2	0	0	0	0	0	0	82	37.0	29.8
2:00 AM	1	0	11	15	20	26	10	2	1	0	0	0	0	86	39.3	32.9
3:00 AM	1	2	4	20	24	18	7	2	1	0	0	0	0	79	38.3	32.2
4:00 AM	0	0	5	20	35	44	19	8	0	0	0	0	0	131	40.5	34.8
5:00 AM	0	4	11	55	115	89	29	10	2	0	0	0	2	317	39.0	33.7
6:00 AM	1	10	23	129	227	133	54	12	6	1	0	0	0	596	38.0	32.7
7:00 AM	2	8	97	270	400	222	57	12	3	0	0	0	0	1071	37.0	31.5
8:00 AM	1	16	78	287	437	197	56	8	3	0	1	0	0	1084	36.0	31.4
9:00 AM	4	20	107	299	388	142	32	8	3	1	0	0	0	1004	35.0	30.3
10:00 AM	2	20	103	284	373	139	38	9	1	0	1	0	0	970	36.0	30.5
11:00 AM	6	27	144	273	356	144	34	7	3	0	1	0	0	995	35.0	29.9
12:00 PM	7	38	150	326	397	176	25	6	4	2	0	0	0	1131	35.0	29.8
1:00 PM	12	32	174	381	348	153	26	7	0	0	0	0	0	1133	35.0	29.1
2:00 PM	22	61	166	350	344	140	30	6	1	1	0	0	0	1121	35.0	28.8
3:00 PM	21	48	168	319	352	137	27	7	1	0	0	0	0	1080	35.0	28.9
4:00 PM	16	48	174	332	350	138	36	7	0	0	0	0	0	1101	35.0	29.0
5:00 PM	7	60	203	347	385	132	33	2	1	1	0	0	0	1171	34.0	28.8
6:00 PM	12	40	173	295	305	135	29	11	1	1	0	0	0	1002	35.0	29.2
7:00 PM	4	31	146	298	252	99	25	3	0	0	0	0	0	858	34.0	28.8
8:00 PM	4	16	126	254	262	83	21	2	2	0	0	0	0	770	34.0	29.2
9:00 PM	3	18	70	218	215	97	21	4	0	2	2	0	0	650	35.0	30.0
10:00 PM	3	12	34	151	163	65	16	4	2	1	0	0	0	451	35.0	30.3
11:00 PM	0	6	38	69	108	54	11	1	1	0	0	0	0	288	36.0	30.7
Total	130	523	2230	5074	5956	2601	644	141	36	10	5	0	2	17352	36.0	29.9
Percent	0.75%	3.01%	12.85%	29.24%	34.32%	14.99%	3.71%	0.81%	0.21%	0.06%	0.03%	0.00%	0.01%			

AM Peak	11:00 AM	11:00 AM	11:00 AM	9:00 AM	8:00 AM	7:00 AM	7:00 AM	6:00 AM	6:00 AM	6:00 AM	8:00 AM		5:00 AM	8:00 AM
Volume	6	27	144	299	437	222	57	12	6	1	1	0	2	1084

PM Peak	2:00 PM	2:00 PM	5:00 PM	1:00 PM	12:00 PM	12:00 PM	4:00 PM	6:00 PM	12:00 PM	12:00 PM	9:00 PM		5:00 PM	
Volume	22	61	203	381	397	176	36	11	4	2	2	0	0	1171

15th Percentile:	24.0 MPH	Average Speed:	29.9 MPH	Posted Speed Limit:	35 MPH
50th Percentile:	30.0 MPH	10 MPH Pace:	26 to 35 MPH	Number of Vehicles > 35 MPH:	2651
85th Percentile:	36.0 MPH	Number in Pace:	11064	Percent of Vehicles > 35 MPH:	15.3%
95th Percentile:	39.0 MPH	Percent in Pace:	63.8%		

Park Avenue (Route 9)
 west of Shirley Street
 City, State: Worcester, MA
 Client: GIA/A. Allen
 Site Code: TBD



PDI File #: 249915 ATR A (speed)

Count Date
 Wednesday, March 20, 2024

Speed (60-minute)

EB																
Start Time:	1 to 14	15 to 19	20 to 24	25 to 29	30 to 34	35 to 39	40 to 44	45 to 49	50 to 54	55 to 59	60 to 64	65 to 69	70+	Total	85th %ile	Ave Speed
12:00 AM	0	4	4	28	33	15	5	1	1	0	0	0	0	91	35.5	31.0
1:00 AM	1	1	5	11	11	11	2	0	0	0	0	0	0	42	36.9	30.9
2:00 AM	1	0	1	9	15	21	2	1	1	0	0	0	0	51	37.5	33.7
3:00 AM	0	0	1	9	14	12	5	5	2	0	0	0	0	48	44.0	35.4
4:00 AM	0	1	4	8	26	22	21	6	2	0	0	0	0	90	43.0	36.0
5:00 AM	0	1	7	20	73	57	26	18	1	0	0	0	0	203	41.0	35.3
6:00 AM	0	1	7	73	159	114	41	7	0	0	1	0	0	403	38.7	33.6
7:00 AM	1	14	35	144	266	173	48	9	0	0	0	0	0	690	37.0	32.2
8:00 AM	0	3	36	170	298	152	38	7	2	1	0	0	0	707	37.0	32.0
9:00 AM	0	8	36	157	253	91	28	5	2	0	0	0	0	580	36.0	31.3
10:00 AM	4	6	33	135	205	115	24	1	2	0	1	0	0	526	36.0	31.5
11:00 AM	1	5	43	145	219	91	23	5	1	0	0	0	0	533	36.0	31.2
12:00 PM	2	6	55	159	217	126	23	3	0	0	0	0	0	591	36.0	31.0
1:00 PM	6	8	55	198	230	86	20	3	0	3	0	0	0	609	35.0	30.3
2:00 PM	0	6	61	195	244	85	17	4	0	0	0	0	0	612	35.0	30.3
3:00 PM	2	12	61	202	194	82	21	4	1	0	0	0	0	579	35.3	30.0
4:00 PM	1	8	42	174	266	75	16	3	0	0	0	0	0	585	35.0	30.6
5:00 PM	0	9	42	179	234	86	20	2	0	0	0	0	0	572	35.0	30.6
6:00 PM	1	3	59	156	176	61	9	7	0	0	0	0	0	472	35.0	30.1
7:00 PM	0	8	44	116	153	48	21	1	1	0	0	0	0	392	35.4	30.3
8:00 PM	0	1	30	112	153	63	12	3	0	0	0	0	0	374	35.1	30.9
9:00 PM	0	2	20	71	125	48	14	8	1	1	0	0	0	290	36.0	31.8
10:00 PM	0	2	16	62	88	59	12	4	0	0	0	0	0	243	38.0	32.0
11:00 PM	1	1	13	44	57	37	11	4	0	0	0	0	0	168	37.0	31.9
Total	21	110	710	2577	3709	1730	459	111	17	5	2	0	0	9451	36.0	31.3
Percent	0.22%	1.16%	7.51%	27.27%	39.24%	18.30%	4.86%	1.17%	0.18%	0.05%	0.02%	0.00%	0.00%			

AM Peak	10:00 AM	7:00 AM	11:00 AM	8:00 AM	8:00 AM	7:00 AM	7:00 AM	5:00 AM	3:00 AM	8:00 AM	6:00 AM			8:00 AM
Volume	4	14	43	170	298	173	48	18	2	1	1	0	0	707

PM Peak	1:00 PM	3:00 PM	2:00 PM	3:00 PM	4:00 PM	12:00 PM	12:00 PM	9:00 PM	3:00 PM	1:00 PM				2:00 PM
Volume	6	12	61	202	266	126	23	8	1	3	0	0	0	612

15th Percentile:	26.0 MPH	Average Speed:	31.3 MPH	Posted Speed Limit:	35 MPH
50th Percentile:	31.0 MPH	10 MPH Pace:	27 to 36 MPH	Number of Vehicles > 35 MPH:	1802
85th Percentile:	36.0 MPH	Number in Pace:	6540	Percent of Vehicles > 35 MPH:	19.1%
95th Percentile:	40.0 MPH	Percent in Pace:	69.2%		

Park Avenue (Route 9)
 west of Shirley Street
 City, State: Worcester, MA
 Client: GIA/A. Allen
 Site Code: TBD



PDI File #: 249915 ATR A (speed)

Count Date
 Wednesday, March 20, 2024

Speed (60-minute)

WB																
Start Time:	1 to 14	15 to 19	20 to 24	25 to 29	30 to 34	35 to 39	40 to 44	45 to 49	50 to 54	55 to 59	60 to 64	65 to 69	70+	Total	85th %ile	Ave Speed
12:00 AM	0	4	15	43	23	5	5	0	0	0	0	0	0	95	33.9	28.6
1:00 AM	0	1	6	7	14	8	2	1	0	1	0	0	0	40	37.0	32.0
2:00 AM	0	4	4	11	16	6	1	0	0	0	0	0	0	42	34.9	29.5
3:00 AM	0	0	3	3	12	4	2	1	0	0	0	0	0	25	37.8	32.6
4:00 AM	0	0	6	11	15	13	3	2	0	0	0	0	0	50	38.0	32.4
5:00 AM	0	1	8	28	35	22	3	2	0	2	0	0	0	101	37.0	31.9
6:00 AM	0	2	21	48	57	39	13	4	1	0	0	0	0	185	37.0	31.6
7:00 AM	4	5	58	106	116	45	18	5	2	0	0	0	0	359	35.0	29.8
8:00 AM	6	14	88	142	131	48	10	1	0	0	0	0	0	440	34.0	28.5
9:00 AM	4	19	85	133	145	49	11	4	0	1	0	0	0	451	34.0	28.8
10:00 AM	5	26	79	140	123	48	8	0	0	1	0	0	0	430	34.0	28.2
11:00 AM	2	35	98	177	163	50	7	1	0	0	0	0	0	533	33.0	28.2
12:00 PM	5	42	108	148	120	66	11	3	0	0	0	0	0	503	35.0	27.9
1:00 PM	3	58	162	173	98	28	8	1	1	0	0	0	0	532	32.0	26.1
2:00 PM	13	83	139	154	131	46	13	2	0	0	0	0	0	581	33.0	26.4
3:00 PM	12	49	134	151	121	67	11	1	0	0	0	0	0	546	34.0	27.2
4:00 PM	13	43	132	195	126	51	6	0	1	0	0	0	0	567	33.0	26.9
5:00 PM	11	49	138	189	104	36	5	0	0	0	0	0	0	532	33.0	26.4
6:00 PM	12	35	156	164	84	18	2	1	0	0	0	0	0	472	31.0	25.6
7:00 PM	3	30	109	168	76	18	7	1	2	0	0	0	0	414	32.0	26.7
8:00 PM	4	23	91	142	73	23	4	2	0	0	0	0	0	362	32.0	26.8
9:00 PM	0	15	72	101	70	13	4	1	0	0	0	0	0	276	32.0	27.1
10:00 PM	2	9	40	89	60	24	4	1	0	0	0	0	0	229	34.0	28.4
11:00 PM	0	7	21	49	52	16	2	1	0	0	0	0	0	148	34.0	29.1
Total	99	554	1773	2572	1965	743	160	35	7	5	0	0	0	7913	34.0	27.6
Percent	1.25%	7.00%	22.41%	32.50%	24.83%	9.39%	2.02%	0.44%	0.09%	0.06%	0.00%	0.00%	0.00%			

AM Peak	8:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	7:00 AM	7:00 AM	7:00 AM	5:00 AM					11:00 AM
Volume	6	35	98	177	163	50	18	5	2	2	0	0	0	533	

PM Peak	2:00 PM	2:00 PM	1:00 PM	4:00 PM	2:00 PM	3:00 PM	2:00 PM	12:00 PM	7:00 PM					2:00 PM
Volume	13	83	162	195	131	67	13	3	2	0	0	0	581	

15th Percentile:	21.0 MPH	Average Speed:	27.6 MPH	Posted Speed Limit:	35 MPH
50th Percentile:	28.0 MPH	10 MPH Pace:	23 to 32 MPH	Number of Vehicles > 35 MPH:	698
85th Percentile:	34.0 MPH	Number in Pace:	4777	Percent of Vehicles > 35 MPH:	8.8%
95th Percentile:	37.0 MPH	Percent in Pace:	60.4%		

Park Avenue (Route 9)
 west of Shirley Street
 City, State: Worcester, MA
 Client: GIA/A. Allen
 Site Code: TBD



PDI File #: 249915 ATR A (speed)

Count Date
 Wednesday, March 20, 2024

Speed (60-minute)

Combined EB and WB

Start Time:	1 to 14	15 to 19	20 to 24	25 to 29	30 to 34	35 to 39	40 to 44	45 to 49	50 to 54	55 to 59	60 to 64	65 to 69	70+	Total	85th %ile	Ave Speed
12:00 AM	0	8	19	71	56	20	10	1	1	0	0	0	0	186	35.0	29.7
1:00 AM	1	2	11	18	25	19	4	1	0	1	0	0	0	82	37.0	31.4
2:00 AM	1	4	5	20	31	27	3	1	1	0	0	0	0	93	37.0	31.8
3:00 AM	0	0	4	12	26	16	7	6	2	0	0	0	0	73	41.4	34.5
4:00 AM	0	1	10	19	41	35	24	8	2	0	0	0	0	140	41.2	34.8
5:00 AM	0	2	15	48	108	79	29	20	1	2	0	0	0	304	40.0	34.1
6:00 AM	0	3	28	121	216	153	54	11	1	0	1	0	0	588	38.0	33.0
7:00 AM	5	19	93	250	382	218	66	14	2	0	0	0	0	1049	37.0	31.4
8:00 AM	6	17	124	312	429	200	48	8	2	1	0	0	0	1147	36.0	30.7
9:00 AM	4	27	121	290	398	140	39	9	2	1	0	0	0	1031	35.0	30.2
10:00 AM	9	32	112	275	328	163	32	1	2	1	1	0	0	956	35.0	30.0
11:00 AM	3	40	141	322	382	141	30	6	1	0	0	0	0	1066	35.0	29.7
12:00 PM	7	48	163	307	337	192	34	6	0	0	0	0	0	1094	36.0	29.6
1:00 PM	9	66	217	371	328	114	28	4	1	3	0	0	0	1141	34.0	28.3
2:00 PM	13	89	200	349	375	131	30	6	0	0	0	0	0	1193	34.0	28.4
3:00 PM	14	61	195	353	315	149	32	5	1	0	0	0	0	1125	35.0	28.6
4:00 PM	14	51	174	369	392	126	22	3	1	0	0	0	0	1152	34.0	28.8
5:00 PM	11	58	180	368	338	122	25	2	0	0	0	0	0	1104	34.0	28.6
6:00 PM	13	38	215	320	260	79	11	8	0	0	0	0	0	944	34.0	27.9
7:00 PM	3	38	153	284	229	66	28	2	3	0	0	0	0	806	34.0	28.4
8:00 PM	4	24	121	254	226	86	16	5	0	0	0	0	0	736	34.0	28.9
9:00 PM	0	17	92	172	195	61	18	9	1	1	0	0	0	566	35.0	29.5
10:00 PM	2	11	56	151	148	83	16	5	0	0	0	0	0	472	36.0	30.3
11:00 PM	1	8	34	93	109	53	13	5	0	0	0	0	0	316	37.0	30.6
Total	120	664	2483	5149	5674	2473	619	146	24	10	2	0	0	17364	35.0	29.6
Percent	0.69%	3.82%	14.30%	29.65%	32.68%	14.24%	3.56%	0.84%	0.14%	0.06%	0.01%	0.00%	0.00%			

AM Peak	10:00 AM	11:00 AM	11:00 AM	11:00 AM	8:00 AM	7:00 AM	7:00 AM	5:00 AM	3:00 AM	5:00 AM	6:00 AM				8:00 AM
Volume	9	40	141	322	429	218	66	20	2	2	1	0	0	0	1147

PM Peak	3:00 PM	2:00 PM	1:00 PM	1:00 PM	4:00 PM	12:00 PM	12:00 PM	9:00 PM	7:00 PM	1:00 PM					2:00 PM
Volume	14	89	217	371	392	192	34	9	3	3	0	0	0	0	1193

15th Percentile:	24.0 MPH	Average Speed:	29.6 MPH	Posted Speed Limit:	35 MPH
50th Percentile:	30.0 MPH	10 MPH Pace:	25 to 34 MPH	Number of Vehicles > 35 MPH:	2500
85th Percentile:	35.0 MPH	Number in Pace:	10823	Percent of Vehicles > 35 MPH:	14.4%
95th Percentile:	39.0 MPH	Percent in Pace:	62.3%		

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Cars and Heavy Vehicles (Combined)

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	4	7	7	0	18	3	85	2	0	90	14	5	10	0	29	3	136	1	0	140	277
7:15 AM	1	13	14	0	28	8	81	9	0	98	4	10	6	0	20	3	192	0	0	195	341
7:30 AM	4	17	14	0	35	14	83	5	0	102	14	17	10	0	41	12	184	6	0	202	380
7:45 AM	8	17	19	0	44	12	78	8	0	98	12	16	12	0	40	9	215	4	0	228	410
Total	17	54	54	0	125	37	327	24	0	388	44	48	38	0	130	27	727	11	0	765	1408
8:00 AM	2	12	19	0	33	6	113	5	0	124	8	22	9	0	39	5	203	2	0	210	406
8:15 AM	2	10	15	0	27	7	97	5	0	109	11	11	11	0	33	4	208	3	0	215	384
8:30 AM	0	10	12	0	22	6	115	6	0	127	12	11	10	1	34	10	174	3	0	187	370
8:45 AM	7	5	13	0	25	5	136	11	0	152	14	8	17	0	39	8	169	3	0	180	396
Total	11	37	59	0	107	24	461	27	0	512	45	52	47	1	145	27	754	11	0	792	1556
Grand Total	28	91	113	0	232	61	788	51	0	900	89	100	85	1	275	54	1481	22	0	1557	2964
Approach %	12.1	39.2	48.7	0.0		6.8	87.6	5.7	0.0		32.4	36.4	30.9	0.4		3.5	95.1	1.4	0.0		
Total %	0.9	3.1	3.8	0.0	7.8	2.1	26.6	1.7	0.0	30.4	3.0	3.4	2.9	0.0	9.3	1.8	50.0	0.7	0.0	52.5	
Exiting Leg Total	183					1683					197					901					2964
Cars	28	87	106	0	221	53	753	49	0	855	88	96	83	1	268	53	1440	20	0	1513	2857
% Cars	100.0	95.6	93.8	0.0	95.3	86.9	95.6	96.1	0.0	95.0	98.9	96.0	97.6	100.0	97.5	98.1	97.2	90.9	0.0	97.2	96.4
Exiting Leg Total	169					1634					190					864					2857
Heavy Vehicles	0	4	7	0	11	8	35	2	0	45	1	4	2	0	7	1	41	2	0	44	107
% Heavy Vehicles	0.0	4.4	6.2	0.0	4.7	13.1	4.4	3.9	0.0	5.0	1.1	4.0	2.4	0.0	2.5	1.9	2.8	9.1	0.0	2.8	3.6
Exiting Leg Total	14					49					7					37					107

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:30 AM	4	17	14	0	35	14	83	5	0	102	14	17	10	0	41	12	184	6	0	202	380
7:45 AM	8	17	19	0	44	12	78	8	0	98	12	16	12	0	40	9	215	4	0	228	410
8:00 AM	2	12	19	0	33	6	113	5	0	124	8	22	9	0	39	5	203	2	0	210	406
8:15 AM	2	10	15	0	27	7	97	5	0	109	11	11	11	0	33	4	208	3	0	215	384
Total Volume	16	56	67	0	139	39	371	23	0	433	45	66	42	0	153	30	810	15	0	855	1580
% Approach Total	11.5	40.3	48.2	0.0		9.0	85.7	5.3	0.0		29.4	43.1	27.5	0.0		3.5	94.7	1.8	0.0		
PHF	0.500	0.824	0.882	0.000	0.790	0.696	0.821	0.719	0.000	0.873	0.804	0.750	0.875	0.000	0.933	0.625	0.942	0.625	0.000	0.938	0.963
Cars	16	54	63	0	133	36	358	22	0	416	45	65	41	0	151	30	793	13	0	836	1536
Cars %	100.0	96.4	94.0	0.0	95.7	92.3	96.5	95.7	0.0	96.1	100.0	98.5	97.6	0.0	98.7	100.0	97.9	86.7	0.0	97.8	97.2
Heavy Vehicles	0	2	4	0	6	3	13	1	0	17	0	1	1	0	2	0	17	2	0	19	44
Heavy Vehicles %	0.0	3.6	6.0	0.0	4.3	7.7	3.5	4.3	0.0	3.9	0.0	1.5	2.4	0.0	1.3	0.0	2.1	13.3	0.0	2.2	2.8
Cars Enter Leg	16	54	63	0	133	36	358	22	0	416	45	65	41	0	151	30	793	13	0	836	1536
Heavy Enter Leg	0	2	4	0	6	3	13	1	0	17	0	1	1	0	2	0	17	2	0	19	44
Total Entering Leg	16	56	67	0	139	39	371	23	0	433	45	66	42	0	153	30	810	15	0	855	1580
Cars Exiting Leg	114					901					106					415					1536
Heavy Exiting Leg	6					21					3					14					44
Total Exiting Leg	120					922					109					429					1580

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Cars

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	4	7	7	0	18	3	80	2	0	85	14	4	10	0	28	3	132	1	0	136	267
7:15 AM	1	12	14	0	27	8	71	8	0	87	4	9	6	0	19	3	185	0	0	188	321
7:30 AM	4	17	13	0	34	12	76	5	0	93	14	16	9	0	39	12	181	6	0	199	365
7:45 AM	8	16	19	0	43	11	75	8	0	94	12	16	12	0	40	9	211	3	0	223	400
Total	17	52	53	0	122	34	302	23	0	359	44	45	37	0	126	27	709	10	0	746	1353
8:00 AM	2	12	19	0	33	6	112	4	0	122	8	22	9	0	39	5	196	2	0	203	397
8:15 AM	2	9	12	0	23	7	95	5	0	107	11	11	11	0	33	4	205	2	0	211	374
8:30 AM	0	9	12	0	21	2	111	6	0	119	12	10	10	1	33	10	167	3	0	180	353
8:45 AM	7	5	10	0	22	4	133	11	0	148	13	8	16	0	37	7	163	3	0	173	380
Total	11	35	53	0	99	19	451	26	0	496	44	51	46	1	142	26	731	10	0	767	1504
Grand Total	28	87	106	0	221	53	753	49	0	855	88	96	83	1	268	53	1440	20	0	1513	2857
Approach %	12.7	39.4	48.0	0.0		6.2	88.1	5.7	0.0		32.8	35.8	31.0	0.4		3.5	95.2	1.3	0.0		
Total %	1.0	3.0	3.7	0.0	7.7	1.9	26.4	1.7	0.0	29.9	3.1	3.4	2.9	0.0	9.4	1.9	50.4	0.7	0.0	53.0	
Exiting Leg Total	169					1634					190					864					2857

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:30 AM	4	17	13	0	34	12	76	5	0	93	14	16	9	0	39	12	181	6	0	199	365
7:45 AM	8	16	19	0	43	11	75	8	0	94	12	16	12	0	40	9	211	3	0	223	400
8:00 AM	2	12	19	0	33	6	112	4	0	122	8	22	9	0	39	5	196	2	0	203	397
8:15 AM	2	9	12	0	23	7	95	5	0	107	11	11	11	0	33	4	205	2	0	211	374
Total Volume	16	54	63	0	133	36	358	22	0	416	45	65	41	0	151	30	793	13	0	836	1536
% Approach Total	12.0	40.6	47.4	0.0		8.7	86.1	5.3	0.0		29.8	43.0	27.2	0.0		3.6	94.9	1.6	0.0		
PHF	0.500	0.794	0.829	0.000	0.773	0.750	0.799	0.688	0.000	0.852	0.804	0.739	0.854	0.000	0.944	0.625	0.940	0.542	0.000	0.937	0.960
Entering Leg	16	54	63	0	133	36	358	22	0	416	45	65	41	0	151	30	793	13	0	836	1536
Exiting Leg	114					901					106					415					1536
Total	247					1317					257					1251					3072

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	5	0	0	5	0	1	0	0	1	0	4	0	0	4	10
7:15 AM	0	1	0	0	1	0	10	1	0	11	0	1	0	0	1	0	7	0	0	7	20
7:30 AM	0	0	1	0	1	2	7	0	0	9	0	1	1	0	2	0	3	0	0	3	15
7:45 AM	0	1	0	0	1	1	3	0	0	4	0	0	0	0	0	0	4	1	0	5	10
Total	0	2	1	0	3	3	25	1	0	29	0	3	1	0	4	0	18	1	0	19	55
8:00 AM	0	0	0	0	0	0	1	1	0	2	0	0	0	0	0	0	7	0	0	7	9
8:15 AM	0	1	3	0	4	0	2	0	0	2	0	0	0	0	0	0	3	1	0	4	10
8:30 AM	0	1	0	0	1	4	4	0	0	8	0	1	0	0	1	0	7	0	0	7	17
8:45 AM	0	0	3	0	3	1	3	0	0	4	1	0	1	0	2	1	6	0	0	7	16
Total	0	2	6	0	8	5	10	1	0	16	1	1	1	0	3	1	23	1	0	25	52
Grand Total	0	4	7	0	11	8	35	2	0	45	1	4	2	0	7	1	41	2	0	44	107
Approach %	0.0	36.4	63.6	0.0		17.8	77.8	4.4	0.0		14.3	57.1	28.6	0.0		2.3	93.2	4.5	0.0		
Total %	0.0	3.7	6.5	0.0	10.3	7.5	32.7	1.9	0.0	42.1	0.9	3.7	1.9	0.0	6.5	0.9	38.3	1.9	0.0	41.1	
Exiting Leg Total	14					49					7					37					107
Buses	0	4	5	0	9	5	17	1	0	23	0	3	1	0	4	0	21	2	0	23	59
% Buses	0.0	100.0	71.4	0.0	81.8	62.5	48.6	50.0	0.0	51.1	0.0	75.0	50.0	0.0	57.1	0.0	51.2	100.0	0.0	52.3	55.1
Exiting Leg Total	10					26					5					18					59
Single-Unit Trucks	0	0	2	0	2	3	13	0	0	16	1	1	1	0	3	1	13	0	0	14	35
% Single-Unit	0.0	0.0	28.6	0.0	18.2	37.5	37.1	0.0	0.0	35.6	100.0	25.0	50.0	0.0	42.9	100.0	31.7	0.0	0.0	31.8	32.7
Exiting Leg Total	4					16					1					14					35
Articulated Trucks	0	0	0	0	0	0	5	1	0	6	0	0	0	0	0	0	7	0	0	7	13
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	14.3	50.0	0.0	13.3	0.0	0.0	0.0	0.0	0.0	0.0	17.1	0.0	0.0	15.9	12.1
Exiting Leg Total	0					7					1					5					13

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	5	0	0	5	0	1	0	0	1	0	4	0	0	4	10
7:15 AM	0	1	0	0	1	0	10	1	0	11	0	1	0	0	1	0	7	0	0	7	20
7:30 AM	0	0	1	0	1	2	7	0	0	9	0	1	1	0	2	0	3	0	0	3	15
7:45 AM	0	1	0	0	1	1	3	0	0	4	0	0	0	0	0	0	4	1	0	5	10
Total Volume	0	2	1	0	3	3	25	1	0	29	0	3	1	0	4	0	18	1	0	19	55
% Approach Total	0.0	66.7	33.3	0.0		10.3	86.2	3.4	0.0		0.0	75.0	25.0	0.0		0.0	94.7	5.3	0.0		
PHF	0.000	0.500	0.250	0.000	0.750	0.375	0.625	0.250	0.000	0.659	0.000	0.750	0.250	0.000	0.500	0.000	0.643	0.250	0.000	0.679	0.688
Buses	0	2	1	0	3	2	12	1	0	15	0	3	0	0	3	0	11	1	0	12	33
Buses %	0.0	100.0	100.0	0.0	100.0	66.7	48.0	100.0	0.0	51.7	0.0	100.0	0.0	0.0	75.0	0.0	61.1	100.0	0.0	63.2	60.0
Single-Unit Trucks	0	0	0	0	0	1	8	0	0	9	0	0	1	0	1	0	5	0	0	5	15
Single-Unit %	0.0	0.0	0.0	0.0	0.0	33.3	32.0	0.0	0.0	31.0	0.0	0.0	100.0	0.0	25.0	0.0	27.8	0.0	0.0	26.3	27.3
Articulated Trucks	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	2	0	0	2	7
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	20.0	0.0	0.0	17.2	0.0	0.0	0.0	0.0	0.0	0.0	11.1	0.0	0.0	10.5	12.7
Buses	0	2	1	0	3	2	12	1	0	15	0	3	0	0	3	0	11	1	0	12	33
Single-Unit Trucks	0	0	0	0	0	1	8	0	0	9	0	0	1	0	1	0	5	0	0	5	15
Articulated Trucks	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	2	0	0	2	7
Total Entering Leg	0	2	1	0	3	3	25	1	0	29	0	3	1	0	4	0	18	1	0	19	55
Buses	6					12					3					12					33
Single-Unit Trucks	1					5					0					9					15
Articulated Trucks	0					2					0					5					7
Total Exiting Leg	7					19					3					26					55

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Buses

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	4	0	0	4	0	1	0	0	1	0	3	0	0	3	8
7:15 AM	0	1	0	0	1	0	1	1	0	2	0	1	0	0	1	0	3	0	0	3	7
7:30 AM	0	0	1	0	1	2	4	0	0	6	0	1	0	0	1	0	2	0	0	2	10
7:45 AM	0	1	0	0	1	0	3	0	0	3	0	0	0	0	0	0	3	1	0	4	8
Total	0	2	1	0	3	2	12	1	0	15	0	3	0	0	3	0	11	1	0	12	33
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	3	3
8:15 AM	0	1	3	0	4	0	1	0	0	1	0	0	0	0	0	0	1	1	0	2	7
8:30 AM	0	1	0	0	1	2	3	0	0	5	0	0	0	0	0	0	5	0	0	5	11
8:45 AM	0	0	1	0	1	1	1	0	0	2	0	0	1	0	1	0	1	0	0	1	5
Total	0	2	4	0	6	3	5	0	0	8	0	0	1	0	1	0	10	1	0	11	26
Grand Total	0	4	5	0	9	5	17	1	0	23	0	3	1	0	4	0	21	2	0	23	59
Approach %	0.0	44.4	55.6	0.0		21.7	73.9	4.3	0.0		0.0	75.0	25.0	0.0		0.0	91.3	8.7	0.0		
Total %	0.0	6.8	8.5	0.0	15.3	8.5	28.8	1.7	0.0	39.0	0.0	5.1	1.7	0.0	6.8	0.0	35.6	3.4	0.0	39.0	
Exiting Leg Total	10					26					5					18					59

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	4	0	0	4	0	1	0	0	1	0	3	0	0	3	8
7:15 AM	0	1	0	0	1	0	1	1	0	2	0	1	0	0	1	0	3	0	0	3	7
7:30 AM	0	0	1	0	1	2	4	0	0	6	0	1	0	0	1	0	2	0	0	2	10
7:45 AM	0	1	0	0	1	0	3	0	0	3	0	0	0	0	0	0	3	1	0	4	8
Total Volume	0	2	1	0	3	2	12	1	0	15	0	3	0	0	3	0	11	1	0	12	33
% Approach Total	0.0	66.7	33.3	0.0		13.3	80.0	6.7	0.0		0.0	100.0	0.0	0.0		0.0	91.7	8.3	0.0		
PHF	0.000	0.500	0.250	0.000	0.750	0.250	0.750	0.250	0.000	0.625	0.000	0.750	0.000	0.000	0.750	0.000	0.917	0.250	0.000	0.750	0.825
Entering Leg	0	2	1	0	3	2	12	1	0	15	0	3	0	0	3	0	11	1	0	12	33
Exiting Leg	6					12					3					12					33
Total	9					27					6					24					66

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Single-Unit Trucks

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
7:00 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2	
7:15 AM	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	3	0	0	3	8	
7:30 AM	0	0	0	0	0	0	2	0	0	2	0	0	1	0	1	0	1	0	0	1	4	
7:45 AM	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	
Total	0	0	0	0	0	1	8	0	0	9	0	0	1	0	1	0	5	0	0	5	15	
8:00 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2	
8:15 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3	
8:30 AM	0	0	0	0	0	2	1	0	0	3	0	1	0	0	1	0	1	0	0	1	5	
8:45 AM	0	0	2	0	2	0	2	0	0	2	1	0	0	0	1	1	4	0	0	5	10	
Total	0	0	2	0	2	2	5	0	0	7	1	1	0	0	2	1	8	0	0	9	20	
Grand Total	0	0	2	0	2	3	13	0	0	16	1	1	1	0	3	1	13	0	0	14	35	
Approach %	0.0	0.0	100.0	0.0		18.8	81.3	0.0	0.0		33.3	33.3	33.3	0.0		7.1	92.9	0.0	0.0			
Total %	0.0	0.0	5.7	0.0	5.7	8.6	37.1	0.0	0.0	45.7	2.9	2.9	2.9	0.0	8.6	2.9	37.1	0.0	0.0	40.0		
Exiting Leg Total						4					16					1					14	35

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
8:00 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2	
8:15 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3	
8:30 AM	0	0	0	0	0	2	1	0	0	3	0	1	0	0	1	0	1	0	0	1	5	
8:45 AM	0	0	2	0	2	0	2	0	0	2	1	0	0	0	1	1	4	0	0	5	10	
Total Volume	0	0	2	0	2	2	5	0	0	7	1	1	0	0	2	1	8	0	0	9	20	
% Approach Total	0.0	0.0	100.0	0.0		28.6	71.4	0.0	0.0		50.0	50.0	0.0	0.0		11.1	88.9	0.0	0.0			
PHF	0.000	0.000	0.250	0.000	0.250	0.250	0.625	0.000	0.000	0.583	0.250	0.250	0.000	0.000	0.500	0.250	0.500	0.000	0.000	0.450	0.500	
Entering Leg	0	0	2	0	2	2	5	0	0	7	1	1	0	0	2	1	8	0	0	9	20	
Exiting Leg						3					11					1					5	20
Total						5					18					3					14	40

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Articulated Trucks

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
7:30 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
Total	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	7
8:00 AM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	4
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
Total	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	6
Grand Total	0	0	0	0	0	0	5	1	0	6	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	13
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	83.3	16.7	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	38.5	7.7	0.0	46.2	0.0	0.0	0.0	0.0	0.0	0.0	53.8	0.0	0.0	53.8	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	0					7					1					5					13					

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
7:15 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	5
7:30 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
8:00 AM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	4
Total Volume	0	0	0	0	0	0	5	1	0	6	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	11
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	83.3	16.7	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.313	0.250	0.000	0.375	0.000	0.000	0.000	0.000	0.000	0.000	0.417	0.000	0.000	0.417	0.000	0.000	0.000	0.000	0.000	0.550
Entering Leg	0	0	0	0	0	0	5	1	0	6	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	11
Exiting Leg	0					5					1					5					11					
Total	0					11					1					10					22					

PDI File #: 249915 A
 Location: N: Maywood Street S: Maywood Street
 Location: E: Park Avenue (Route 9) W: Park Avenue (Route 9)
 City, State: Worcester, MA
 Client: GIA/A. Allen
 Site Code: TBD
 Count Date: Wednesday, March 20, 2024
 Start Time: 7:00 AM
 End Time: 9:00 AM
 Class:



Bicycles (on Roadway and Crosswalks)

	Maywood Street							Park Avenue (Route 9)							Maywood Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
Exiting Leg Total	0							0							0							0							0

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

7:00 AM	Maywood Street							Park Avenue (Route 9)							Maywood Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000		
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Exiting Leg	0							0							0							0							0
Total	0							0							0							0							0

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class: **Pedestrians**



Pedestrians

	Maywood Street							Park Avenue (Route 9)							Maywood Street							Park Avenue (Route 9)							Total	
	from North							from East							from South							from West								
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	2	2	0	0	0	0	0	0	0	3
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	1	0	1	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	0	0	1	0	1	0	0	0	0	3	0	3	0	0	0	0	0	2	2	0	0	0	0	0	0	0	6	
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	2	0	2	0	0	0	0	0	1	1	5	
8:15 AM	0	0	0	0	0	1	1	0	0	0	0	0	2	2	0	0	0	0	0	2	2	0	0	0	0	2	0	2	7	
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	2	0	2	0	0	0	0	0	0	0	5	
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
Total	0	0	0	0	0	1	1	0	0	0	0	8	2	10	0	0	0	0	4	2	6	0	0	0	0	2	1	3	20	
Grand Total	0	0	0	0	1	1	2	0	0	0	0	11	2	13	0	0	0	0	4	4	8	0	0	0	0	2	1	3	26	
Approach %	0	0	0	0	50	50		0	0	0	0	84.6	15.4		0	0	0	0	50	50		0	0	0	0	66.7	33.3			
Total %	0	0	0	0	3.85	3.85	7.69	0	0	0	0	42.3	7.69	50	0	0	0	0	15.4	15.4	30.8	0	0	0	0	7.69	3.85	11.5		
Exiting Leg Total	2							13							8							3							26	

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Maywood Street							Park Avenue (Route 9)							Maywood Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	2	0	2	0	0	0	0	0	1	1	5
8:15 AM	0	0	0	0	0	1	1	0	0	0	0	0	2	2	0	0	0	0	0	2	2	0	0	0	0	2	0	2	7
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	2	0	2	0	0	0	0	0	0	0	5
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
Total Volume	0	0	0	0	0	1	1	0	0	0	0	8	2	10	0	0	0	0	4	2	6	0	0	0	0	2	1	3	20
% Approach Total	0.0	0.0	0.0	0.0	0.0	100.0		0.0	0.0	0.0	0.0	80.0	20.0		0.0	0.0	0.0	0.0	66.7	33.3		0.0	0.0	0.0	0.0	66.7	33.3		
PHF	0.000	0.000	0.000	0.000	0.000	0.250	0.250	0.000	0.000	0.000	0.000	0.667	0.250	0.833	0.000	0.000	0.000	0.000	0.500	0.250	0.750	0.000	0.000	0.000	0.000	0.250	0.250	0.375	0.714
Entering Leg	0	0	0	0	0	1	1	0	0	0	0	8	2	10	0	0	0	0	4	2	6	0	0	0	0	2	1	3	20
Exiting Leg	1							10							6							3							20
Total	2							20							12							6							40

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars and Heavy Vehicles (Combined)

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	3	10	11	0	24	14	211	11	0	236	11	13	21	0	45	6	158	2	0	166	471
4:15 PM	4	12	8	0	24	12	180	14	0	206	16	14	13	0	43	7	124	1	0	132	405
4:30 PM	0	11	10	0	21	8	185	15	0	208	17	16	16	0	49	10	132	4	0	146	424
4:45 PM	3	11	15	0	29	6	194	16	0	216	19	15	11	0	45	5	123	2	0	130	420
Total	10	44	44	0	98	40	770	56	0	866	63	58	61	0	182	28	537	9	0	574	1720
5:00 PM	4	11	9	0	24	8	159	11	0	178	15	24	23	0	62	4	138	0	0	142	406
5:15 PM	0	13	11	0	24	10	184	17	0	211	15	22	10	0	47	8	138	2	0	148	430
5:30 PM	1	7	7	0	15	15	161	14	0	190	25	21	17	0	63	5	139	1	0	145	413
5:45 PM	4	16	9	0	29	5	158	21	0	184	18	16	9	0	43	5	118	5	0	128	384
Total	9	47	36	0	92	38	662	63	0	763	73	83	59	0	215	22	533	8	0	563	1633
Grand Total	19	91	80	0	190	78	1432	119	0	1629	136	141	120	0	397	50	1070	17	0	1137	3353
Approach %	10.0	47.9	42.1	0.0		4.8	87.9	7.3	0.0		34.3	35.5	30.2	0.0		4.4	94.1	1.5	0.0		
Total %	0.6	2.7	2.4	0.0	5.7	2.3	42.7	3.5	0.0	48.6	4.1	4.2	3.6	0.0	11.8	1.5	31.9	0.5	0.0	33.9	
Exiting Leg Total	236					1286					260					1571					3353
Cars	19	90	72	0	181	71	1416	118	0	1605	135	139	118	0	392	50	1053	17	0	1120	3298
% Cars	100.0	98.9	90.0	0.0	95.3	91.0	98.9	99.2	0.0	98.5	99.3	98.6	98.3	0.0	98.7	100.0	98.4	100.0	0.0	98.5	98.4
Exiting Leg Total	227					1260					258					1553					3298
Heavy Vehicles	0	1	8	0	9	7	16	1	0	24	1	2	2	0	5	0	17	0	0	17	55
% Heavy Vehicles	0.0	1.1	10.0	0.0	4.7	9.0	1.1	0.8	0.0	1.5	0.7	1.4	1.7	0.0	1.3	0.0	1.6	0.0	0.0	1.5	1.6
Exiting Leg Total	9					26					2					18					55

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	3	10	11	0	24	14	211	11	0	236	11	13	21	0	45	6	158	2	0	166	471
4:15 PM	4	12	8	0	24	12	180	14	0	206	16	14	13	0	43	7	124	1	0	132	405
4:30 PM	0	11	10	0	21	8	185	15	0	208	17	16	16	0	49	10	132	4	0	146	424
4:45 PM	3	11	15	0	29	6	194	16	0	216	19	15	11	0	45	5	123	2	0	130	420
Total Volume	10	44	44	0	98	40	770	56	0	866	63	58	61	0	182	28	537	9	0	574	1720
% Approach Total	10.2	44.9	44.9	0.0		4.6	88.9	6.5	0.0		34.6	31.9	33.5	0.0		4.9	93.6	1.6	0.0		
PHF	0.625	0.917	0.733	0.000	0.845	0.714	0.912	0.875	0.000	0.917	0.829	0.906	0.726	0.000	0.929	0.700	0.850	0.563	0.000	0.864	0.913
Cars	10	44	39	0	93	36	757	56	0	849	62	57	59	0	178	28	526	9	0	563	1683
Cars %	100.0	100.0	88.6	0.0	94.9	90.0	98.3	100.0	0.0	98.0	98.4	98.3	96.7	0.0	97.8	100.0	98.0	100.0	0.0	98.1	97.8
Heavy Vehicles	0	0	5	0	5	4	13	0	0	17	1	1	2	0	4	0	11	0	0	11	37
Heavy Vehicles %	0.0	0.0	11.4	0.0	5.1	10.0	1.7	0.0	0.0	2.0	1.6	1.7	3.3	0.0	2.2	0.0	2.0	0.0	0.0	1.9	2.2
Cars Enter Leg	10	44	39	0	93	36	757	56	0	849	62	57	59	0	178	28	526	9	0	563	1683
Heavy Enter Leg	0	0	5	0	5	4	13	0	0	17	1	1	2	0	4	0	11	0	0	11	37
Total Entering Leg	10	44	44	0	98	40	770	56	0	866	63	58	61	0	182	28	537	9	0	574	1720
Cars Exiting Leg	102					627					128					826					1683
Heavy Exiting Leg	5					17					0					15					37
Total Exiting Leg	107					644					128					841					1720

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	3	10	9	0	22	12	207	11	0	230	11	13	19	0	43	6	155	2	0	163	458					
4:15 PM	4	12	8	0	24	10	174	14	0	198	16	14	13	0	43	7	121	1	0	129	394					
4:30 PM	0	11	8	0	19	8	184	15	0	207	17	16	16	0	49	10	130	4	0	144	419					
4:45 PM	3	11	14	0	28	6	192	16	0	214	18	14	11	0	43	5	120	2	0	127	412					
Total	10	44	39	0	93	36	757	56	0	849	62	57	59	0	178	28	526	9	0	563	1683					
5:00 PM	4	11	8	0	23	7	158	11	0	176	15	24	23	0	62	4	136	0	0	140	401					
5:15 PM	0	13	10	0	23	9	183	16	0	208	15	21	10	0	46	8	136	2	0	146	423					
5:30 PM	1	7	6	0	14	14	160	14	0	188	25	21	17	0	63	5	137	1	0	143	408					
5:45 PM	4	15	9	0	28	5	158	21	0	184	18	16	9	0	43	5	118	5	0	128	383					
Total	9	46	33	0	88	35	659	62	0	756	73	82	59	0	214	22	527	8	0	557	1615					
Grand Total	19	90	72	0	181	71	1416	118	0	1605	135	139	118	0	392	50	1053	17	0	1120	3298					
Approach %	10.5	49.7	39.8	0.0		4.4	88.2	7.4	0.0		34.4	35.5	30.1	0.0		4.5	94.0	1.5	0.0							
Total %	0.6	2.7	2.2	0.0	5.5	2.2	42.9	3.6	0.0	48.7	4.1	4.2	3.6	0.0	11.9	1.5	31.9	0.5	0.0	34.0						
Exiting Leg Total						227					1260					258					1553					3298

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	3	10	9	0	22	12	207	11	0	230	11	13	19	0	43	6	155	2	0	163	458					
4:15 PM	4	12	8	0	24	10	174	14	0	198	16	14	13	0	43	7	121	1	0	129	394					
4:30 PM	0	11	8	0	19	8	184	15	0	207	17	16	16	0	49	10	130	4	0	144	419					
4:45 PM	3	11	14	0	28	6	192	16	0	214	18	14	11	0	43	5	120	2	0	127	412					
Total Volume	10	44	39	0	93	36	757	56	0	849	62	57	59	0	178	28	526	9	0	563	1683					
% Approach Total	10.8	47.3	41.9	0.0		4.2	89.2	6.6	0.0		34.8	32.0	33.1	0.0		5.0	93.4	1.6	0.0							
PHF	0.625	0.917	0.696	0.000	0.830	0.750	0.914	0.875	0.000	0.923	0.861	0.891	0.776	0.000	0.908	0.700	0.848	0.563	0.000	0.863	0.919					
Entering Leg	10	44	39	0	93	36	757	56	0	849	62	57	59	0	178	28	526	9	0	563	1683					
Exiting Leg						102					627					128					826					1683
Total						195					1476					306					1389					3366

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	2	0	2	2	4	0	0	6	0	0	2	0	2	0	3	0	0	3	13
4:15 PM	0	0	0	0	0	2	6	0	0	8	0	0	0	0	0	0	3	0	0	3	11
4:30 PM	0	0	2	0	2	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	5
4:45 PM	0	0	1	0	1	0	2	0	0	2	1	1	0	0	2	0	3	0	0	3	8
Total	0	0	5	0	5	4	13	0	0	17	1	1	2	0	4	0	11	0	0	11	37
5:00 PM	0	0	1	0	1	1	1	0	0	2	0	0	0	0	0	0	2	0	0	2	5
5:15 PM	0	0	1	0	1	1	1	1	0	3	0	1	0	0	1	0	2	0	0	2	7
5:30 PM	0	0	1	0	1	1	1	0	0	2	0	0	0	0	0	0	2	0	0	2	5
5:45 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	1	3	0	4	3	3	1	0	7	0	1	0	0	1	0	6	0	0	6	18
Grand Total	0	1	8	0	9	7	16	1	0	24	1	2	2	0	5	0	17	0	0	17	55
Approach %	0.0	11.1	88.9	0.0		29.2	66.7	4.2	0.0		20.0	40.0	40.0	0.0		0.0	100.0	0.0	0.0		
Total %	0.0	1.8	14.5	0.0	16.4	12.7	29.1	1.8	0.0	43.6	1.8	3.6	3.6	0.0	9.1	0.0	30.9	0.0	0.0	30.9	
Exiting Leg Total	9					26					2					18					55
Buses	0	0	8	0	8	6	5	0	0	11	0	0	2	0	2	0	8	0	0	8	29
% Buses	0.0	0.0	100.0	0.0	88.9	85.7	31.3	0.0	0.0	45.8	0.0	0.0	100.0	0.0	40.0	0.0	47.1	0.0	0.0	47.1	52.7
Exiting Leg Total	6					16					0					7					29
Single-Unit Trucks	0	1	0	0	1	0	7	1	0	8	1	2	0	0	3	0	9	0	0	9	21
% Single-Unit	0.0	100.0	0.0	0.0	11.1	0.0	43.8	100.0	0.0	33.3	100.0	100.0	0.0	0.0	60.0	0.0	52.9	0.0	0.0	52.9	38.2
Exiting Leg Total	2					10					2					7					21
Articulated Trucks	0	0	0	0	0	1	4	0	0	5	0	0	0	0	0	0	0	0	0	0	5
% Articulated	0.0	0.0	0.0	0.0	0.0	14.3	25.0	0.0	0.0	20.8	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	9.1
Exiting Leg Total	1					0					0					4					5

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	2	0	2	2	4	0	0	6	0	0	2	0	2	0	3	0	0	3	13
4:15 PM	0	0	0	0	0	2	6	0	0	8	0	0	0	0	0	0	3	0	0	3	11
4:30 PM	0	0	2	0	2	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	5
4:45 PM	0	0	1	0	1	0	2	0	0	2	1	1	0	0	2	0	3	0	0	3	8
Total Volume	0	0	5	0	5	4	13	0	0	17	1	1	2	0	4	0	11	0	0	11	37
% Approach Total	0.0	0.0	100.0	0.0		23.5	76.5	0.0	0.0		25.0	25.0	50.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.000	0.625	0.000	0.625	0.500	0.542	0.000	0.000	0.531	0.250	0.250	0.250	0.000	0.500	0.000	0.917	0.000	0.000	0.917	0.712
Buses	0	0	5	0	5	3	5	0	0	8	0	0	2	0	2	0	5	0	0	5	20
Buses %	0.0	0.0	100.0	0.0	100.0	75.0	38.5	0.0	0.0	47.1	0.0	0.0	100.0	0.0	50.0	0.0	45.5	0.0	0.0	45.5	54.1
Single-Unit Trucks	0	0	0	0	0	0	7	0	0	7	1	1	0	0	2	0	6	0	0	6	15
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	53.8	0.0	0.0	41.2	100.0	100.0	0.0	0.0	50.0	0.0	54.5	0.0	0.0	54.5	40.5
Articulated Trucks	0	0	0	0	0	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	2
Articulated %	0.0	0.0	0.0	0.0	0.0	25.0	7.7	0.0	0.0	11.8	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	5.4
Buses	0	0	5	0	5	3	5	0	0	8	0	0	2	0	2	0	5	0	0	5	20
Single-Unit Trucks	0	0	0	0	0	0	7	0	0	7	1	1	0	0	2	0	6	0	0	6	15
Articulated Trucks	0	0	0	0	0	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	2
Total Entering Leg	0	0	5	0	5	4	13	0	0	17	1	1	2	0	4	0	11	0	0	11	37
Buses	3					10					0					7					20
Single-Unit Trucks	1					7					0					7					15
Articulated Trucks	1					0					0					1					2
Total Exiting Leg	5					17					0					15					37

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Buses

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
4:00 PM	0	0	2	0	2	1	3	0	0	4	0	0	2	0	2	0	1	0	0	1	9	
4:15 PM	0	0	0	0	0	2	1	0	0	3	0	0	0	0	0	0	2	0	0	2	5	
4:30 PM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	3	
4:45 PM	0	0	1	0	1	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	3	
Total	0	0	5	0	5	3	5	0	0	8	0	0	2	0	2	0	5	0	0	5	20	
5:00 PM	0	0	1	0	1	1	0	0	0	1	0	0	0	0	0	0	1	0	0	1	3	
5:15 PM	0	0	1	0	1	1	0	0	0	1	0	0	0	0	0	0	1	0	0	1	3	
5:30 PM	0	0	1	0	1	1	0	0	0	1	0	0	0	0	0	0	1	0	0	1	3	
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	0	3	0	3	3	0	0	0	3	0	0	0	0	0	0	3	0	0	3	9	
Grand Total	0	0	8	0	8	6	5	0	0	11	0	0	2	0	2	0	8	0	0	8	29	
Approach %	0.0	0.0	100.0	0.0		54.5	45.5	0.0	0.0		0.0	0.0	100.0	0.0		0.0	100.0	0.0	0.0			
Total %	0.0	0.0	27.6	0.0	27.6	20.7	17.2	0.0	0.0	37.9	0.0	0.0	6.9	0.0	6.9	0.0	27.6	0.0	0.0	27.6		
Exiting Leg Total						6					16					0					7	29

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	2	0	2	1	3	0	0	4	0	0	2	0	2	0	1	0	0	1	9
4:15 PM	0	0	0	0	0	2	1	0	0	3	0	0	0	0	0	0	2	0	0	2	5
4:30 PM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	3
4:45 PM	0	0	1	0	1	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	3
Total Volume	0	0	5	0	5	3	5	0	0	8	0	0	2	0	2	0	5	0	0	5	20
% Approach Total	0.0	0.0	100.0	0.0		37.5	62.5	0.0	0.0		0.0	0.0	100.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.000	0.625	0.000	0.625	0.375	0.417	0.000	0.000	0.500	0.000	0.000	0.250	0.000	0.250	0.000	0.625	0.000	0.000	0.625	0.556
Entering Leg	0					3					0					0					20
Exiting Leg	3					10					0					7					20
Total	8					18					2					12					40

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Single-Unit Trucks

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3	
4:15 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	1	0	0	1	5	
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2	
4:45 PM	0	0	0	0	0	0	1	0	0	1	1	1	0	0	2	0	2	0	0	2	5	
Total	0	0	0	0	0	0	7	0	0	7	1	1	0	0	2	0	6	0	0	6	15	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1	
5:15 PM	0	0	0	0	0	0	0	1	0	1	0	1	0	0	1	0	1	0	0	1	3	
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1	
5:45 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
Total	0	1	0	0	1	0	0	1	0	1	0	1	0	0	1	0	3	0	0	3	6	
Grand Total	0	1	0	0	1	0	7	1	0	8	1	2	0	0	3	0	9	0	0	9	21	
Approach %	0.0	100.0	0.0	0.0		0.0	87.5	12.5	0.0		33.3	66.7	0.0	0.0		0.0	100.0	0.0	0.0			
Total %	0.0	4.8	0.0	0.0	4.8	0.0	33.3	4.8	0.0	38.1	4.8	9.5	0.0	0.0	14.3	0.0	42.9	0.0	0.0	42.9		
Exiting Leg Total						2					10					2					7	21

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3	
4:15 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	1	0	0	1	5	
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2	
4:45 PM	0	0	0	0	0	0	1	0	0	1	1	1	0	0	2	0	2	0	0	2	5	
Total Volume	0	0	0	0	0	0	7	0	0	7	1	1	0	0	2	0	6	0	0	6	15	
% Approach Total	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		50.0	50.0	0.0	0.0		0.0	100.0	0.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.438	0.000	0.000	0.438	0.250	0.250	0.000	0.000	0.250	0.000	0.750	0.000	0.000	0.750	0.750	
Entering Leg	0	0	0	0	0	0	7	0	0	7	1	1	0	0	2	0	6	0	0	6	15	
Exiting Leg						1					7					0					7	15
Total						1					14					2					13	30

PDI File #: **249915 A**
 Location: **N: Maywood Street S: Maywood Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Articulated Trucks

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
5:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
5:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
Grand Total	0	0	0	0	0	1	4	0	0	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5
Approach %	0.0	0.0	0.0	0.0	0.0	20.0	80.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	20.0	80.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total						1						0						0	4	5						

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maywood Street					Park Avenue (Route 9)					Maywood Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
5:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total Volume	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.750	0.000	0.000	0.750	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.750
Entering Leg	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
Exiting Leg						0						0						0	3	3						
Total						0						3						0						3	6	

PDI File #: 249915 A
 Location: N: Maywood Street S: Maywood Street
 Location: E: Park Avenue (Route 9) W: Park Avenue (Route 9)
 City, State: Worcester, MA
 Client: GIA/A. Allen
 Site Code: TBD
 Count Date: Wednesday, March 20, 2024
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



Bicycles (on Roadway and Crosswalks)

	Maywood Street								Park Avenue (Route 9)								Maywood Street								Park Avenue (Route 9)								Total
	from North								from East								from South								from West								
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total		Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total		Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total		Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1			
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
Total	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1			
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
Grand Total	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1			
Approach %	0.0	0.0	0.0	0.0	0.0	100.0		0.0	0.0	0.0	0.0	0.0	0.0					0.0	0.0	0.0	0.0	0.0	0.0										
Total %	0.0	0.0	0.0	0.0	0.0	100.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0			
Exiting Leg Total								1								0								0								1	

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Maywood Street								Park Avenue (Route 9)								Maywood Street								Park Avenue (Route 9)								Total
	from North								from East								from South								from West								
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total		Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total		Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total		Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1				
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0				
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0				
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0				
Total Volume	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1				
% Approach Total	0.0	0.0	0.0	0.0	0.0	100.0		0.0	0.0	0.0	0.0	0.0	0.0					0.0	0.0	0.0	0.0	0.0	0.0										
PHF	0.000	0.000	0.000	0.000	0.000	0.250	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.250				
Entering Leg	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1				
Exiting Leg								1								0								0								1	
Total								2								0								0								2	

PDI File #: 249915 A
 Location: N: Maywood Street S: Maywood Street
 Location: E: Park Avenue (Route 9) W: Park Avenue (Route 9)
 City, State: Worcester, MA
 Client: GIA/A. Allen
 Site Code: TBD
 Count Date: Wednesday, March 20, 2024
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class: Pedestrians



Pedestrians

	Maywood Street							Park Avenue (Route 9)							Maywood Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	2	2	6		
4:15 PM	0	0	0	0	0	3	3	0	0	0	0	2	1	3	0	0	0	0	2	0	2	0	0	0	0	0	8		
4:30 PM	0	0	0	0	1	0	1	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2		
4:45 PM	0	0	0	0	2	0	2	0	0	0	0	1	9	10	0	0	0	0	2	1	3	0	0	0	0	1	1	16	
Total	0	0	0	0	4	4	8	0	0	0	0	3	11	14	0	0	0	0	6	1	7	0	0	0	0	3	3	32	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1	0	0	0	0	0	0	1	
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	1	1	0	0	0	0	1	1	2	0	0	0	0	0	0	3	
5:30 PM	0	0	0	0	0	0	0	0	0	0	3	1	4	4	0	0	0	0	2	1	3	0	0	0	0	0	0	7	
5:45 PM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	2	3	0	0	0	0	2	1	3	7	
Total	0	0	0	0	1	0	1	0	0	0	0	3	2	5	0	0	0	0	4	5	9	0	0	0	0	2	1	3	18
Grand Total	0	0	0	0	5	4	9	0	0	0	0	6	13	19	0	0	0	0	10	6	16	0	0	0	0	2	4	6	50
Approach %	0	0	0	0	55.6	44.4		0	0	0	0	31.6	68.4		0	0	0	0	62.5	37.5		0	0	0	0	33.3	66.7		
Total %	0	0	0	0	10	8	18	0	0	0	0	12	26	38	0	0	0	0	20	12	32	0	0	0	0	4	8	12	
Exiting Leg Total	9							19							16							6							50

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Maywood Street							Park Avenue (Route 9)							Maywood Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	2	2	6	
4:15 PM	0	0	0	0	0	3	3	0	0	0	0	2	1	3	0	0	0	0	2	0	2	0	0	0	0	0	0	8	
4:30 PM	0	0	0	0	1	0	1	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
4:45 PM	0	0	0	0	2	0	2	0	0	0	0	1	9	10	0	0	0	0	2	1	3	0	0	0	0	0	1	1	16
Total Volume	0	0	0	0	4	4	8	0	0	0	0	3	11	14	0	0	0	0	6	1	7	0	0	0	0	0	3	3	32
% Approach Total	0.0	0.0	0.0	0.0	50.0	50.0		0.0	0.0	0.0	0.0	21.4	78.6		0.0	0.0	0.0	0.0	85.7	14.3		0.0	0.0	0.0	0.0	0.0	100.0		
PHF	0.000	0.000	0.000	0.000	0.500	0.333	0.667	0.000	0.000	0.000	0.000	0.375	0.306	0.350	0.000	0.000	0.000	0.000	0.750	0.250	0.583	0.000	0.000	0.000	0.000	0.000	0.375	0.375	0.500
Entering Leg	0	0	0	0	4	4	8	0	0	0	0	3	11	14	0	0	0	0	6	1	7	0	0	0	0	0	3	3	32
Exiting Leg	8							14							7							3							32
Total	16							28							14							6							64

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Cars and Heavy Vehicles (Combined)

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	1	120	0	0	121	0	0	0	0	0	0	232	0	0	232	353
7:15 AM	0	0	0	0	0	0	119	0	0	119	0	0	0	0	0	0	248	0	0	248	367
7:30 AM	0	0	0	0	0	0	107	0	0	107	0	0	0	0	0	0	182	1	0	183	290
7:45 AM	0	0	0	0	0	0	133	0	0	133	0	0	0	0	0	1	205	1	0	207	340
Total	0	0	0	0	0	1	479	0	0	480	0	0	0	0	0	1	867	2	0	870	1350
8:00 AM	0	0	0	0	0	0	138	0	0	138	0	0	1	0	1	0	170	2	0	172	311
8:15 AM	0	0	0	0	0	0	119	0	0	119	1	0	0	0	1	1	149	0	0	150	270
8:30 AM	0	0	0	0	0	0	124	1	0	125	0	0	0	0	0	0	140	1	0	141	266
8:45 AM	0	0	0	0	0	2	132	0	0	134	1	0	0	0	1	1	158	0	0	159	294
Total	0	0	0	0	0	2	513	1	0	516	2	0	1	0	3	2	617	3	0	622	1141
Grand Total	0	0	0	0	0	3	992	1	0	996	2	0	1	0	3	3	1484	5	0	1492	2491
Approach %	0.0	0.0	0.0	0.0	0.0	0.3	99.6	0.1	0.0	99.6	66.7	0.0	33.3	0.0	66.7	0.2	99.5	0.3	0.0	99.8	
Total %	0.0	0.0	0.0	0.0	0.0	0.1	39.8	0.0	0.0	40.0	0.1	0.0	0.0	0.0	0.1	0.1	59.6	0.2	0.0	59.9	
Exiting Leg Total	8					1486					4					993					2491
Cars	0	0	0	0	0	3	950	1	0	954	2	0	1	0	3	3	1421	5	0	1429	2386
% Cars	0.0	0.0	0.0	0.0	0.0	100.0	95.8	100.0	0.0	95.8	100.0	0.0	100.0	0.0	100.0	100.0	95.8	100.0	0.0	95.8	
Exiting Leg Total	8					1423					4					951					2386
Heavy Vehicles	0	0	0	0	0	0	42	0	0	42	0	0	0	0	0	0	63	0	0	63	105
% Heavy Vehicles	0.0	0.0	0.0	0.0	0.0	0.0	4.2	0.0	0.0	4.2	0.0	0.0	0.0	0.0	0.0	0.0	4.2	0.0	0.0	4.2	4.2
Exiting Leg Total	0					63					0					42					105

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	1	120	0	0	121	0	0	0	0	0	0	232	0	0	232	353
7:15 AM	0	0	0	0	0	0	119	0	0	119	0	0	0	0	0	0	248	0	0	248	367
7:30 AM	0	0	0	0	0	0	107	0	0	107	0	0	0	0	0	0	182	1	0	183	290
7:45 AM	0	0	0	0	0	0	133	0	0	133	0	0	0	0	0	1	205	1	0	207	340
Total Volume	0	0	0	0	0	1	479	0	0	480	0	0	0	0	0	1	867	2	0	870	1350
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.2	99.8	0.0	0.0	99.8	0.0	0.0	0.0	0.0	0.0	0.1	99.7	0.2	0.0	99.9	
PHF	0.000	0.000	0.000	0.000	0.000	0.250	0.900	0.000	0.000	0.902	0.000	0.000	0.000	0.000	0.000	0.250	0.874	0.500	0.000	0.877	0.920
Cars	0	0	0	0	0	1	465	0	0	466	0	0	0	0	0	1	836	2	0	839	1305
Cars %	0.0	0.0	0.0	0.0	0.0	100.0	97.1	0.0	0.0	97.1	0.0	0.0	0.0	0.0	0.0	100.0	96.4	100.0	0.0	96.4	96.7
Heavy Vehicles	0	0	0	0	0	0	14	0	0	14	0	0	0	0	0	0	31	0	0	31	45
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	2.9	0.0	0.0	2.9	0.0	0.0	0.0	0.0	0.0	0.0	3.6	0.0	0.0	3.6	3.3
Cars Enter Leg	0	0	0	0	0	1	465	0	0	466	0	0	0	0	0	1	836	2	0	839	1305
Heavy Enter Leg	0	0	0	0	0	0	14	0	0	14	0	0	0	0	0	0	31	0	0	31	45
Total Entering Leg	0	0	0	0	0	1	479	0	0	480	0	0	0	0	0	1	867	2	0	870	1350
Cars Exiting Leg	3					836					1					465					1305
Heavy Exiting Leg	0					31					0					14					45
Total Exiting Leg	3					867					1					479					1350

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Cars

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	1	117	0	0	118	0	0	0	0	0	0	226	0	0	226	344
7:15 AM	0	0	0	0	0	0	117	0	0	117	0	0	0	0	0	0	241	0	0	241	358
7:30 AM	0	0	0	0	0	0	101	0	0	101	0	0	0	0	0	0	174	1	0	175	276
7:45 AM	0	0	0	0	0	0	130	0	0	130	0	0	0	0	0	1	195	1	0	197	327
Total	0	0	0	0	0	1	465	0	0	466	0	0	0	0	0	1	836	2	0	839	1305
8:00 AM	0	0	0	0	0	0	132	0	0	132	0	0	1	0	1	0	161	2	0	163	296
8:15 AM	0	0	0	0	0	0	111	0	0	111	1	0	0	0	1	1	138	0	0	139	251
8:30 AM	0	0	0	0	0	0	117	1	0	118	0	0	0	0	0	0	135	1	0	136	254
8:45 AM	0	0	0	0	0	2	125	0	0	127	1	0	0	0	1	1	151	0	0	152	280
Total	0	0	0	0	0	2	485	1	0	488	2	0	1	0	3	2	585	3	0	590	1081
Grand Total	0	0	0	0	0	3	950	1	0	954	2	0	1	0	3	3	1421	5	0	1429	2386
Approach %	0.0	0.0	0.0	0.0	0.0	0.3	99.6	0.1	0.0	0.0	66.7	0.0	33.3	0.0	0.0	0.2	99.4	0.3	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.1	39.8	0.0	0.0	40.0	0.1	0.0	0.0	0.0	0.1	0.1	59.6	0.2	0.0	59.9	
Exiting Leg Total						8						4						951	2386		

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	1	117	0	0	118	0	0	0	0	0	0	226	0	0	226	344
7:15 AM	0	0	0	0	0	0	117	0	0	117	0	0	0	0	0	0	241	0	0	241	358
7:30 AM	0	0	0	0	0	0	101	0	0	101	0	0	0	0	0	0	174	1	0	175	276
7:45 AM	0	0	0	0	0	0	130	0	0	130	0	0	0	0	0	1	195	1	0	197	327
Total Volume	0	0	0	0	0	1	465	0	0	466	0	0	0	0	0	1	836	2	0	839	1305
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.250	99.8	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.1	99.6	0.2	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.250	0.894	0.000	0.000	0.896	0.000	0.000	0.000	0.000	0.000	0.250	0.867	0.500	0.000	0.870	0.911
Entering Leg	0	0	0	0	0	1	465	0	0	466	0	0	0	0	0	1	836	2	0	839	1305
Exiting Leg						3						1						465	1305		
Total						3						1						1304	2610		

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**



Class: Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	6	0	0	6	9
7:15 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	7	0	0	7	9
7:30 AM	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	8	0	0	8	14
7:45 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	10	0	0	10	13
Total	0	0	0	0	0	0	14	0	0	14	0	0	0	0	0	0	31	0	0	31	45
8:00 AM	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	9	0	0	9	15
8:15 AM	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	0	11	0	0	11	19
8:30 AM	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	5	0	0	5	12
8:45 AM	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	7	0	0	7	14
Total	0	0	0	0	0	0	28	0	0	28	0	0	0	0	0	0	32	0	0	32	60
Grand Total	0	0	0	0	0	0	42	0	0	42	0	0	0	0	0	0	63	0	0	63	105
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	40.0	0.0	0.0	40.0	0.0	0.0	0.0	0.0	0.0	0.0	60.0	0.0	0.0	60.0	
Exiting Leg Total	0					63					0					42					105
Buses	0	0	0	0	0	0	10	0	0	10	0	0	0	0	0	0	30	0	0	30	40
% Buses	0.0	0.0	0.0	0.0	0.0	0.0	23.8	0.0	0.0	23.8	0.0	0.0	0.0	0.0	0.0	0.0	47.6	0.0	0.0	47.6	38.1
Exiting Leg Total	0					30					0					10					40
Single-Unit Trucks	0	0	0	0	0	0	28	0	0	28	0	0	0	0	0	0	27	0	0	27	55
% Single-Unit	0.0	0.0	0.0	0.0	0.0	0.0	66.7	0.0	0.0	66.7	0.0	0.0	0.0	0.0	0.0	0.0	42.9	0.0	0.0	42.9	52.4
Exiting Leg Total	0					27					0					28					55
Articulated Trucks	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	6	0	0	6	10
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	9.5	0.0	0.0	9.5	0.0	0.0	0.0	0.0	0.0	0.0	9.5	0.0	0.0	9.5	9.5
Exiting Leg Total	0					6					0					4					10

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:30 AM	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	8	0	0	8	14
7:45 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	10	0	0	10	13
8:00 AM	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	9	0	0	9	15
8:15 AM	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	0	11	0	0	11	19
Total Volume	0	0	0	0	0	0	23	0	0	23	0	0	0	0	0	0	38	0	0	38	61
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.719	0.000	0.000	0.719	0.000	0.000	0.000	0.000	0.000	0.000	0.864	0.000	0.000	0.864	0.803
Buses	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	21	0	0	21	28
Buses %	0.0	0.0	0.0	0.0	0.0	0.0	30.4	0.0	0.0	30.4	0.0	0.0	0.0	0.0	0.0	0.0	55.3	0.0	0.0	55.3	45.9
Single-Unit Trucks	0	0	0	0	0	0	14	0	0	14	0	0	0	0	0	0	15	0	0	15	29
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	60.9	0.0	0.0	60.9	0.0	0.0	0.0	0.0	0.0	0.0	39.5	0.0	0.0	39.5	47.5
Articulated Trucks	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	4
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	8.7	0.0	0.0	8.7	0.0	0.0	0.0	0.0	0.0	0.0	5.3	0.0	0.0	5.3	6.6
Buses	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	21	0	0	21	28
Single-Unit Trucks	0	0	0	0	0	0	14	0	0	14	0	0	0	0	0	0	15	0	0	15	29
Articulated Trucks	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	4
Total Entering Leg	0	0	0	0	0	0	23	0	0	23	0	0	0	0	0	0	38	0	0	38	61
Buses	0					21					0					7					28
Single-Unit Trucks	0					15					0					14					29
Articulated Trucks	0					2					0					2					4
Total Exiting Leg	0					38					0					23					61

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Buses

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	4	4
7:15 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
7:30 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	7	0	0	7	11
7:45 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	4
Total	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	16	0	0	16	22
8:00 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	6	0	0	6	8
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	0	0	5	5
8:30 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
8:45 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
Total	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	14	0	0	14	18
Grand Total	0	0	0	0	0	0	10	0	0	10	0	0	0	0	0	0	30	0	0	30	40
Approach %	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	25.0	0.0	0.0	25.0	0.0	0.0	0.0	0.0	0.0	0.0	75.0	0.0	0.0	75.0	
Exiting Leg Total	0					30					0					10					40

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:30 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	7	0	0	7	11
7:45 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	4
8:00 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	6	0	0	6	8
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	0	0	5	5
Total Volume	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	21	0	0	21	28
% Approach Total	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.438	0.000	0.000	0.438	0.000	0.000	0.000	0.000	0.000	0.000	0.750	0.000	0.000	0.750	0.636
Entering Leg	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	21	0	0	21	28
Exiting Leg	0					21					0					7					28
Total	0					28					0					28					56

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Single-Unit Trucks

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	4
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	3	3
7:30 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	2
7:45 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	7	0	0	7	9
Total	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	11	0	0	11	18
8:00 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	3	0	0	3	6
8:15 AM	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	5	0	0	5	12
8:30 AM	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	2	0	0	2	8
8:45 AM	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	6	0	0	6	11
Total	0	0	0	0	0	0	21	0	0	21	0	0	0	0	0	0	16	0	0	16	37
Grand Total	0	0	0	0	0	0	28	0	0	28	0	0	0	0	0	0	27	0	0	27	55
Approach %	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	50.9	0.0	0.0	50.9	0.0	0.0	0.0	0.0	0.0	0.0	49.1	0.0	0.0	49.1	
Exiting Leg Total	0					27					0					28					55

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
8:00 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	3	0	0	3	6
8:15 AM	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	5	0	0	5	12
8:30 AM	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	2	0	0	2	8
8:45 AM	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	6	0	0	6	11
Total Volume	0	0	0	0	0	0	21	0	0	21	0	0	0	0	0	0	16	0	0	16	37
% Approach Total	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.750	0.000	0.000	0.750	0.000	0.000	0.000	0.000	0.000	0.000	0.667	0.000	0.000	0.667	0.771
Entering Leg	0	0	0	0	0	0	21	0	0	21	0	0	0	0	0	0	16	0	0	16	37
Exiting Leg	0					16					0					21					37
Total	0					37					0					37					74

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Articulated Trucks

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
7:15 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	5
8:00 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
8:15 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
8:45 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	2	0	0	2	5
Grand Total	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	6	0	0	6	10
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	40.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	60.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	40.0	0.0	0.0	40.0	0.0	0.0	0.0	0.0	0.0	0.0	60.0	0.0	0.0	60.0	
Exiting Leg Total	0					6					0					4					10

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
7:15 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	5
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	40.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	60.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.500	0.417
Entering Leg	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	5
Exiting Leg	0					4					0					1					5
Total	0					5					0					5					10

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class: **Pedestrians**



Pedestrians

	Fern Street							Park Avenue (Route 9)							Parking Lot							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	1	
7:30 AM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
7:45 AM	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
Total	0	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	3	
8:00 AM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
8:15 AM	0	0	0	0	3	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	
8:30 AM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
8:45 AM	0	0	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
Total	0	0	0	0	6	4	10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	
Grand Total	0	0	0	0	7	5	12	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	13		
Approach %	0	0	0	0	58.3	41.7		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	100	0				
Total %	0	0	0	0	53.8	38.5	92.3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7.69	0	7.69			
Exiting Leg Total	12							0							0							1							13

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

8:00 AM	Fern Street							Park Avenue (Route 9)							Parking Lot							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
8:00 AM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
8:15 AM	0	0	0	0	3	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	
8:30 AM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
8:45 AM	0	0	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
Total Volume	0	0	0	0	6	4	10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	
% Approach Total	0.0	0.0	0.0	0.0	60.0	40.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.500	0.500	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.500		
Entering Leg	0	0	0	0	6	4	10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10		
Exiting Leg	10							0							0							0							10
Total	20							0							0							0							20

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars and Heavy Vehicles (Combined)

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	1	183	0	0	184	0	0	0	0	0	0	180	2	0	182	366
4:15 PM	0	0	0	0	0	2	214	0	0	216	0	0	0	0	0	0	152	4	0	156	372
4:30 PM	0	0	0	0	0	1	192	0	0	193	1	0	0	0	1	0	174	2	0	176	370
4:45 PM	0	0	0	0	0	3	194	0	0	197	1	0	0	0	1	0	141	1	0	142	340
Total	0	0	0	0	0	7	783	0	0	790	2	0	0	0	2	0	647	9	0	656	1448
5:00 PM	0	0	0	0	0	3	176	0	0	179	1	0	0	0	1	0	139	0	0	139	319
5:15 PM	0	0	0	0	0	1	141	0	0	142	0	0	0	0	0	0	129	1	0	130	272
5:30 PM	0	0	0	0	0	4	179	0	0	183	1	0	0	0	1	0	125	3	0	128	312
5:45 PM	0	0	0	0	0	3	136	0	0	139	0	0	0	0	0	0	155	2	0	157	296
Total	0	0	0	0	0	11	632	0	0	643	2	0	0	0	2	0	548	6	0	554	1199
Grand Total	0	0	0	0	0	18	1415	0	0	1433	4	0	0	0	4	0	1195	15	0	1210	2647
Approach %	0.0	0.0	0.0	0.0	0.0	1.3	98.7	0.0	0.0		100.0	0.0	0.0	0.0		0.0	98.8	1.2	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.7	53.5	0.0	0.0	54.1	0.2	0.0	0.0	0.0	0.2	0.0	45.1	0.6	0.0	45.7	
Exiting Leg Total	33					1199					0					1415					2647
Cars	0	0	0	0	0	18	1403	0	0	1421	4	0	0	0	4	0	1176	15	0	1191	2616
% Cars	0.0	0.0	0.0	0.0	0.0	100.0	99.2	0.0	0.0	99.2	100.0	0.0	0.0	0.0	100.0	0.0	98.4	100.0	0.0	98.4	98.8
Exiting Leg Total	33					1180					0					1403					2616
Heavy Vehicles	0	0	0	0	0	0	12	0	0	12	0	0	0	0	0	0	19	0	0	19	31
% Heavy Vehicles	0.0	0.0	0.0	0.0	0.0	0.0	0.8	0.0	0.0	0.8	0.0	0.0	0.0	0.0	0.0	0.0	1.6	0.0	0.0	1.6	1.2
Exiting Leg Total	0					19					0					12					31

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	1	183	0	0	184	0	0	0	0	0	0	180	2	0	182	366
4:15 PM	0	0	0	0	0	2	214	0	0	216	0	0	0	0	0	0	152	4	0	156	372
4:30 PM	0	0	0	0	0	1	192	0	0	193	1	0	0	0	1	0	174	2	0	176	370
4:45 PM	0	0	0	0	0	3	194	0	0	197	1	0	0	0	1	0	141	1	0	142	340
Total Volume	0	0	0	0	0	7	783	0	0	790	2	0	0	0	2	0	647	9	0	656	1448
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.9	99.1	0.0	0.0		100.0	0.0	0.0	0.0		0.0	98.6	1.4	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.583	0.915	0.000	0.000	0.914	0.500	0.000	0.000	0.000	0.500	0.000	0.899	0.563	0.000	0.901	0.973
Cars	0	0	0	0	0	7	776	0	0	783	2	0	0	0	2	0	637	9	0	646	1431
Cars %	0.0	0.0	0.0	0.0	0.0	100.0	99.1	0.0	0.0	99.1	100.0	0.0	0.0	0.0	100.0	0.0	98.5	100.0	0.0	98.5	98.8
Heavy Vehicles	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	10	0	0	10	17
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	0.9	0.0	0.0	0.9	0.0	0.0	0.0	0.0	0.0	0.0	1.5	0.0	0.0	1.5	1.2
Cars Enter Leg	0	0	0	0	0	7	776	0	0	783	2	0	0	0	2	0	637	9	0	646	1431
Heavy Enter Leg	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	10	0	0	10	17
Total Entering Leg	0	0	0	0	0	7	783	0	0	790	2	0	0	0	2	0	647	9	0	656	1448
Cars Exiting Leg	16					639					0					776					1431
Heavy Exiting Leg	0					10					0					7					17
Total Exiting Leg	16					649					0					783					1448

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	1	182	0	0	183	0	0	0	0	0	0	176	2	0	178	361
4:15 PM	0	0	0	0	0	2	210	0	0	212	0	0	0	0	0	0	149	4	0	153	365
4:30 PM	0	0	0	0	0	1	190	0	0	191	1	0	0	0	1	0	172	2	0	174	366
4:45 PM	0	0	0	0	0	3	194	0	0	197	1	0	0	0	1	0	140	1	0	141	339
Total	0	0	0	0	0	7	776	0	0	783	2	0	0	0	2	0	637	9	0	646	1431
5:00 PM	0	0	0	0	0	3	175	0	0	178	1	0	0	0	1	0	136	0	0	136	315
5:15 PM	0	0	0	0	0	1	139	0	0	140	0	0	0	0	0	0	128	1	0	129	269
5:30 PM	0	0	0	0	0	4	178	0	0	182	1	0	0	0	1	0	122	3	0	125	308
5:45 PM	0	0	0	0	0	3	135	0	0	138	0	0	0	0	0	0	153	2	0	155	293
Total	0	0	0	0	0	11	627	0	0	638	2	0	0	0	2	0	539	6	0	545	1185
Grand Total	0	0	0	0	0	18	1403	0	0	1421	4	0	0	0	4	0	1176	15	0	1191	2616
Approach %	0.0	0.0	0.0	0.0		1.3	98.7	0.0	0.0		100.0	0.0	0.0	0.0		0.0	98.7	1.3	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.7	53.6	0.0	0.0	54.3	0.2	0.0	0.0	0.0	0.2	0.0	45.0	0.6	0.0	45.5	
Exiting Leg Total	33					1180					0					1403					2616

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	1	182	0	0	183	0	0	0	0	0	0	176	2	0	178	361
4:15 PM	0	0	0	0	0	2	210	0	0	212	0	0	0	0	0	0	149	4	0	153	365
4:30 PM	0	0	0	0	0	1	190	0	0	191	1	0	0	0	1	0	172	2	0	174	366
4:45 PM	0	0	0	0	0	3	194	0	0	197	1	0	0	0	1	0	140	1	0	141	339
Total Volume	0	0	0	0	0	7	776	0	0	783	2	0	0	0	2	0	637	9	0	646	1431
% Approach Total	0.0	0.0	0.0	0.0		0.9	99.1	0.0	0.0		100.0	0.0	0.0	0.0		0.0	98.6	1.4	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.583	0.924	0.000	0.000	0.923	0.500	0.000	0.000	0.000	0.500	0.000	0.905	0.563	0.000	0.907	0.977
Entering Leg	0	0	0	0	0	7	776	0	0	783	2	0	0	0	2	0	637	9	0	646	1431
Exiting Leg	16					639					0					776					1431
Total	16					1422					2					1422					2862

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**



Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	5
4:15 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	3	0	0	3	7
4:30 PM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	4
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
Total	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	10	0	0	10	17
5:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	4
5:15 PM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	3
5:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	4
5:45 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
Total	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	9	0	0	9	14
Grand Total	0	0	0	0	0	0	12	0	0	12	0	0	0	0	0	0	19	0	0	19	31
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	38.7	0.0	0.0	38.7	0.0	0.0	0.0	0.0	0.0	0.0	61.3	0.0	0.0	61.3	
Exiting Leg Total	0					19					0					12					31
Buses	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	10	0	0	10	16
% Buses	0.0	0.0	0.0	0.0	0.0	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	52.6	0.0	0.0	52.6	51.6
Exiting Leg Total	0					10					0					6					16
Single-Unit Trucks	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	7	0	0	7	9
% Single-Unit	0.0	0.0	0.0	0.0	0.0	0.0	16.7	0.0	0.0	16.7	0.0	0.0	0.0	0.0	0.0	0.0	36.8	0.0	0.0	36.8	29.0
Exiting Leg Total	0					7					0					2					9
Articulated Trucks	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	2	0	0	2	6
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	33.3	0.0	0.0	33.3	0.0	0.0	0.0	0.0	0.0	0.0	10.5	0.0	0.0	10.5	19.4
Exiting Leg Total	0					2					0					4					6

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	5
4:15 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	3	0	0	3	7
4:30 PM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	4
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
Total Volume	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	10	0	0	10	17
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.438	0.000	0.000	0.438	0.000	0.000	0.000	0.000	0.000	0.000	0.625	0.000	0.000	0.625	0.607
Buses	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	6	0	0	6	9
Buses %	0.0	0.0	0.0	0.0	0.0	0.0	42.9	0.0	0.0	42.9	0.0	0.0	0.0	0.0	0.0	0.0	60.0	0.0	0.0	60.0	52.9
Single-Unit Trucks	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	5
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	14.3	0.0	0.0	14.3	0.0	0.0	0.0	0.0	0.0	0.0	40.0	0.0	0.0	40.0	29.4
Articulated Trucks	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	3
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	42.9	0.0	0.0	42.9	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	17.6
Buses	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	6	0	0	6	9
Single-Unit Trucks	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	5
Articulated Trucks	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	3
Total Entering Leg	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	10	0	0	10	17
Buses	0					6					0					3					9
Single-Unit Trucks	0					4					0					1					5
Articulated Trucks	0					0					0					3					3
Total Exiting Leg	0					10					0					7					17

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Buses

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	2
4:15 PM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	4
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	6	0	0	6	9
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
5:15 PM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	2
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	2
5:45 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
Total	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	4	0	0	4	7
Grand Total	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	10	0	0	10	16
Approach %	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	37.5	0.0	0.0	37.5	0.0	0.0	0.0	0.0	0.0	0.0	62.5	0.0	0.0	62.5	
Exiting Leg Total	0					10					0					6					16

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	2
4:15 PM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	4
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	6	0	0	6	9
% Approach Total	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.375	0.000	0.000	0.375	0.000	0.000	0.000	0.000	0.000	0.000	0.750	0.000	0.000	0.750	0.563
Entering Leg	0					3					0					6					9
Exiting Leg	0					6					0					3					9
Total	0					9					0					9					18

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Single-Unit Trucks

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	2
4:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
Total	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	5
5:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	4
Grand Total	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	7	0	0	7	9
Approach %	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	22.2	0.0	0.0	22.2	0.0	0.0	0.0	0.0	0.0	0.0	77.8	0.0	0.0	77.8	
Exiting Leg Total	0					7					0					2					9

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
5:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
Total Volume	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	6
% Approach Total	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.500	0.500
Entering Leg	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	6
Exiting Leg	0					4					0					2					6
Total	0					6					0					6					12

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Articulated Trucks

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	3
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
5:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
Total	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
Grand Total	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	2	0	0	2	6
Approach %	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	66.7	0.0	0.0	66.7	0.0	0.0	0.0	0.0	0.0	0.0	33.3	0.0	0.0	33.3	
Exiting Leg Total	0					2					0					4					6

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Fern Street					Park Avenue (Route 9)					Parking Lot					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	3
% Approach Total	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.750	0.000	0.000	0.750	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.750
Entering Leg	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	3
Exiting Leg	0					0					0					3					3
Total	0					3					0					3					6

PDI File #: **249915 B**
 Location: **N: Fern Street S: Parking Lot**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class: **Pedestrians**



Pedestrians

	Fern Street							Park Avenue (Route 9)							Parking Lot							Park Avenue (Route 9)							Total						
	from North							from East							from South							from West													
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total							
4:00 PM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	3	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
Total	0	0	0	0	5	2	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	
5:00 PM	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
5:15 PM	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
5:30 PM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
5:45 PM	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
Total	0	0	0	0	3	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	
Grand Total	0	0	0	0	8	4	12	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	
Approach %	0	0	0	0	66.7	33.3		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
Total %	0	0	0	0	66.7	33.3	100	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
Exiting Leg Total	12							0							0							12													

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Fern Street							Park Avenue (Route 9)							Parking Lot							Park Avenue (Route 9)							Total						
	from North							from East							from South							from West													
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total							
4:00 PM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	3	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:45 PM	0	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
Total Volume	0	0	0	0	5	2	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	
% Approach Total	0.0	0.0	0.0	0.0	71.4	28.6		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.417	0.500	0.438	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.438			
Entering Leg	0	0	0	0	5	2	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	
Exiting Leg	7							0							0							7													
Total	14							0							0							14													

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Cars and Heavy Vehicles (Combined)

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	1	0	1	0	2	1	89	2	0	92	2	0	1	0	3	0	168	2	0	170	267
7:15 AM	1	0	1	0	2	1	97	3	0	101	1	0	1	0	2	0	188	1	0	189	294
7:30 AM	0	0	1	0	1	2	102	2	0	106	1	0	0	0	1	0	215	2	0	217	325
7:45 AM	4	0	1	0	5	1	90	1	0	92	2	0	2	0	4	0	259	2	0	261	362
Total	6	0	4	0	10	5	378	8	0	391	6	0	4	0	10	0	830	7	0	837	1248
8:00 AM	5	0	1	0	6	0	119	2	0	121	0	0	0	0	0	0	225	1	0	226	353
8:15 AM	2	0	3	0	5	0	122	3	0	125	2	0	1	0	3	0	237	3	0	240	373
8:30 AM	1	0	3	1	5	0	120	5	0	125	3	0	1	0	4	0	188	0	0	188	322
8:45 AM	3	0	0	0	3	1	143	7	0	151	4	0	1	0	5	1	204	1	0	206	365
Total	11	0	7	1	19	1	504	17	0	522	9	0	3	0	12	1	854	5	0	860	1413
Grand Total	17	0	11	1	29	6	882	25	0	913	15	0	7	0	22	1	1684	12	0	1697	2661
Approach %	58.6	0.0	37.9	3.4		0.7	96.6	2.7	0.0		68.2	0.0	31.8	0.0		0.1	99.2	0.7	0.0		
Total %	0.6	0.0	0.4	0.0	1.1	0.2	33.1	0.9	0.0	34.3	0.6	0.0	0.3	0.0	0.8	0.0	63.3	0.5	0.0	63.8	
Exiting Leg Total	19					1710					26					906					2661
Cars	16	0	11	1	28	6	834	25	0	865	15	0	6	0	21	1	1637	12	0	1650	2564
% Cars	94.1	0.0	100.0	100.0	96.6	100.0	94.6	100.0	0.0	94.7	100.0	0.0	85.7	0.0	95.5	100.0	97.2	100.0	0.0	97.2	96.4
Exiting Leg Total	19					1663					26					856					2564
Heavy Vehicles	1	0	0	0	1	0	48	0	0	48	0	0	1	0	1	0	47	0	0	47	97
% Heavy Vehicles	5.9	0.0	0.0	0.0	3.4	0.0	5.4	0.0	0.0	5.3	0.0	0.0	14.3	0.0	4.5	0.0	2.8	0.0	0.0	2.8	3.6
Exiting Leg Total	0					47					0					50					97

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:30 AM	0	0	1	0	1	2	102	2	0	106	1	0	0	0	1	0	215	2	0	217	325
7:45 AM	4	0	1	0	5	1	90	1	0	92	2	0	2	0	4	0	259	2	0	261	362
8:00 AM	5	0	1	0	6	0	119	2	0	121	0	0	0	0	0	0	225	1	0	226	353
8:15 AM	2	0	3	0	5	0	122	3	0	125	2	0	1	0	3	0	237	3	0	240	373
Total Volume	11	0	6	0	17	3	433	8	0	444	5	0	3	0	8	0	936	8	0	944	1413
% Approach Total	64.7	0.0	35.3	0.0		0.7	97.5	1.8	0.0		62.5	0.0	37.5	0.0		0.0	99.2	0.8	0.0		
PHF	0.550	0.000	0.500	0.000	0.708	0.375	0.887	0.667	0.000	0.888	0.625	0.000	0.375	0.000	0.500	0.000	0.903	0.667	0.000	0.904	0.947
Cars	10	0	6	0	16	3	414	8	0	425	5	0	2	0	7	0	913	8	0	921	1369
Cars %	90.9	0.0	100.0	0.0	94.1	100.0	95.6	100.0	0.0	95.7	100.0	0.0	66.7	0.0	87.5	0.0	97.5	100.0	0.0	97.6	96.9
Heavy Vehicles	1	0	0	0	1	0	19	0	0	19	0	0	1	0	1	0	23	0	0	23	44
Heavy Vehicles %	9.1	0.0	0.0	0.0	5.9	0.0	4.4	0.0	0.0	4.3	0.0	0.0	33.3	0.0	12.5	0.0	2.5	0.0	0.0	2.4	3.1
Cars Enter Leg	10	0	6	0	16	3	414	8	0	425	5	0	2	0	7	0	913	8	0	921	1369
Heavy Enter Leg	1	0	0	0	1	0	19	0	0	19	0	0	1	0	1	0	23	0	0	23	44
Total Entering Leg	11	0	6	0	17	3	433	8	0	444	5	0	3	0	8	0	936	8	0	944	1413
Cars Exiting Leg	11					924					8					426					1369
Heavy Exiting Leg	0					23					0					21					44
Total Exiting Leg	11					947					8					447					1413

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Cars

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	1	0	1	0	2	1	83	2	0	86	2	0	1	0	3	0	165	2	0	167	258
7:15 AM	1	0	1	0	2	1	86	3	0	90	1	0	1	0	2	0	184	1	0	185	279
7:30 AM	0	0	1	0	1	2	93	2	0	97	1	0	0	0	1	0	212	2	0	214	313
7:45 AM	4	0	1	0	5	1	86	1	0	88	2	0	1	0	3	0	253	2	0	255	351
Total	6	0	4	0	10	5	348	8	0	361	6	0	3	0	9	0	814	7	0	821	1201
8:00 AM	5	0	1	0	6	0	115	2	0	117	0	0	0	0	0	0	218	1	0	219	342
8:15 AM	1	0	3	0	4	0	120	3	0	123	2	0	1	0	3	0	230	3	0	233	363
8:30 AM	1	0	3	1	5	0	113	5	0	118	3	0	1	0	4	0	179	0	0	179	306
8:45 AM	3	0	0	0	3	1	138	7	0	146	4	0	1	0	5	1	196	1	0	198	352
Total	10	0	7	1	18	1	486	17	0	504	9	0	3	0	12	1	823	5	0	829	1363
Grand Total	16	0	11	1	28	6	834	25	0	865	15	0	6	0	21	1	1637	12	0	1650	2564
Approach %	57.1	0.0	39.3	3.6		0.7	96.4	2.9	0.0		71.4	0.0	28.6	0.0		0.1	99.2	0.7	0.0		
Total %	0.6	0.0	0.4	0.0	1.1	0.2	32.5	1.0	0.0	33.7	0.6	0.0	0.2	0.0	0.8	0.0	63.8	0.5	0.0	64.4	
Exiting Leg Total	19					1663					26					856					2564

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:30 AM	0	0	1	0	1	2	93	2	0	97	1	0	0	0	1	0	212	2	0	214	313
7:45 AM	4	0	1	0	5	1	86	1	0	88	2	0	1	0	3	0	253	2	0	255	351
8:00 AM	5	0	1	0	6	0	115	2	0	117	0	0	0	0	0	0	218	1	0	219	342
8:15 AM	1	0	3	0	4	0	120	3	0	123	2	0	1	0	3	0	230	3	0	233	363
Total Volume	10	0	6	0	16	3	414	8	0	425	5	0	2	0	7	0	913	8	0	921	1369
% Approach Total	62.5	0.0	37.5	0.0		0.7	97.4	1.9	0.0		71.4	0.0	28.6	0.0		0.0	99.1	0.9	0.0		
PHF	0.500	0.000	0.500	0.000	0.667	0.375	0.863	0.667	0.000	0.864	0.625	0.000	0.500	0.000	0.583	0.000	0.902	0.667	0.000	0.903	0.943
Entering Leg	10	0	6	0	16	3	414	8	0	425	5	0	2	0	7	0	913	8	0	921	1369
Exiting Leg	11					924					8					426					1369
Total	27					1349					15					1347					2738

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	3	0	0	3	9
7:15 AM	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	4	0	0	4	15
7:30 AM	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	0	3	0	0	3	12
7:45 AM	0	0	0	0	0	0	4	0	0	4	0	0	1	0	1	0	6	0	0	6	11
Total	0	0	0	0	0	0	30	0	0	30	0	0	1	0	1	0	16	0	0	16	47
8:00 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	7	0	0	7	11
8:15 AM	1	0	0	0	1	0	2	0	0	2	0	0	0	0	0	0	7	0	0	7	10
8:30 AM	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	9	0	0	9	16
8:45 AM	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	8	0	0	8	13
Total	1	0	0	0	1	0	18	0	0	18	0	0	0	0	0	0	31	0	0	31	50
Grand Total	1	0	0	0	1	0	48	0	0	48	0	0	1	0	1	0	47	0	0	47	97
Approach %	100.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	100.0	0.0		0.0	100.0	0.0	0.0		
Total %	1.0	0.0	0.0	0.0	1.0	0.0	49.5	0.0	0.0	49.5	0.0	0.0	1.0	0.0	1.0	0.0	48.5	0.0	0.0	48.5	
Exiting Leg Total	0					47					0					50					97
Buses	1	0	0	0	1	0	22	0	0	22	0	0	1	0	1	0	26	0	0	26	50
% Buses	100.0	0.0	0.0	0.0	100.0	0.0	45.8	0.0	0.0	45.8	0.0	0.0	100.0	0.0	100.0	0.0	55.3	0.0	0.0	55.3	51.5
Exiting Leg Total	0					26					0					24					50
Single-Unit Trucks	0	0	0	0	0	0	19	0	0	19	0	0	0	0	0	0	17	0	0	17	36
% Single-Unit	0.0	0.0	0.0	0.0	0.0	0.0	39.6	0.0	0.0	39.6	0.0	0.0	0.0	0.0	0.0	0.0	36.2	0.0	0.0	36.2	37.1
Exiting Leg Total	0					17					0					19					36
Articulated Trucks	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	4	0	0	4	11
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	14.6	0.0	0.0	14.6	0.0	0.0	0.0	0.0	0.0	0.0	8.5	0.0	0.0	8.5	11.3
Exiting Leg Total	0					4					0					7					11

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

8:00 AM	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
8:00 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	7	0	0	7	11
8:15 AM	1	0	0	0	1	0	2	0	0	2	0	0	0	0	0	0	7	0	0	7	10
8:30 AM	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	9	0	0	9	16
8:45 AM	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	8	0	0	8	13
Total Volume	1	0	0	0	1	0	18	0	0	18	0	0	0	0	0	0	31	0	0	31	50
% Approach Total	100.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.250	0.000	0.000	0.000	0.250	0.000	0.643	0.000	0.000	0.643	0.000	0.000	0.000	0.000	0.000	0.000	0.861	0.000	0.000	0.861	0.781
Buses	1	0	0	0	1	0	8	0	0	8	0	0	0	0	0	0	16	0	0	16	25
Buses %	100.0	0.0	0.0	0.0	100.0	0.0	44.4	0.0	0.0	44.4	0.0	0.0	0.0	0.0	0.0	0.0	51.6	0.0	0.0	51.6	50.0
Single-Unit Trucks	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	0	11	0	0	11	19
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	44.4	0.0	0.0	44.4	0.0	0.0	0.0	0.0	0.0	0.0	35.5	0.0	0.0	35.5	38.0
Articulated Trucks	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	6
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	11.1	0.0	0.0	11.1	0.0	0.0	0.0	0.0	0.0	0.0	12.9	0.0	0.0	12.9	12.0
Buses	1	0	0	0	1	0	8	0	0	8	0	0	0	0	0	0	16	0	0	16	25
Single-Unit Trucks	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	0	11	0	0	11	19
Articulated Trucks	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	6
Total Entering Leg	1	0	0	0	1	0	18	0	0	18	0	0	0	0	0	0	31	0	0	31	50
Buses	0					16					0					9					25
Single-Unit Trucks	0					11					0					8					19
Articulated Trucks	0					4					0					2					6
Total Exiting Leg	0					31					0					19					50

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Buses

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	2	0	0	2	6
7:15 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	3	0	0	3	5
7:30 AM	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	3	0	0	3	9
7:45 AM	0	0	0	0	0	0	2	0	0	2	0	0	1	0	1	0	2	0	0	2	5
Total	0	0	0	0	0	0	14	0	0	14	0	0	1	0	1	0	10	0	0	10	25
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	4	4
8:15 AM	1	0	0	0	1	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	4
8:30 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	8	0	0	8	12
8:45 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	2	0	0	2	5
Total	1	0	0	0	1	0	8	0	0	8	0	0	0	0	0	0	16	0	0	16	25
Grand Total	1	0	0	0	1	0	22	0	0	22	0	0	1	0	1	0	26	0	0	26	50
Approach %	100.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	100.0	0.0		0.0	100.0	0.0	0.0		
Total %	2.0	0.0	0.0	0.0	2.0	0.0	44.0	0.0	0.0	44.0	0.0	0.0	2.0	0.0	2.0	0.0	52.0	0.0	0.0	52.0	
Exiting Leg Total	0					26					0					24					50

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	2	0	0	2	6
7:15 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	3	0	0	3	5
7:30 AM	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	3	0	0	3	9
7:45 AM	0	0	0	0	0	0	2	0	0	2	0	0	1	0	1	0	2	0	0	2	5
Total Volume	0	0	0	0	0	0	14	0	0	14	0	0	1	0	1	0	10	0	0	10	25
% Approach Total	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	100.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.583	0.000	0.000	0.583	0.000	0.000	0.250	0.000	0.250	0.000	0.833	0.000	0.000	0.833	0.694
Entering Leg	0					14					1					10					25
Exiting Leg	0					10					0					15					25
Total	0					24					1					25					50

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Single-Unit Trucks

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	3
7:15 AM	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	1	0	0	1	6
7:30 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	2
7:45 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	6
Total	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	6	0	0	6	17
8:00 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	4
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	4	4
8:30 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	3
8:45 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	6	0	0	6	8
Total	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	0	11	0	0	11	19
Grand Total	0	0	0	0	0	0	19	0	0	19	0	0	0	0	0	0	17	0	0	17	36
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	52.8	0.0	0.0	52.8	0.0	0.0	0.0	0.0	0.0	0.0	47.2	0.0	0.0	47.2	
Exiting Leg Total	0					17					0					19					36

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
8:00 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	4
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	4	4
8:30 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	3
8:45 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	6	0	0	6	8
Total Volume	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	0	11	0	0	11	19
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.667	0.000	0.000	0.667	0.000	0.000	0.000	0.000	0.000	0.000	0.458	0.000	0.000	0.458	0.594
Entering Leg	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	0	11	0	0	11	19
Exiting Leg	0					11					0					8					19
Total	0					19					0					19					38

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Articulated Trucks

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	0	0	0	0	0	4
7:30 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	0	0	0	0	0	5
8:00 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	0	2	3
8:15 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	0	1	2
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	4	0	0	0	4	6
Grand Total	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	4	0	0	0	4	11
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0
Total %	0.0	0.0	0.0	0.0	0.0	0.0	63.6	0.0	0.0	63.6	0.0	0.0	0.0	0.0	0.0	0.0	36.4	0.0	0.0	0.0	0.0	36.4
Exiting Leg Total	0					4					0					7					11	

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
7:15 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	0	0	0	0	0	4
7:30 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	0	2	3
Total Volume	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	2	0	0	0	2	8
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.375	0.000	0.000	0.375	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.250	0.500	
Entering Leg	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	2	0	0	0	2	8
Exiting Leg	0					2					0					6					8	
Total	0					8					0					8					16	

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Bicycles (on Roadway and Crosswalks)

	Fairfield Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2		
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	3		
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	3	3		
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	
Exiting Leg Total	0							3							0							0							3

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

7:30 AM	Fairfield Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1		
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	2		
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	3	3		
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.375	0.000	0.375		
Entering Leg	0							0							0							0							3
Exiting Leg	0							3							0							0							3
Total	0							3							0							0							6

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class: **Pedestrians**



Pedestrians

	Fairfield Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total						
	from North							from East							from South							from West													
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total							
7:00 AM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
7:15 AM	0	0	0	0	0	4	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
7:30 AM	0	0	0	0	4	2	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6
7:45 AM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	0	0	6	6	12	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	2	5	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7
8:30 AM	0	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
8:45 AM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	2	2
Total	0	0	0	0	4	6	10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	11	
Grand Total	0	0	0	0	10	12	22	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	23	
Approach %	0	0	0	0	45.5	54.5		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	100	0			
Total %	0	0	0	0	43.5	52.2	95.7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4.35	0	4.35		
Exiting Leg Total	22							0							0							1							23						

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Fairfield Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total						
	from North							from East							from South							from West													
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total							
7:30 AM	0	0	0	0	4	2	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6
7:45 AM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	2	5	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7
Total Volume	0	0	0	0	7	7	14	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14
% Approach Total	0.0	0.0	0.0	0.0	50.0	50.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.438	0.350	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.500	
Entering Leg	0	0	0	0	7	7	14	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	
Exiting Leg	14							0							0							0							14						
Total	28							0							0							0							28						

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars and Heavy Vehicles (Combined)

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	7	0	0	0	7	1	245	9	0	255	6	0	0	0	6	3	173	0	0	176	444
4:15 PM	4	0	2	0	6	1	223	3	0	227	7	0	3	2	12	1	160	1	0	162	407
4:30 PM	3	1	2	0	6	2	194	6	0	202	5	0	1	0	6	1	153	1	0	155	369
4:45 PM	1	0	0	0	1	1	230	6	0	237	6	0	0	0	6	1	152	3	0	156	400
Total	15	1	4	0	20	5	892	24	0	921	24	0	4	2	30	6	638	5	0	649	1620
5:00 PM	1	1	0	0	2	3	184	4	1	192	3	0	0	0	3	0	171	1	0	172	369
5:15 PM	0	1	1	0	2	2	215	2	0	219	3	0	0	0	3	2	150	3	0	155	379
5:30 PM	5	0	1	0	6	3	184	6	0	193	4	0	2	0	6	1	158	2	1	162	367
5:45 PM	2	0	2	0	4	0	196	1	0	197	3	0	0	0	3	1	146	1	0	148	352
Total	8	2	4	0	14	8	779	13	1	801	13	0	2	0	15	4	625	7	1	637	1467
Grand Total	23	3	8	0	34	13	1671	37	1	1722	37	0	6	2	45	10	1263	12	1	1286	3087
Approach %	67.6	8.8	23.5	0.0		0.8	97.0	2.1	0.1		82.2	0.0	13.3	4.4		0.8	98.2	0.9	0.1		
Total %	0.7	0.1	0.3	0.0	1.1	0.4	54.1	1.2	0.0	55.8	1.2	0.0	0.2	0.1	1.5	0.3	40.9	0.4	0.0	41.7	
Exiting Leg Total	25					1309					52					1701					3087
Cars	23	3	8	0	34	12	1651	37	1	1701	37	0	5	1	43	9	1238	12	1	1260	3038
% Cars	100.0	100.0	100.0	0.0	100.0	92.3	98.8	100.0	100.0	98.8	100.0	0.0	83.3	50.0	95.6	90.0	98.0	100.0	100.0	98.0	98.4
Exiting Leg Total	24					1284					50					1680					3038
Heavy Vehicles	0	0	0	0	0	1	20	0	0	21	0	0	1	1	2	1	25	0	0	26	49
% Heavy Vehicles	0.0	0.0	0.0	0.0	0.0	7.7	1.2	0.0	0.0	1.2	0.0	0.0	16.7	50.0	4.4	10.0	2.0	0.0	0.0	2.0	1.6
Exiting Leg Total	1					25					2					21					49

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	7	0	0	0	7	1	245	9	0	255	6	0	0	0	6	3	173	0	0	176	444
4:15 PM	4	0	2	0	6	1	223	3	0	227	7	0	3	2	12	1	160	1	0	162	407
4:30 PM	3	1	2	0	6	2	194	6	0	202	5	0	1	0	6	1	153	1	0	155	369
4:45 PM	1	0	0	0	1	1	230	6	0	237	6	0	0	0	6	1	152	3	0	156	400
Total Volume	15	1	4	0	20	5	892	24	0	921	24	0	4	2	30	6	638	5	0	649	1620
% Approach Total	75.0	5.0	20.0	0.0		0.5	96.9	2.6	0.0		80.0	0.0	13.3	6.7		0.9	98.3	0.8	0.0		
PHF	0.536	0.250	0.500	0.000	0.714	0.625	0.910	0.667	0.000	0.903	0.857	0.000	0.333	0.250	0.625	0.500	0.922	0.417	0.000	0.922	0.912
Cars	15	1	4	0	20	5	881	24	0	910	24	0	3	1	28	5	624	5	0	634	1592
Cars %	100.0	100.0	100.0	0.0	100.0	100.0	98.8	100.0	0.0	98.8	100.0	0.0	75.0	50.0	93.3	83.3	97.8	100.0	0.0	97.7	98.3
Heavy Vehicles	0	0	0	0	0	0	11	0	0	11	0	0	1	1	2	1	14	0	0	15	28
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	1.2	0.0	0.0	1.2	0.0	0.0	25.0	50.0	6.7	16.7	2.2	0.0	0.0	2.3	1.7
Cars Enter Leg	15	1	4	0	20	5	881	24	0	910	24	0	3	1	28	5	624	5	0	634	1592
Heavy Enter Leg	0	0	0	0	0	0	11	0	0	11	0	0	1	1	2	1	14	0	0	15	28
Total Entering Leg	15	1	4	0	20	5	892	24	0	921	24	0	4	2	30	6	638	5	0	649	1620
Cars Exiting Leg	10					652					31					899					1592
Heavy Exiting Leg	0					14					2					12					28
Total Exiting Leg	10					666					33					911					1620

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	7	0	0	0	7	1	241	9	0	251	6	0	0	0	6	2	169	0	0	171	435
4:15 PM	4	0	2	0	6	1	218	3	0	222	7	0	2	1	10	1	157	1	0	159	397
4:30 PM	3	1	2	0	6	2	193	6	0	201	5	0	1	0	6	1	149	1	0	151	364
4:45 PM	1	0	0	0	1	1	229	6	0	236	6	0	0	0	6	1	149	3	0	153	396
Total	15	1	4	0	20	5	881	24	0	910	24	0	3	1	28	5	624	5	0	634	1592
5:00 PM	1	1	0	0	2	3	180	4	1	188	3	0	0	0	3	0	167	1	0	168	361
5:15 PM	0	1	1	0	2	2	212	2	0	216	3	0	0	0	3	2	146	3	0	151	372
5:30 PM	5	0	1	0	6	2	182	6	0	190	4	0	2	0	6	1	156	2	1	160	362
5:45 PM	2	0	2	0	4	0	196	1	0	197	3	0	0	0	3	1	145	1	0	147	351
Total	8	2	4	0	14	7	770	13	1	791	13	0	2	0	15	4	614	7	1	626	1446
Grand Total	23	3	8	0	34	12	1651	37	1	1701	37	0	5	1	43	9	1238	12	1	1260	3038
Approach %	67.6	8.8	23.5	0.0		0.7	97.1	2.2	0.1		86.0	0.0	11.6	2.3		0.7	98.3	1.0	0.1		
Total %	0.8	0.1	0.3	0.0	1.1	0.4	54.3	1.2	0.0	56.0	1.2	0.0	0.2	0.0	1.4	0.3	40.8	0.4	0.0	41.5	
Exiting Leg Total	24					1284					50					1680					3038

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	7	0	0	0	7	1	241	9	0	251	6	0	0	0	6	2	169	0	0	171	435
4:15 PM	4	0	2	0	6	1	218	3	0	222	7	0	2	1	10	1	157	1	0	159	397
4:30 PM	3	1	2	0	6	2	193	6	0	201	5	0	1	0	6	1	149	1	0	151	364
4:45 PM	1	0	0	0	1	1	229	6	0	236	6	0	0	0	6	1	149	3	0	153	396
Total Volume	15	1	4	0	20	5	881	24	0	910	24	0	3	1	28	5	624	5	0	634	1592
% Approach Total	75.0	5.0	20.0	0.0		0.5	96.8	2.6	0.0		85.7	0.0	10.7	3.6		0.8	98.4	0.8	0.0		
PHF	0.536	0.250	0.500	0.000	0.714	0.625	0.914	0.667	0.000	0.906	0.857	0.000	0.375	0.250	0.700	0.625	0.923	0.417	0.000	0.927	0.915
Entering Leg	15	1	4	0	20	5	881	24	0	910	24	0	3	1	28	5	624	5	0	634	1592
Exiting Leg						10					652					31					899
Total	30					1562					59					1533					3184

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	1	4	0	0	5	9
4:15 PM	0	0	0	0	0	0	5	0	0	5	0	0	1	1	2	0	3	0	0	3	10
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	5
4:45 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	4
Total	0	0	0	0	0	0	11	0	0	11	0	0	1	1	2	1	14	0	0	15	28
5:00 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	4	0	0	4	8
5:15 PM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	4	0	0	4	7
5:30 PM	0	0	0	0	0	1	2	0	0	3	0	0	0	0	0	0	2	0	0	2	5
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
Total	0	0	0	0	0	1	9	0	0	10	0	0	0	0	0	0	11	0	0	11	21
Grand Total	0	0	0	0	0	1	20	0	0	21	0	0	1	1	2	1	25	0	0	26	49
Approach %	0.0	0.0	0.0	0.0	0.0	4.8	95.2	0.0	0.0	42.9	0.0	0.0	50.0	50.0	100.0	3.8	96.2	0.0	0.0	53.1	
Total %	0.0	0.0	0.0	0.0	0.0	2.0	40.8	0.0	0.0	42.9	0.0	0.0	2.0	2.0	4.1	2.0	51.0	0.0	0.0	53.1	
Exiting Leg Total	1					25					2					21					49
Buses	0	0	0	0	0	0	8	0	0	8	0	0	1	1	2	1	12	0	0	13	23
% Buses	0.0	0.0	0.0	0.0	0.0	0.0	40.0	0.0	0.0	38.1	0.0	0.0	100.0	100.0	100.0	100.0	48.0	0.0	0.0	50.0	46.9
Exiting Leg Total	0					12					2					9					23
Single-Unit Trucks	0	0	0	0	0	1	8	0	0	9	0	0	0	0	0	0	13	0	0	13	22
% Single-Unit	0.0	0.0	0.0	0.0	0.0	100.0	40.0	0.0	0.0	42.9	0.0	0.0	0.0	0.0	0.0	0.0	52.0	0.0	0.0	50.0	44.9
Exiting Leg Total	1					13					0					8					22
Articulated Trucks	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	0	0	0	0	4
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	20.0	0.0	0.0	19.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	8.2
Exiting Leg Total	0					0					0					4					4

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:00 PM	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	1	4	0	0	5	9
4:15 PM	0	0	0	0	0	0	5	0	0	5	0	0	1	1	2	0	3	0	0	3	10
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	5
4:45 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	4
Total Volume	0	0	0	0	0	0	11	0	0	11	0	0	1	1	2	1	14	0	0	15	28
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	42.9	0.0	0.0	50.0	50.0	100.0	6.7	93.3	0.0	0.0	53.1	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.550	0.000	0.000	0.550	0.000	0.000	0.250	0.250	0.250	0.250	0.875	0.000	0.000	0.750	0.700
Buses	0	0	0	0	0	0	5	0	0	5	0	0	1	1	2	1	8	0	0	9	16
Buses %	0.0	0.0	0.0	0.0	0.0	0.0	45.5	0.0	0.0	45.5	0.0	0.0	100.0	100.0	100.0	100.0	57.1	0.0	0.0	60.0	57.1
Single-Unit Trucks	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	6	0	0	6	11
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	45.5	0.0	0.0	45.5	0.0	0.0	0.0	0.0	0.0	0.0	42.9	0.0	0.0	40.0	39.3
Articulated Trucks	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	9.1	0.0	0.0	9.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	3.6
Buses	0	0	0	0	0	0	5	0	0	5	0	0	1	1	2	1	8	0	0	9	16
Single-Unit Trucks	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	6	0	0	6	11
Articulated Trucks	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Total Entering Leg	0	0	0	0	0	0	11	0	0	11	0	0	1	1	2	1	14	0	0	15	28
Buses	0					8					2					6					16
Single-Unit Trucks	0					6					0					5					11
Articulated Trucks	0					0					0					1					1
Total Exiting Leg	0					14					2					12					28

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Buses

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	1	2	0	0	3	7
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	1	2	0	2	0	0	2	4
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	3	3
4:45 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
Total	0	0	0	0	0	0	5	0	0	5	0	0	1	1	2	1	8	0	0	9	16
5:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
5:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
5:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	4	0	0	4	7
Grand Total	0	0	0	0	0	0	8	0	0	8	0	0	1	1	2	1	12	0	0	13	23
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	50.0	50.0	0.0	7.7	92.3	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	34.8	0.0	0.0	34.8	0.0	0.0	4.3	4.3	8.7	4.3	52.2	0.0	0.0	56.5	
Exiting Leg Total	0					12					2					9					23

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	1	2	0	0	3	7
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	1	2	0	2	0	0	2	4
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	3	3
4:45 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
Total Volume	0	0	0	0	0	0	5	0	0	5	0	0	1	1	2	1	8	0	0	9	16
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	50.0	50.0	0.0	11.1	88.9	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.313	0.000	0.000	0.313	0.000	0.000	0.250	0.250	0.250	0.250	0.667	0.000	0.000	0.750	0.571
Entering Leg	0	0	0	0	0	0	5	0	0	5	0	0	1	1	2	1	8	0	0	9	16
Exiting Leg	0					8					2					6					16
Total	0					13					4					15					32

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Single-Unit Trucks

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	2
4:15 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	1	0	0	1	5
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	2
Total	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	6	0	0	6	11
5:00 PM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	3	0	0	3	5
5:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	4
5:30 PM	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
Total	0	0	0	0	0	1	3	0	0	4	0	0	0	0	0	0	7	0	0	7	11
Grand Total	0	0	0	0	0	1	8	0	0	9	0	0	0	0	0	0	13	0	0	13	22
Approach %	0.0	0.0	0.0	0.0	0.0	11.1	88.9	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	4.5	36.4	0.0	0.0	40.9	0.0	0.0	0.0	0.0	0.0	0.0	59.1	0.0	0.0	59.1	
Exiting Leg Total	1					13					0					8					22

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:15 PM	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:15 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	1	0	0	1	5
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	2
5:00 PM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	3	0	0	3	5
Total Volume	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	7	0	0	7	14
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.438	0.000	0.000	0.438	0.000	0.000	0.000	0.000	0.000	0.000	0.583	0.000	0.000	0.583	0.700
Entering Leg	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	7	0	0	7	14
Exiting Leg	0					7					0					7					14
Total	0					14					0					14					28

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Articulated Trucks

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
5:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
5:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	3
Grand Total	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	0	0	0	0	4
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total %	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					4					4

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Fairfield Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
5:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Total Volume	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	3
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.750	0.000	0.000	0.750	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.750
Entering Leg	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	3
Exiting Leg	0					0					0					3					3
Total	0					3					0					3					6

PDI File #: 249915 C
 Location: N: Fairfield Street S: Shirley Street
 Location: E: Park Avenue (Route 9) W: Park Avenue (Route 9)
 City, State: Worcester, MA
 Client: GIA/A. Allen
 Site Code: TBD
 Count Date: Wednesday, March 20, 2024
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



Bicycles (on Roadway and Crosswalks)

	Fairfield Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total							
	from North							from East							from South							from West														
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total								
4:00 PM	0	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
4:15 PM	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	3	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
Total	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
Grand Total	0	0	0	0	3	3	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6
Approach %	0.0	0.0	0.0	0.0	50.0	50.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0					
Total %	0.0	0.0	0.0	0.0	50.0	50.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0					
Exiting Leg Total	6							0							0							6														

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Fairfield Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total							
	from North							from East							from South							from West														
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total								
4:00 PM	0	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
4:15 PM	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	3	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
% Approach Total	0.0	0.0	0.0	0.0	75.0	25.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0					
PHF	0.000	0.000	0.000	0.000	0.375	0.250	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.500				
Entering Leg	0	0	0	0	3	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
Exiting Leg	4							0							0							4														
Total	8							0							0							8														

PDI File #: **249915 C**
 Location: **N: Fairfield Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class: **Pedestrians**



Pedestrians

	Fairfield Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	2	1	3	0	0	0	0	0	1	1	7
4:15 PM	0	0	0	0	2	1	3	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	4
4:30 PM	0	0	0	0	1	3	4	0	0	0	0	0	0	0	0	0	0	0	3	2	5	0	0	0	0	0	0	0	9
4:45 PM	0	0	0	0	8	3	11	0	0	0	0	0	0	0	0	0	0	0	2	2	4	0	0	0	0	0	1	1	16
Total	0	0	0	0	12	9	21	0	0	0	0	0	0	0	0	0	0	0	8	5	13	0	0	0	0	0	2	2	36
5:00 PM	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	2
5:15 PM	0	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
5:30 PM	0	0	0	0	3	1	4	0	0	0	0	0	0	0	0	0	0	0	2	1	3	0	0	0	0	0	0	0	7
5:45 PM	0	0	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	1	3	4	0	0	0	0	0	0	0	7
Total	0	0	0	0	5	5	10	0	0	0	0	0	0	0	0	0	0	0	3	5	8	0	0	0	0	0	0	0	18
Grand Total	0	0	0	0	17	14	31	0	0	0	0	0	0	0	0	0	0	0	11	10	21	0	0	0	0	0	2	2	54
Approach %	0	0	0	0	54.8	45.2		0	0	0	0	0	0	0	0	0	0	0	52.4	47.6		0	0	0	0	0	100		
Total %	0	0	0	0	31.5	25.9	57.4	0	0	0	0	0	0	0	0	0	0	0	20.4	18.5	38.9	0	0	0	0	0	3.7	3.7	
Exiting Leg Total	31							0							21							2							54

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Fairfield Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	2	1	3	0	0	0	0	0	1	1	7
4:15 PM	0	0	0	0	2	1	3	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	4
4:30 PM	0	0	0	0	1	3	4	0	0	0	0	0	0	0	0	0	0	0	3	2	5	0	0	0	0	0	0	0	9
4:45 PM	0	0	0	0	8	3	11	0	0	0	0	0	0	0	0	0	0	0	2	2	4	0	0	0	0	0	1	1	16
Total Volume	0	0	0	0	12	9	21	0	0	0	0	0	0	0	0	0	0	0	8	5	13	0	0	0	0	0	2	2	36
% Approach Total	0.0	0.0	0.0	0.0	57.1	42.9		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	61.5	38.5		0.0	0.0	0.0	0.0	0.0	100.0		
PHF	0.000	0.000	0.000	0.000	0.375	0.750	0.477	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.667	0.625	0.650	0.000	0.000	0.000	0.000	0.000	0.500	0.500	0.563
Entering Leg	0	0	0	0	12	9	21	0	0	0	0	0	0	0	0	0	0	0	8	5	13	0	0	0	0	0	2	2	36
Exiting Leg	21							0							13							2							36
Total	42							0							26							4							72

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Cars and Heavy Vehicles (Combined)

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	1	1	0	2	0	0	0	0	0	1	3	0	0	4	0	0	0	0	0	6
7:15 AM	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	4
7:30 AM	0	4	0	0	4	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	5
7:45 AM	0	2	1	0	3	1	0	0	0	1	0	3	0	0	3	0	0	0	0	0	7
Total	0	10	2	0	12	1	0	0	0	1	1	8	0	0	9	0	0	0	0	0	22
8:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	2
8:15 AM	0	4	0	0	4	0	0	0	0	0	1	3	0	0	4	0	0	0	0	0	8
8:30 AM	0	5	0	0	5	1	0	0	0	1	0	3	0	0	3	0	0	0	0	0	9
8:45 AM	0	8	1	0	9	2	0	0	0	2	0	3	0	0	3	0	0	0	0	0	14
Total	0	18	1	0	19	3	0	0	0	3	1	9	0	1	11	0	0	0	0	0	33
Grand Total	0	28	3	0	31	4	0	0	0	4	2	17	0	1	20	0	0	0	0	0	55
Approach %	0.0	90.3	9.7	0.0		100.0	0.0	0.0	0.0		10.0	85.0	0.0	5.0		0.0	0.0	0.0	0.0		
Total %	0.0	50.9	5.5	0.0	56.4	7.3	0.0	0.0	0.0	7.3	3.6	30.9	0.0	1.8	36.4	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	21					5					29					0					55
Cars	0	28	3	0	31	4	0	0	0	4	2	16	0	1	19	0	0	0	0	0	54
% Cars	0.0	100.0	100.0	0.0	100.0	100.0	0.0	0.0	0.0	100.0	100.0	94.1	0.0	100.0	95.0	0.0	0.0	0.0	0.0	0.0	98.2
Exiting Leg Total	20					5					29					0					54
Heavy Vehicles	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
% Heavy Vehicles	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	5.9	0.0	0.0	5.0	0.0	0.0	0.0	0.0	0.0	1.8
Exiting Leg Total	1					0					0					0					1

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
8:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	2
8:15 AM	0	4	0	0	4	0	0	0	0	0	1	3	0	0	4	0	0	0	0	0	8
8:30 AM	0	5	0	0	5	1	0	0	0	1	0	3	0	0	3	0	0	0	0	0	9
8:45 AM	0	8	1	0	9	2	0	0	0	2	0	3	0	0	3	0	0	0	0	0	14
Total Volume	0	18	1	0	19	3	0	0	0	3	1	9	0	1	11	0	0	0	0	0	33
% Approach Total	0.0	94.7	5.3	0.0		100.0	0.0	0.0	0.0		9.1	81.8	0.0	9.1		0.0	0.0	0.0	0.0		
PHF	0.000	0.563	0.250	0.000	0.528	0.375	0.000	0.000	0.000	0.375	0.250	0.750	0.000	0.250	0.688	0.000	0.000	0.000	0.000	0.000	0.589
Cars	0	18	1	0	19	3	0	0	0	3	1	9	0	1	11	0	0	0	0	0	33
Cars %	0.0	100.0	100.0	0.0	100.0	100.0	0.0	0.0	0.0	100.0	100.0	100.0	0.0	100.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0
Heavy Vehicles	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Cars Enter Leg	0	18	1	0	19	3	0	0	0	3	1	9	0	1	11	0	0	0	0	0	33
Heavy Enter Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	0	18	1	0	19	3	0	0	0	3	1	9	0	1	11	0	0	0	0	0	33
Cars Exiting Leg	12					2					19					0					33
Heavy Exiting Leg	0					0					0					0					0
Total Exiting Leg	12					2					19					0					33

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Cars

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
7:00 AM	0	1	1	0	2	0	0	0	0	0	1	3	0	0	4	0	0	0	0	0	0	0	0	0	0	6
7:15 AM	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	4
7:30 AM	0	4	0	0	4	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	5
7:45 AM	0	2	1	0	3	1	0	0	0	1	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	6
Total	0	10	2	0	12	1	0	0	0	1	1	7	0	0	8	0	0	0	0	0	0	0	0	0	0	21
8:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2
8:15 AM	0	4	0	0	4	0	0	0	0	0	1	3	0	0	4	0	0	0	0	0	0	0	0	0	0	8
8:30 AM	0	5	0	0	5	1	0	0	0	1	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	9
8:45 AM	0	8	1	0	9	2	0	0	0	2	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	14
Total	0	18	1	0	19	3	0	0	0	3	1	9	0	1	11	0	0	0	0	0	0	0	0	0	0	33
Grand Total	0	28	3	0	31	4	0	0	0	4	2	16	0	1	19	0	0	0	0	0	0	0	0	0	0	54
Approach %	0.0	90.3	9.7	0.0		100.0	0.0	0.0	0.0		10.5	84.2	0.0	5.3		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	51.9	5.6	0.0	57.4	7.4	0.0	0.0	0.0	7.4	3.7	29.6	0.0	1.9	35.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total						20					5					29					0	54				

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
8:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2
8:15 AM	0	4	0	0	4	0	0	0	0	0	1	3	0	0	4	0	0	0	0	0	0	0	0	0	0	8
8:30 AM	0	5	0	0	5	1	0	0	0	1	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	9
8:45 AM	0	8	1	0	9	2	0	0	0	2	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	14
Total Volume	0	18	1	0	19	3	0	0	0	3	1	9	0	1	11	0	0	0	0	0	0	0	0	0	0	33
% Approach Total	0.0	94.7	5.3	0.0		100.0	0.0	0.0	0.0		9.1	81.8	0.0	9.1		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.563	0.250	0.000	0.528	0.375	0.000	0.000	0.000	0.375	0.250	0.750	0.000	0.250	0.688	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.589	
Entering Leg	0	18	1	0	19	3	0	0	0	3	1	9	0	1	11	0	0	0	0	0	0	0	0	0	0	33
Exiting Leg						12					2					19					0	33				
Total						31					5					30					0	66				

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	1					0					0					0					1					
Buses	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
% Buses	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0
Exiting Leg Total	1					0					0					0					1					
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Single-Unit	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0					
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0					

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Total Volume	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.250
Buses	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Buses %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Buses	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	0					0					1					0					1					
Buses	1					0					0					0					1					
Single-Unit Trucks	0					0					0					0					0					
Articulated Trucks	0					0					0					0					0					
Total Exiting Leg	1					0					0					0					1					

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Buses

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1
Total	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	1					0					0					0					1	

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1
Total Volume	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.250
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1
Exiting Leg	1					0					0					0					1	
Total	1					0					1					0					2	

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Single-Unit Trucks

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0	

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0					0					0					0					0	
Total	0					0					0					0					0	

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Articulated Trucks

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0			
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0	

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0					0					0					0					0	
Total	0					0					0					0					0	

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Bicycles (on Roadway and Crosswalks)

	Shirley Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	0							0							0							0							0

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Shirley Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Exiting Leg	0							0							0							0							0
Total	0							0							0							0							0

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class: **Pedestrians**



Pedestrians

	Shirley Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
7:30 AM	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	2	
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
8:45 AM	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1	0	1	0	0	0	0	0	0	0	2	
Total	0	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0	1	0	1	0	0	0	0	2	0	2	5	
Grand Total	0	0	0	0	0	1	1	0	0	0	0	1	1	2	0	0	0	0	1	0	1	0	0	0	2	0	2	6	
Approach %	0	0	0	0	0	100		0	0	0	0	50	50		0	0	0	0	100	0		0	0	0	100	0			
Total %	0	0	0	0	0	16.7	16.7	0	0	0	0	16.7	16.7	33.3	0	0	0	0	16.7	0	16.7	0	0	0	33.3	0	33.3		
Exiting Leg Total	1							2							1							2							6

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

8:00 AM	Shirley Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	2		
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
8:45 AM	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1	0	1	0	0	0	0	0	0	0	2	
Total Volume	0	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0	1	0	1	0	0	0	0	2	0	2	5	
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	50.0	50.0		0.0	0.0	0.0	0.0	100.0	0.0		0.0	0.0	0.0	100.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.250	0.500	0.000	0.000	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.000	0.250	0.000	0.250	0.625	
Entering Leg	0	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0	1	0	1	0	0	0	0	2	0	2	5	
Exiting Leg	0							2							1							2							5
Total	0							4							2							4							10

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars and Heavy Vehicles (Combined)

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	13	1	0	14	1	0	0	0	1	0	6	0	0	6	0	0	0	0	0	21
4:15 PM	1	3	0	0	4	1	0	1	0	2	2	9	0	0	11	0	0	0	0	0	17
4:30 PM	1	5	2	0	8	1	0	0	0	1	0	5	0	0	5	0	0	0	0	0	14
4:45 PM	0	5	2	0	7	1	0	0	0	1	0	5	0	0	5	0	0	2	0	2	15
Total	2	26	5	0	33	4	0	1	0	5	2	25	0	0	27	0	0	2	0	2	67
5:00 PM	0	3	1	1	5	0	0	3	0	3	1	2	0	0	3	0	0	0	0	0	11
5:15 PM	0	5	0	0	5	1	0	0	0	1	1	2	0	0	3	0	0	0	0	0	9
5:30 PM	1	4	1	1	7	1	0	0	0	1	0	4	0	0	4	0	0	0	0	0	12
5:45 PM	0	2	0	0	2	0	0	2	0	2	0	3	1	0	4	0	0	0	0	0	8
Total	1	14	2	2	19	2	0	5	0	7	2	11	1	0	14	0	0	0	0	0	40
Grand Total	3	40	7	2	52	6	0	6	0	12	4	36	1	0	41	0	0	2	0	2	107
Approach %	5.8	76.9	13.5	3.8		50.0	0.0	50.0	0.0		9.8	87.8	2.4	0.0		0.0	0.0	100.0	0.0		
Total %	2.8	37.4	6.5	1.9	48.6	5.6	0.0	5.6	0.0	11.2	3.7	33.6	0.9	0.0	38.3	0.0	0.0	1.9	0.0	1.9	
Exiting Leg Total	46					11					46					4					107
Cars	3	39	7	2	51	6	0	6	0	12	4	35	1	0	40	0	0	2	0	2	105
% Cars	100.0	97.5	100.0	100.0	98.1	100.0	0.0	100.0	0.0	100.0	100.0	97.2	100.0	0.0	97.6	0.0	0.0	100.0	0.0	100.0	98.1
Exiting Leg Total	45					11					45					4					105
Heavy Vehicles	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
% Heavy Vehicles	0.0	2.5	0.0	0.0	1.9	0.0	0.0	0.0	0.0	0.0	0.0	2.8	0.0	0.0	2.4	0.0	0.0	0.0	0.0	0.0	1.9
Exiting Leg Total	1					0					1					0					2

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	13	1	0	14	1	0	0	0	1	0	6	0	0	6	0	0	0	0	0	21
4:15 PM	1	3	0	0	4	1	0	1	0	2	2	9	0	0	11	0	0	0	0	0	17
4:30 PM	1	5	2	0	8	1	0	0	0	1	0	5	0	0	5	0	0	0	0	0	14
4:45 PM	0	5	2	0	7	1	0	0	0	1	0	5	0	0	5	0	0	2	0	2	15
Total Volume	2	26	5	0	33	4	0	1	0	5	2	25	0	0	27	0	0	2	0	2	67
% Approach Total	6.1	78.8	15.2	0.0		80.0	0.0	20.0	0.0		7.4	92.6	0.0	0.0		0.0	0.0	100.0	0.0		
PHF	0.500	0.500	0.625	0.000	0.589	1.000	0.000	0.250	0.000	0.625	0.250	0.694	0.000	0.000	0.614	0.000	0.000	0.250	0.000	0.250	0.798
Cars	2	25	5	0	32	4	0	1	0	5	2	24	0	0	26	0	0	2	0	2	65
Cars %	100.0	96.2	100.0	0.0	97.0	100.0	0.0	100.0	0.0	100.0	100.0	96.0	0.0	0.0	96.3	0.0	0.0	100.0	0.0	100.0	97.0
Heavy Vehicles	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
Heavy Vehicles %	0.0	3.8	0.0	0.0	3.0	0.0	0.0	0.0	0.0	0.0	0.0	4.0	0.0	0.0	3.7	0.0	0.0	0.0	0.0	0.0	3.0
Cars Enter Leg	2	25	5	0	32	4	0	1	0	5	2	24	0	0	26	0	0	2	0	2	65
Heavy Enter Leg	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
Total Entering Leg	2	26	5	0	33	4	0	1	0	5	2	25	0	0	27	0	0	2	0	2	67
Cars Exiting Leg	30					7					26					2					65
Heavy Exiting Leg	1					0					1					0					2
Total Exiting Leg	31					7					27					2					67

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	0	12	1	0	13	1	0	0	0	1	0	6	0	0	6	0	0	0	0	0	0	0	0	0	0	20
4:15 PM	1	3	0	0	4	1	0	1	0	2	2	8	0	0	10	0	0	0	0	0	0	0	0	0	0	16
4:30 PM	1	5	2	0	8	1	0	0	0	1	0	5	0	0	5	0	0	0	0	0	0	0	0	0	0	14
4:45 PM	0	5	2	0	7	1	0	0	0	1	0	5	0	0	5	0	0	2	0	2	0	0	2	0	2	15
Total	2	25	5	0	32	4	0	1	0	5	2	24	0	0	26	0	0	2	0	2	0	0	2	0	2	65
5:00 PM	0	3	1	1	5	0	0	3	0	3	1	2	0	0	3	0	0	0	0	0	0	0	0	0	0	11
5:15 PM	0	5	0	0	5	1	0	0	0	1	1	2	0	0	3	0	0	0	0	0	0	0	0	0	0	9
5:30 PM	1	4	1	1	7	1	0	0	0	1	0	4	0	0	4	0	0	0	0	0	0	0	0	0	0	12
5:45 PM	0	2	0	0	2	0	0	2	0	2	0	3	1	0	4	0	0	0	0	0	0	0	0	0	0	8
Total	1	14	2	2	19	2	0	5	0	7	2	11	1	0	14	0	0	0	0	0	0	0	0	0	0	40
Grand Total	3	39	7	2	51	6	0	6	0	12	4	35	1	0	40	0	0	2	0	2	0	0	2	0	2	105
Approach %	5.9	76.5	13.7	3.9		50.0	0.0	50.0	0.0		10.0	87.5	2.5	0.0		0.0	0.0	100.0	0.0		0.0	0.0	100.0	0.0		
Total %	2.9	37.1	6.7	1.9	48.6	5.7	0.0	5.7	0.0	11.4	3.8	33.3	1.0	0.0	38.1	0.0	0.0	1.9	0.0	1.9	0.0	0.0	1.9	0.0	1.9	
Exiting Leg Total						45					11					45					4		105			

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	0	12	1	0	13	1	0	0	0	1	0	6	0	0	6	0	0	0	0	0	0	0	0	0	0	20
4:15 PM	1	3	0	0	4	1	0	1	0	2	2	8	0	0	10	0	0	0	0	0	0	0	0	0	0	16
4:30 PM	1	5	2	0	8	1	0	0	0	1	0	5	0	0	5	0	0	0	0	0	0	0	0	0	0	14
4:45 PM	0	5	2	0	7	1	0	0	0	1	0	5	0	0	5	0	0	2	0	2	0	0	2	0	2	15
Total Volume	2	25	5	0	32	4	0	1	0	5	2	24	0	0	26	0	0	2	0	2	0	0	2	0	2	65
% Approach Total	6.3	78.1	15.6	0.0		80.0	0.0	20.0	0.0		7.7	92.3	0.0	0.0		0.0	0.0	100.0	0.0		0.0	0.0	100.0	0.0		
PHF	0.500	0.521	0.625	0.000	0.615	1.000	0.000	0.250	0.000	0.625	0.250	0.750	0.000	0.000	0.650	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.250	0.000	0.250	0.813
Entering Leg	2	25	5	0	32	4	0	1	0	5	2	24	0	0	26	0	0	2	0	2	0	0	2	0	2	65
Exiting Leg						30					7					26					2		65			
Total						62					12					52					4		130			

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class: **Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)**



	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
Approach %	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	1					0					1					0					2					
Buses	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
% Buses	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0
Exiting Leg Total	1					0					1					0					2					
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Single-Unit	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0					
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0					

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
% Approach Total	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.500	
Buses	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
Buses %	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Buses	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Entering Leg	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
Buses	1					0					1					0					2					
Single-Unit Trucks	0					0					0					0					0					
Articulated Trucks	0					0					0					0					0					
Total Exiting Leg	1					0					1					0					2					

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Buses

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
Approach %	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
Total %	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	1					0					1					0					2					

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total					
	from North					from East					from South					from West										
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total						
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
% Approach Total	0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		
PHF	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.500
Entering Leg	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
Exiting Leg	1					0					1					0					2					
Total	2					0					2					0					4					

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Single-Unit Trucks

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total				
	from North					from East					from South					from West									
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total					
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0				

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total				
	from North					from East					from South					from West									
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total					
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0					0					0					0					0				
Total	0					0					0					0					0				

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Articulated Trucks

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Approach %	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0			
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Exiting Leg Total	0					0					0					0					0	

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Shirley Street					Park Avenue (Route 9)					Shirley Street					Park Avenue (Route 9)					Total	
	from North					from East					from South					from West						
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Approach Total	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
Entering Leg	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exiting Leg	0					0					0					0					0	
Total	0					0					0					0					0	

PDI File #: **249915 D**
 Location: **N: Shirley Street S: Shirley Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class: **Pedestrians**



Pedestrians

	Shirley Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	1	3	0	0	0	0	0	0	0	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
Total	0	0	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0	2	2	4	0	0	0	0	0	0	0	6
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	1	
5:15 PM	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1	0	1	0	0	0	0	0	0	0	2	
5:30 PM	0	0	0	0	0	0	0	0	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
Total	0	0	0	0	0	0	0	0	0	0	2	3	5	0	0	0	0	1	0	1	0	0	0	0	0	1	1	7	
Grand Total	0	0	0	0	0	0	0	0	0	0	3	4	7	0	0	0	0	3	2	5	0	0	0	0	0	1	1	13	
Approach %	0	0	0	0	0	0	0	0	0	0	42.9	57.1		0	0	0	0	60	40		0	0	0	0	0	100			
Total %	0	0	0	0	0	0	0	0	0	0	23.1	30.8	53.8	0	0	0	0	23.1	15.4	38.5	0	0	0	0	0	7.69	7.69		
Exiting Leg Total	0							7							5							1							13

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

4:45 PM	Shirley Street							Park Avenue (Route 9)							Shirley Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:45 PM	0	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	1	
5:15 PM	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1	0	1	0	0	0	0	0	0	0	2	
5:30 PM	0	0	0	0	0	0	0	0	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
Total Volume	0	0	0	0	0	0	0	0	0	0	3	3	6	0	0	0	0	1	0	1	0	0	0	0	0	1	1	8	
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	50.0	50.0		0.0	0.0	0.0	0.0	100.0	0.0		0.0	0.0	0.0	0.0	0.0	100.0			
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.750	0.375	0.500	0.000	0.000	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.250	0.250	0.667	
Entering Leg	0	0	0	0	0	0	0	0	0	0	3	3	6	0	0	0	0	1	0	1	0	0	0	0	0	1	1	8	
Exiting Leg	0							6							1							1							8
Total	0							12							2							2							16

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Cars and Heavy Vehicles (Combined)

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	89	0	0	89	2	0	2	0	4	1	165	0	0	166	259
7:15 AM	0	0	0	0	0	0	101	2	0	103	0	0	2	0	2	1	179	0	0	180	285
7:30 AM	0	0	0	0	0	0	99	0	0	99	2	0	4	0	6	4	216	0	0	220	325
7:45 AM	1	0	0	0	1	0	92	1	0	93	1	0	0	0	1	1	249	0	0	250	345
Total	1	0	0	0	1	0	381	3	0	384	5	0	8	0	13	7	809	0	0	816	1214
8:00 AM	0	0	0	0	0	0	116	5	0	121	5	0	1	0	6	4	219	1	0	224	351
8:15 AM	0	0	0	0	0	0	121	9	0	130	3	0	2	0	5	1	238	0	0	239	374
8:30 AM	0	0	0	0	0	0	114	3	0	117	2	0	3	0	5	0	183	0	0	183	305
8:45 AM	0	0	0	0	0	0	146	14	0	160	2	0	6	0	8	4	199	0	0	203	371
Total	0	0	0	0	0	0	497	31	0	528	12	0	12	0	24	9	839	1	0	849	1401
Grand Total	1	0	0	0	1	0	878	34	0	912	17	0	20	0	37	16	1648	1	0	1665	2615
Approach %	100.0	0.0	0.0	0.0		0.0	96.3	3.7	0.0		45.9	0.0	54.1	0.0		1.0	99.0	0.1	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	33.6	1.3	0.0	34.9	0.7	0.0	0.8	0.0	1.4	0.6	63.0	0.0	0.0	63.7	
Exiting Leg Total	1					1665					50					899					2615
Cars	1	0	0	0	1	0	837	34	0	871	17	0	16	0	33	16	1599	1	0	1616	2521
% Cars	100.0	0.0	0.0	0.0	100.0	0.0	95.3	100.0	0.0	95.5	100.0	0.0	80.0	0.0	89.2	100.0	97.0	100.0	0.0	97.1	96.4
Exiting Leg Total	1					1616					50					854					2521
Heavy Vehicles	0	0	0	0	0	0	41	0	0	41	0	0	4	0	4	0	49	0	0	49	94
% Heavy Vehicles	0.0	0.0	0.0	0.0	0.0	0.0	4.7	0.0	0.0	4.5	0.0	0.0	20.0	0.0	10.8	0.0	3.0	0.0	0.0	2.9	3.6
Exiting Leg Total	0					49					0					45					94

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
8:00 AM	0	0	0	0	0	0	116	5	0	121	5	0	1	0	6	4	219	1	0	224	351
8:15 AM	0	0	0	0	0	0	121	9	0	130	3	0	2	0	5	1	238	0	0	239	374
8:30 AM	0	0	0	0	0	0	114	3	0	117	2	0	3	0	5	0	183	0	0	183	305
8:45 AM	0	0	0	0	0	0	146	14	0	160	2	0	6	0	8	4	199	0	0	203	371
Total Volume	0	0	0	0	0	0	497	31	0	528	12	0	12	0	24	9	839	1	0	849	1401
% Approach Total	0.0	0.0	0.0	0.0		0.0	94.1	5.9	0.0		50.0	0.0	50.0	0.0		1.1	98.8	0.1	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.851	0.554	0.000	0.825	0.600	0.000	0.500	0.000	0.750	0.563	0.881	0.250	0.000	0.888	0.936
Cars	0	0	0	0	0	0	484	31	0	515	12	0	10	0	22	9	809	1	0	819	1356
Cars %	0.0	0.0	0.0	0.0	0.0	0.0	97.4	100.0	0.0	97.5	100.0	0.0	83.3	0.0	91.7	100.0	96.4	100.0	0.0	96.5	96.8
Heavy Vehicles	0	0	0	0	0	0	13	0	0	13	0	0	2	0	2	0	30	0	0	30	45
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	2.6	0.0	0.0	2.5	0.0	0.0	16.7	0.0	8.3	0.0	3.6	0.0	0.0	3.5	3.2
Cars Enter Leg	0	0	0	0	0	0	484	31	0	515	12	0	10	0	22	9	809	1	0	819	1356
Heavy Enter Leg	0	0	0	0	0	0	13	0	0	13	0	0	2	0	2	0	30	0	0	30	45
Total Entering Leg	0	0	0	0	0	0	497	31	0	528	12	0	12	0	24	9	839	1	0	849	1401
Cars Exiting Leg	1					821					40					494					1356
Heavy Exiting Leg	0					30					0					15					45
Total Exiting Leg	1					851					40					509					1401

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Cars

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	82	0	0	82	2	0	2	0	4	1	161	0	0	162	248
7:15 AM	0	0	0	0	0	0	91	2	0	93	0	0	1	0	1	1	172	0	0	173	267
7:30 AM	0	0	0	0	0	0	91	0	0	91	2	0	3	0	5	4	213	0	0	217	313
7:45 AM	1	0	0	0	1	0	89	1	0	90	1	0	0	0	1	1	244	0	0	245	337
Total	1	0	0	0	1	0	353	3	0	356	5	0	6	0	11	7	790	0	0	797	1165
8:00 AM	0	0	0	0	0	0	113	5	0	118	5	0	1	0	6	4	213	1	0	218	342
8:15 AM	0	0	0	0	0	0	120	9	0	129	3	0	1	0	4	1	233	0	0	234	367
8:30 AM	0	0	0	0	0	0	109	3	0	112	2	0	2	0	4	0	173	0	0	173	289
8:45 AM	0	0	0	0	0	0	142	14	0	156	2	0	6	0	8	4	190	0	0	194	358
Total	0	0	0	0	0	0	484	31	0	515	12	0	10	0	22	9	809	1	0	819	1356
Grand Total	1	0	0	0	1	0	837	34	0	871	17	0	16	0	33	16	1599	1	0	1616	2521
Approach %	100.0	0.0	0.0	0.0		0.0	96.1	3.9	0.0		51.5	0.0	48.5	0.0		1.0	98.9	0.1	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	33.2	1.3	0.0	34.5	0.7	0.0	0.6	0.0	1.3	0.6	63.4	0.0	0.0	64.1	
Exiting Leg Total	1					1616					50					854					2521

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:30 AM	0	0	0	0	0	0	91	0	0	91	2	0	3	0	5	4	213	0	0	217	313
7:45 AM	1	0	0	0	1	0	89	1	0	90	1	0	0	0	1	1	244	0	0	245	337
8:00 AM	0	0	0	0	0	0	113	5	0	118	5	0	1	0	6	4	213	1	0	218	342
8:15 AM	0	0	0	0	0	0	120	9	0	129	3	0	1	0	4	1	233	0	0	234	367
Total Volume	1	0	0	0	1	0	413	15	0	428	11	0	5	0	16	10	903	1	0	914	1359
% Approach Total	100.0	0.0	0.0	0.0		0.0	96.5	3.5	0.0		68.8	0.0	31.3	0.0		1.1	98.8	0.1	0.0		
PHF	0.250	0.000	0.000	0.000	0.250	0.000	0.860	0.417	0.000	0.829	0.550	0.000	0.417	0.000	0.667	0.625	0.925	0.250	0.000	0.933	0.926
Entering Leg	1	0	0	0	1	0	413	15	0	428	11	0	5	0	16	10	903	1	0	914	1359
Exiting Leg	1					914					25					419					1359
Total	2					1342					41					1333					2718

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	4	0	0	4	11
7:15 AM	0	0	0	0	0	0	10	0	0	10	0	0	1	0	1	0	7	0	0	7	18
7:30 AM	0	0	0	0	0	0	8	0	0	8	0	0	1	0	1	0	3	0	0	3	12
7:45 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	5	0	0	5	8
Total	0	0	0	0	0	0	28	0	0	28	0	0	2	0	2	0	19	0	0	19	49
8:00 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	6	0	0	6	9
8:15 AM	0	0	0	0	0	0	1	0	0	1	0	0	1	0	1	0	5	0	0	5	7
8:30 AM	0	0	0	0	0	0	5	0	0	5	0	0	1	0	1	0	10	0	0	10	16
8:45 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	9	0	0	9	13
Total	0	0	0	0	0	0	13	0	0	13	0	0	2	0	2	0	30	0	0	30	45
Grand Total	0	0	0	0	0	0	41	0	0	41	0	0	4	0	4	0	49	0	0	49	94
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	43.6	0.0	0.0	43.6	0.0	0.0	4.3	0.0	4.3	0.0	52.1	0.0	0.0	52.1	
Exiting Leg Total	0					49					0					45					94
Buses	0	0	0	0	0	0	19	0	0	19	0	0	2	0	2	0	29	0	0	29	50
% Buses	0.0	0.0	0.0	0.0	0.0	0.0	46.3	0.0	0.0	46.3	0.0	0.0	50.0	0.0	50.0	0.0	59.2	0.0	0.0	59.2	53.2
Exiting Leg Total	0					29					0					21					50
Single-Unit Trucks	0	0	0	0	0	0	16	0	0	16	0	0	1	0	1	0	14	0	0	14	31
% Single-Unit	0.0	0.0	0.0	0.0	0.0	0.0	39.0	0.0	0.0	39.0	0.0	0.0	25.0	0.0	25.0	0.0	28.6	0.0	0.0	28.6	33.0
Exiting Leg Total	0					14					0					17					31
Articulated Trucks	0	0	0	0	0	0	6	0	0	6	0	0	1	0	1	0	6	0	0	6	13
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	14.6	0.0	0.0	14.6	0.0	0.0	25.0	0.0	25.0	0.0	12.2	0.0	0.0	12.2	13.8
Exiting Leg Total	0					6					0					7					13

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	4	0	0	4	11
7:15 AM	0	0	0	0	0	0	10	0	0	10	0	0	1	0	1	0	7	0	0	7	18
7:30 AM	0	0	0	0	0	0	8	0	0	8	0	0	1	0	1	0	3	0	0	3	12
7:45 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	5	0	0	5	8
Total Volume	0	0	0	0	0	0	28	0	0	28	0	0	2	0	2	0	19	0	0	19	49
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.700	0.000	0.000	0.700	0.000	0.000	0.500	0.000	0.500	0.000	0.679	0.000	0.000	0.679	0.681
Buses	0	0	0	0	0	0	13	0	0	13	0	0	1	0	1	0	12	0	0	12	26
Buses %	0.0	0.0	0.0	0.0	0.0	0.0	46.4	0.0	0.0	46.4	0.0	0.0	50.0	0.0	50.0	0.0	63.2	0.0	0.0	63.2	53.1
Single-Unit Trucks	0	0	0	0	0	0	10	0	0	10	0	0	1	0	1	0	5	0	0	5	16
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	35.7	0.0	0.0	35.7	0.0	0.0	50.0	0.0	50.0	0.0	26.3	0.0	0.0	26.3	32.7
Articulated Trucks	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	2	0	0	2	7
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	17.9	0.0	0.0	17.9	0.0	0.0	0.0	0.0	0.0	0.0	10.5	0.0	0.0	10.5	14.3
Buses	0	0	0	0	0	0	13	0	0	13	0	0	1	0	1	0	12	0	0	12	26
Single-Unit Trucks	0	0	0	0	0	0	10	0	0	10	0	0	1	0	1	0	5	0	0	5	16
Articulated Trucks	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	2	0	0	2	7
Total Entering Leg	0	0	0	0	0	0	28	0	0	28	0	0	2	0	2	0	19	0	0	19	49
Buses	0					12					0					14					26
Single-Unit Trucks	0					5					0					11					16
Articulated Trucks	0					2					0					5					7
Total Exiting Leg	0					19					0					30					49

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Buses

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	3	0	0	3	7
7:15 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	6
7:30 AM	0	0	0	0	0	0	5	0	0	5	0	0	1	0	1	0	3	0	0	3	9
7:45 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	4
Total	0	0	0	0	0	0	13	0	0	13	0	0	1	0	1	0	12	0	0	12	26
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	4	4
8:15 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
8:30 AM	0	0	0	0	0	0	3	0	0	3	0	0	1	0	1	0	8	0	0	8	12
8:45 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	3	0	0	3	5
Total	0	0	0	0	0	0	6	0	0	6	0	0	1	0	1	0	17	0	0	17	24
Grand Total	0	0	0	0	0	0	19	0	0	19	0	0	2	0	2	0	29	0	0	29	50
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	
Total %	0.0	0.0	0.0	0.0	0.0	0.0	38.0	0.0	0.0	38.0	0.0	0.0	4.0	0.0	4.0	0.0	58.0	0.0	0.0	58.0	
Exiting Leg Total	0					29					0					21					50

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	3	0	0	3	7
7:15 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	4	0	0	4	6
7:30 AM	0	0	0	0	0	0	5	0	0	5	0	0	1	0	1	0	3	0	0	3	9
7:45 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	4
Total Volume	0	0	0	0	0	0	13	0	0	13	0	0	1	0	1	0	12	0	0	12	26
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.650	0.000	0.000	0.650	0.000	0.000	0.250	0.000	0.250	0.000	0.750	0.000	0.000	0.750	0.722
Entering Leg	0					13					1					12					26
Exiting Leg	0					12					0					14					26
Total	0					25					1					26					52

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Single-Unit Trucks

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	4
7:15 AM	0	0	0	0	0	0	4	0	0	4	0	0	1	0	1	0	2	0	0	2	7
7:30 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	2
7:45 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
Total	0	0	0	0	0	0	10	0	0	10	0	0	1	0	1	0	5	0	0	5	16
8:00 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	3
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
8:30 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	3
8:45 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	6	0	0	6	8
Total	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	9	0	0	9	15
Grand Total	0	0	0	0	0	0	16	0	0	16	0	0	1	0	1	0	14	0	0	14	31
Approach %	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	100.0	0.0		0.0	100.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	51.6	0.0	0.0	51.6	0.0	0.0	3.2	0.0	3.2	0.0	45.2	0.0	0.0	45.2	
Exiting Leg Total	0					14					0					17					31

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	4
7:15 AM	0	0	0	0	0	0	4	0	0	4	0	0	1	0	1	0	2	0	0	2	7
7:30 AM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	2
7:45 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
Total Volume	0	0	0	0	0	0	10	0	0	10	0	0	1	0	1	0	5	0	0	5	16
% Approach Total	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	100.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.625	0.000	0.000	0.625	0.000	0.000	0.250	0.000	0.250	0.000	0.625	0.000	0.000	0.625	0.571
Entering Leg	0					10					1					5					16
Exiting Leg	0					5					0					11					16
Total	0					15					1					16					32

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **7:00 AM**
 End Time: **9:00 AM**
 Class:



Articulated Trucks

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	1	0	0	1	5
7:30 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
Total	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	2	0	0	2	7
8:00 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	2	0	0	2	3
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	1	0	0	1	0	0	1	0	1	0	4	0	0	4	6
Grand Total	0	0	0	0	0	0	6	0	0	6	0	0	1	0	1	0	6	0	0	6	13
Approach %	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	100.0	0.0		0.0	100.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	46.2	0.0	0.0	46.2	0.0	0.0	7.7	0.0	7.7	0.0	46.2	0.0	0.0	46.2	
Exiting Leg Total	0					6					0					7					13

Peak Hour Analysis from 07:00 AM to 09:00 AM begins at:

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
7:15 AM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	1	0	0	1	5
7:30 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
8:00 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
Total Volume	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	3	0	0	3	9
% Approach Total	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.375	0.000	0.000	0.375	0.000	0.000	0.000	0.000	0.000	0.000	0.750	0.000	0.000	0.750	0.450
Entering Leg	0					6					0					3					9
Exiting Leg	0					3					0					6					9
Total	0					9					0					9					18

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars and Heavy Vehicles (Combined)

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	1	0	0	0	1	0	248	6	0	254	8	0	11	0	19	6	180	0	0	186	460
4:15 PM	1	0	0	0	1	0	216	6	0	222	7	0	8	0	15	4	162	0	0	166	404
4:30 PM	0	0	0	0	0	0	192	6	0	198	6	0	11	0	17	5	154	0	0	159	374
4:45 PM	0	0	0	0	0	0	232	6	0	238	4	1	7	0	12	1	156	0	0	157	407
Total	2	0	0	0	2	0	888	24	0	912	25	1	37	0	63	16	652	0	0	668	1645
5:00 PM	0	0	0	0	0	0	184	6	0	190	9	0	8	0	17	9	175	0	0	184	391
5:15 PM	0	0	0	0	0	0	199	5	0	204	12	0	21	0	33	4	147	0	0	151	388
5:30 PM	0	0	0	0	0	0	189	8	0	197	14	0	11	0	25	11	163	0	0	174	396
5:45 PM	0	0	0	0	0	0	191	11	0	202	12	0	8	0	20	8	136	0	0	144	366
Total	0	0	0	0	0	0	763	30	0	793	47	0	48	0	95	32	621	0	0	653	1541
Grand Total	2	0	0	0	2	0	1651	54	0	1705	72	1	85	0	158	48	1273	0	0	1321	3186
Approach %	100.0	0.0	0.0	0.0		0.0	96.8	3.2	0.0		45.6	0.6	53.8	0.0		3.6	96.4	0.0	0.0		
Total %	0.1	0.0	0.0	0.0	0.1	0.0	51.8	1.7	0.0	53.5	2.3	0.0	2.7	0.0	5.0	1.5	40.0	0.0	0.0	41.5	
Exiting Leg Total	1					1345					102					1738					3186
Cars	2	0	0	0	2	0	1631	54	0	1685	72	1	84	0	157	48	1249	0	0	1297	3141
% Cars	100.0	0.0	0.0	0.0	100.0	0.0	98.8	100.0	0.0	98.8	100.0	100.0	98.8	0.0	99.4	100.0	98.1	0.0	0.0	98.2	98.6
Exiting Leg Total	1					1321					102					1717					3141
Heavy Vehicles	0	0	0	0	0	0	20	0	0	20	0	0	1	0	1	0	24	0	0	24	45
% Heavy Vehicles	0.0	0.0	0.0	0.0	0.0	0.0	1.2	0.0	0.0	1.2	0.0	0.0	1.2	0.0	0.6	0.0	1.9	0.0	0.0	1.8	1.4
Exiting Leg Total	0					24					0					21					45

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	1	0	0	0	1	0	248	6	0	254	8	0	11	0	19	6	180	0	0	186	460
4:15 PM	1	0	0	0	1	0	216	6	0	222	7	0	8	0	15	4	162	0	0	166	404
4:30 PM	0	0	0	0	0	0	192	6	0	198	6	0	11	0	17	5	154	0	0	159	374
4:45 PM	0	0	0	0	0	0	232	6	0	238	4	1	7	0	12	1	156	0	0	157	407
Total Volume	2	0	0	0	2	0	888	24	0	912	25	1	37	0	63	16	652	0	0	668	1645
% Approach Total	100.0	0.0	0.0	0.0		0.0	97.4	2.6	0.0		39.7	1.6	58.7	0.0		2.4	97.6	0.0	0.0		
PHF	0.500	0.000	0.000	0.000	0.500	0.000	0.895	1.000	0.000	0.898	0.781	0.250	0.841	0.000	0.829	0.667	0.906	0.000	0.000	0.898	0.894
Cars	2	0	0	0	2	0	876	24	0	900	25	1	36	0	62	16	638	0	0	654	1618
Cars %	100.0	0.0	0.0	0.0	100.0	0.0	98.6	100.0	0.0	98.7	100.0	100.0	97.3	0.0	98.4	100.0	97.9	0.0	0.0	97.9	98.4
Heavy Vehicles	0	0	0	0	0	0	12	0	0	12	0	0	1	0	1	0	14	0	0	14	27
Heavy Vehicles %	0.0	0.0	0.0	0.0	0.0	0.0	1.4	0.0	0.0	1.3	0.0	0.0	2.7	0.0	1.6	0.0	2.1	0.0	0.0	2.1	1.6
Cars Enter Leg	2	0	0	0	2	0	876	24	0	900	25	1	36	0	62	16	638	0	0	654	1618
Heavy Enter Leg	0	0	0	0	0	0	12	0	0	12	0	0	1	0	1	0	14	0	0	14	27
Total Entering Leg	2	0	0	0	2	0	888	24	0	912	25	1	37	0	63	16	652	0	0	668	1645
Cars Exiting Leg	1					663					40					914					1618
Heavy Exiting Leg	0					14					0					13					27
Total Exiting Leg	1					677					40					927					1645

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Cars

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	1	0	0	0	1	0	245	6	0	251	8	0	10	0	18	6	177	0	0	183	453
4:15 PM	1	0	0	0	1	0	210	6	0	216	7	0	8	0	15	4	159	0	0	163	395
4:30 PM	0	0	0	0	0	0	191	6	0	197	6	0	11	0	17	5	149	0	0	154	368
4:45 PM	0	0	0	0	0	0	230	6	0	236	4	1	7	0	12	1	153	0	0	154	402
Total	2	0	0	0	2	0	876	24	0	900	25	1	36	0	62	16	638	0	0	654	1618
5:00 PM	0	0	0	0	0	0	183	6	0	189	9	0	8	0	17	9	171	0	0	180	386
5:15 PM	0	0	0	0	0	0	195	5	0	200	12	0	21	0	33	4	144	0	0	148	381
5:30 PM	0	0	0	0	0	0	186	8	0	194	14	0	11	0	25	11	161	0	0	172	391
5:45 PM	0	0	0	0	0	0	191	11	0	202	12	0	8	0	20	8	135	0	0	143	365
Total	0	0	0	0	0	0	755	30	0	785	47	0	48	0	95	32	611	0	0	643	1523
Grand Total	2	0	0	0	2	0	1631	54	0	1685	72	1	84	0	157	48	1249	0	0	1297	3141
Approach %	100.0	0.0	0.0	0.0		0.0	96.8	3.2	0.0		45.9	0.6	53.5	0.0		3.7	96.3	0.0	0.0		
Total %	0.1	0.0	0.0	0.0	0.1	0.0	51.9	1.7	0.0	53.6	2.3	0.0	2.7	0.0	5.0	1.5	39.8	0.0	0.0	41.3	
Exiting Leg Total					1					1321					102					1717	3141

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	1	0	0	0	1	0	245	6	0	251	8	0	10	0	18	6	177	0	0	183	453
4:15 PM	1	0	0	0	1	0	210	6	0	216	7	0	8	0	15	4	159	0	0	163	395
4:30 PM	0	0	0	0	0	0	191	6	0	197	6	0	11	0	17	5	149	0	0	154	368
4:45 PM	0	0	0	0	0	0	230	6	0	236	4	1	7	0	12	1	153	0	0	154	402
Total Volume	2	0	0	0	2	0	876	24	0	900	25	1	36	0	62	16	638	0	0	654	1618
% Approach Total	100.0	0.0	0.0	0.0		0.0	97.3	2.7	0.0		40.3	1.6	58.1	0.0		2.4	97.6	0.0	0.0		
PHF	0.500	0.000	0.000	0.000	0.500	0.000	0.894	1.000	0.000	0.896	0.781	0.250	0.818	0.000	0.861	0.667	0.901	0.000	0.000	0.893	0.893
Entering Leg	2	0	0	0	2	0	876	24	0	900	25	1	36	0	62	16	638	0	0	654	1618
Exiting Leg					1					663					40					914	1618
Total					3					1563					102					1568	3236

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class: **Heavy Vehicles-Combined (Buses, Single-Unit Trucks, Articulated Trucks)**



	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	3	0	0	3	0	0	1	0	1	0	3	0	0	3	7
4:15 PM	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	3	0	0	3	9
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	5	0	0	5	6
4:45 PM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	3	0	0	3	5
Total	0	0	0	0	0	0	12	0	0	12	0	0	1	0	1	0	14	0	0	14	27
5:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	4	0	0	4	5
5:15 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	3	0	0	3	7
5:30 PM	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	2	0	0	2	5
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
Total	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	0	10	0	0	10	18
Grand Total	0	0	0	0	0	0	20	0	0	20	0	0	1	0	1	0	24	0	0	24	45
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0
Total %	0.0	0.0	0.0	0.0	0.0	0.0	44.4	0.0	0.0	44.4	0.0	0.0	2.2	0.0	2.2	0.0	53.3	0.0	0.0	53.3	0.0
Exiting Leg Total	0					24					0					21					45
Buses	0	0	0	0	0	0	8	0	0	8	0	0	1	0	1	0	14	0	0	14	23
% Buses	0.0	0.0	0.0	0.0	0.0	0.0	40.0	0.0	0.0	40.0	0.0	0.0	100.0	0.0	100.0	0.0	58.3	0.0	0.0	58.3	51.1
Exiting Leg Total	0					14					0					9					23
Single-Unit Trucks	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	0	10	0	0	10	19
% Single-Unit	0.0	0.0	0.0	0.0	0.0	0.0	45.0	0.0	0.0	45.0	0.0	0.0	0.0	0.0	0.0	0.0	41.7	0.0	0.0	41.7	42.2
Exiting Leg Total	0					10					0					9					19
Articulated Trucks	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	3
% Articulated	0.0	0.0	0.0	0.0	0.0	0.0	15.0	0.0	0.0	15.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	6.7
Exiting Leg Total	0					0					0					3					3

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	3	0	0	3	0	0	1	0	1	0	3	0	0	3	7
4:15 PM	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	3	0	0	3	9
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	5	0	0	5	6
4:45 PM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	3	0	0	3	5
Total Volume	0	0	0	0	0	0	12	0	0	12	0	0	1	0	1	0	14	0	0	14	27
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.500	0.000	0.000	0.250	0.000	0.250	0.000	0.700	0.000	0.000	0.700	0.750
Buses	0	0	0	0	0	0	5	0	0	5	0	0	1	0	1	0	8	0	0	8	14
Buses %	0.0	0.0	0.0	0.0	0.0	0.0	41.7	0.0	0.0	41.7	0.0	0.0	100.0	0.0	100.0	0.0	57.1	0.0	0.0	57.1	51.9
Single-Unit Trucks	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	6	0	0	6	12
Single-Unit %	0.0	0.0	0.0	0.0	0.0	0.0	50.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	42.9	0.0	0.0	42.9	44.4
Articulated Trucks	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Articulated %	0.0	0.0	0.0	0.0	0.0	0.0	8.3	0.0	0.0	8.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	3.7
Buses	0	0	0	0	0	0	5	0	0	5	0	0	1	0	1	0	8	0	0	8	14
Single-Unit Trucks	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	6	0	0	6	12
Articulated Trucks	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
Total Entering Leg	0	0	0	0	0	0	12	0	0	12	0	0	1	0	1	0	14	0	0	14	27
Buses	0					8					0					6					14
Single-Unit Trucks	0					6					0					6					12
Articulated Trucks	0					0					0					1					1
Total Exiting Leg	0					14					0					13					27

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Buses

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	3	0	0	3	0	0	1	0	1	0	2	0	0	2	6
4:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	3	3
4:45 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
Total	0	0	0	0	0	0	5	0	0	5	0	0	1	0	1	0	8	0	0	8	14
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	2
5:15 PM	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	2	0	0	2	4
5:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	6	0	0	6	9
Grand Total	0	0	0	0	0	0	8	0	0	8	0	0	1	0	1	0	14	0	0	14	23
Approach %	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	100.0	0.0		0.0	100.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	34.8	0.0	0.0	34.8	0.0	0.0	4.3	0.0	4.3	0.0	60.9	0.0	0.0	60.9	
Exiting Leg Total	0					14					0					9					23

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	3	0	0	3	0	0	1	0	1	0	2	0	0	2	6
4:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	3	3
4:45 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
Total Volume	0	0	0	0	0	0	5	0	0	5	0	0	1	0	1	0	8	0	0	8	14
% Approach Total	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	100.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.417	0.000	0.000	0.417	0.000	0.000	0.250	0.000	0.250	0.000	0.667	0.000	0.000	0.667	0.583
Entering Leg	0	0	0	0	0	0	5	0	0	5	0	0	1	0	1	0	8	0	0	8	14
Exiting Leg	0					8					0					6					14
Total	0					13					1					14					28

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Single-Unit Trucks

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
4:15 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	1	0	0	1	5
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
4:45 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
Total	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	6	0	0	6	12
5:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
5:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
5:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
Total	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	4	0	0	4	7
Grand Total	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	0	10	0	0	10	19
Approach %	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	47.4	0.0	0.0	47.4	0.0	0.0	0.0	0.0	0.0	0.0	52.6	0.0	0.0	52.6	
Exiting Leg Total	0					10					0					9					19

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total
	from North					from East					from South					from West					
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	
4:15 PM	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	1	0	0	1	5
4:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
4:45 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
5:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	2	3
Total Volume	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	7	0	0	7	14
% Approach Total	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.438	0.000	0.000	0.438	0.000	0.000	0.000	0.000	0.000	0.000	0.875	0.000	0.000	0.875	0.700
Entering Leg	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	0	7	0	0	7	14
Exiting Leg	0					7					0					7					14
Total	0					14					0					14					28

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Articulated Trucks

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total				
	from North					from East					from South					from West									
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total					
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
4:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1			
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
Total	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1			
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
5:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1			
5:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1			
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
Total	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2			
Grand Total	0	0	0	0	0	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	0	3			
Approach %	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0						
Total %	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Exiting Leg Total						0						0						0						3	3

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Parking Lot					Park Avenue (Route 9)					Downing Street					Park Avenue (Route 9)					Total				
	from North					from East					from South					from West									
	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total	Right	Thru	Left	U-Turn	Total					
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
5:15 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1			
5:30 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1			
Total Volume	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2			
% Approach Total	0.0	0.0	0.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0						
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.500			
Entering Leg	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2			
Exiting Leg						0						0						0						2	2
Total						0						2						0						2	4

PDI File #: 249915 E
 Location: N: Parking Lot S: Downing Street
 Location: E: Park Avenue (Route 9) W: Park Avenue (Route 9)
 City, State: Worcester, MA
 Client: GIA/A. Allen
 Site Code: TBD
 Count Date: Wednesday, March 20, 2024
 Start Time: 4:00 PM
 End Time: 6:00 PM
 Class:



Bicycles (on Roadway and Crosswalks)

	Parking Lot							Park Avenue (Route 9)							Downing Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	1	0	0	0	0	1	0	0	0	0	1	0	1	0	0	0	0	0	0	0	2	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	0	0	0	0	0	0	1	0	0	0	0	1	0	0	0	0	1	0	1	0	0	0	0	0	0	0	2	
Approach %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
Total %	0.0	0.0	0.0	0.0	0.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	50.0	0.0	0.0	0.0	0.0	50.0	0.0	50.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Exiting Leg Total	1							0							1							0	2						

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Parking Lot							Park Avenue (Route 9)							Downing Street							Park Avenue (Route 9)							Total
	from North							from East							from South							from West							
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total	Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total	Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total	Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total	
4:00 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	1	0	0	0	0	1	0	0	0	0	1	0	1	0	0	0	0	0	0	0	2	
% Approach Total	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	100.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
PHF	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.500	
Entering Leg	0	0	0	0	0	0	0	1	0	0	0	0	1	0	0	0	0	1	0	1	0	0	0	0	0	0	0	2	
Exiting Leg	1							0							1							0	2						
Total	1							1							2							0	4						

PDI File #: **249915 E**
 Location: **N: Parking Lot S: Downing Street**
 Location: **E: Park Avenue (Route 9) W: Park Avenue (Route 9)**
 City, State: **Worcester, MA**
 Client: **GIA/A. Allen**
 Site Code: **TBD**
 Count Date: **Wednesday, March 20, 2024**
 Start Time: **4:00 PM**
 End Time: **6:00 PM**
 Class:



Pedestrians

	Parking Lot								Park Avenue (Route 9)								Downing Street								Park Avenue (Route 9)								Total
	from North								from East								from South								from West								
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total		Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total		Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total		Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	2	1	3		0	0	0	0	2	3	5		0	0	0	0	0	0	0		8	
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	1	2		0	0	0	0	2	0	2		0	0	0	0	0	0	0		4	
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	1		0	0	0	0	2	1	3		0	0	0	0	0	0	0		4	
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	4	4		0	0	0	0	2	3	5		0	0	0	0	0	0	1	1	10	
Total	0	0	0	0	0	0	0	0	0	0	0	3	7	10		0	0	0	0	8	7	15		0	0	0	0	0	0	1	1	26	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0		0	0	0	0	0	1	1		0	0	0	0	0	0	1	1	2	
5:15 PM	0	0	0	0	3	0	3	0	0	0	0	6	0	6		0	0	0	0	1	1	2		0	0	0	0	0	0	0	0	11	
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	1		0	0	0	0	2	1	3		0	0	0	0	0	0	0	0	4	
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0		0	0	0	0	1	3	4		0	0	0	0	0	0	0	0	4	
Total	0	0	0	0	3	0	3	0	0	0	0	6	1	7		0	0	0	0	4	6	10		0	0	0	0	0	0	1	1	21	
Grand Total	0	0	0	0	3	0	3	0	0	0	0	9	8	17		0	0	0	0	12	13	25		0	0	0	0	0	0	2	2	47	
Approach %	0	0	0	0	100	0		0	0	0	0	52.9	47.1		0	0	0	0	48	52		0	0	0	0	0	0	100					
Total %	0	0	0	0	6.38	0	6.38	0	0	0	0	19.1	17	36.2		0	0	0	0	25.5	27.7	53.2		0	0	0	0	0	0	4.26	4.26		
Exiting Leg Total	3							17							25							2							47				

Peak Hour Analysis from 04:00 PM to 06:00 PM begins at:

	Parking Lot								Park Avenue (Route 9)								Downing Street								Park Avenue (Route 9)								Total
	from North								from East								from South								from West								
	Right	Thru	Left	U-Turn	CW-EB	CW-WB	Total		Right	Thru	Left	U-Turn	CW-SB	CW-NB	Total		Right	Thru	Left	U-Turn	CW-WB	CW-EB	Total		Right	Thru	Left	U-Turn	CW-NB	CW-SB	Total		
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	1		0	0	0	0	2	1	3		0	0	0	0	0	0	0		4	
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	4	4		0	0	0	0	2	3	5		0	0	0	0	0	0	1	1	10	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0		0	0	0	0	0	1	1		0	0	0	0	0	0	1	1	2	
5:15 PM	0	0	0	0	3	0	3	0	0	0	0	6	0	6		0	0	0	0	1	1	2		0	0	0	0	0	0	0	0	11	
Total Volume	0	0	0	0	3	0	3	0	0	0	0	6	5	11		0	0	0	0	5	6	11		0	0	0	0	0	0	2	2	27	
% Approach Total	0.0	0.0	0.0	0.0	100.0	0.0		0.0	0.0	0.0	0.0	54.5	45.5		0.0	0.0	0.0	0.0	45.5	54.5		0.0	0.0	0.0	0.0	0.0	0.0	100.0					
PHF	0.000	0.000	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.000	0.250	0.313	0.458		0.000	0.000	0.000	0.000	0.625	0.500	0.550		0.000	0.000	0.000	0.000	0.000	0.500	0.500	0.614		
Entering Leg	0	0	0	0	3	0	3	0	0	0	0	6	5	11		0	0	0	0	5	6	11		0	0	0	0	0	0	2	2	27	
Exiting Leg	3							11							11							2							27				
Total	6							22							22							4							54				



***MASSDOT SEASONAL ADJUSTMENT FACTORS AND
HISTORICAL GROWTH***





GREEN INTERNATIONAL AFFILIATES, INC.
Civil and Structural Engineers
 200 Ames Pond Drive, Suite 200
 TEWKSBURY, MA 01876

JOB 487 Park Avenue, Worcester TIAS

SHEET NO.	1	OF	1
CALCULATED BY	JG	DATE	4/12/2024
CHECKED BY	AA	DATE	4/12/2024
DESCRIPTION			

Daily Avg. Counts on Weekdays

From MassDOT Interactive Transportation Data Management System

	MassDOT spot count, Location ID 3333	
Int-190 North of Lincoln Street (Worcester)		
Year	2013	2022
Daily Traffic Volume	133,519	141,020
Average Annual Growth Rate		0.61%

	MassDOT spot count, Location ID 3894	
I-290 at Worcester		
Year	2013	2023
Daily Traffic Volume	65,177	80,351
Average Annual Growth Rate		2.12%

Average Annual Growth:	1.36%
Say:	1.5%

Massachusetts Highway Department
Statewide Traffic Data Collection
2019 Weekday Seasonal Factors

Factor Group	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC	Axle Factor
R1	1.22	1.14	1.12	1.06	1.00	0.96	0.87	0.85	0.96	0.99	1.04	1.12	0.85
R2	0.95	0.96	0.98	0.97	0.97	0.93	0.97	0.94	0.96	0.90	0.92	0.93	0.96
R3	1.15	1.06	1.07	1.00	0.89	0.88	0.89	0.89	0.95	0.92	1.02	1.01	0.97
R4-R7	1.09	1.09	1.11	1.02	0.96	0.92	0.89	0.89	0.99	0.98	1.09	1.13	0.98
U1-Boston	1.03	1.01	0.98	0.94	0.94	0.92	0.95	0.93	0.94	0.94	0.97	1.04	0.96
U1-Essex	1.09	1.06	1.03	0.99	0.94	0.90	0.88	0.86	0.93	0.94	0.99	1.06	0.93
U1-Southeast	1.06	1.05	1.01	0.97	0.95	0.93	0.93	0.90	0.94	0.94	0.98	1.04	0.98
U1-West	1.19	1.14	1.09	0.95	0.92	0.89	0.89	0.86	0.91	0.95	0.97	1.07	0.84
U1-Worcester	1.02	1.04	0.97	0.94	0.93	0.91	0.95	0.91	0.93	0.92	0.95	1.10	0.88
U2	1.01	1.00	0.94	0.93	0.91	0.89	0.93	0.90	0.90	0.91	0.94	1.02	0.99
U3	1.06	1.03	0.98	0.94	0.93	0.91	0.95	0.91	0.92	0.93	0.97	1.00	0.98
U4-U7	1.01	1.00	0.95	0.92	0.88	0.86	0.92	0.91	0.92	0.94	0.99	1.04	0.99
Rec - East	1.04	1.16	1.12	0.98	0.92	0.88	0.77	0.81	0.94	1.02	1.08	1.12	0.99
Rec - West	1.30	1.23	1.32	1.18	0.95	0.82	0.70	0.69	0.97	0.96	1.16	1.15	0.98

Round off:

0-999 = 10

>1000 = 100

U = Urban

R = Rural

1 - Interstate

2 - Freeway and Expressway

3 - Other Principal Arterial

4 - Minor Arterial

5 - Major Collector

6 - Minor Collector

7 - Local Road and Street

Recreational - East Group - Cape Cod (all towns) including the town of Plymouth south of Route 3A (stations 7014,7079,7080,7090,7091,7092,7093,7094,7095,7096,7097,7108 and 7178), Martha's Vineyard and Nantucket.

Recreational - West Group - Continuous Stations 2 and 189 including stations 1066,1067,1083,1084,1085,1086,1087,1088,1089,1090,1091,1092,1093,1094,1095,1096,1097,1098,1099,1100,1101,1102,1103,1104,1105,1106,1107,1108,1113, 1114,1116,2196,2197 and 2198.

CRASH RATE CALCULATIONS

INTERSECTION CRASH RATE WORKSHEET

TOWN : Worcester

COUNT DATE : 3/20/2024

UNSIGNALIZED :

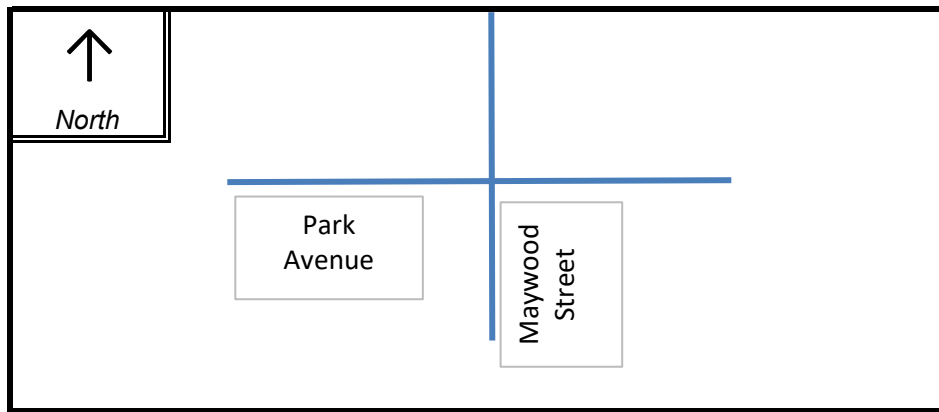
SIGNALIZED :

~ INTERSECTION DATA ~

MAJOR STREET : Park Avenue (Route 12)

MINOR STREET(S) : Maywood Street

**INTERSECTION
DIAGRAM**



PEAK HOUR VOLUMES

APPROACH :	1	2	3	4	5	Total Peak Hourly Approach Volume
DIRECTION :	NB	SB	EB	WB		
PEAK HOURLY VOLUMES (PM) :	182	98	574	866		1,720

" K " FACTOR : INTERSECTION ADT (**V**) = TOTAL DAILY APPROACH VOLUME :

TOTAL # OF CRASHES : # OF YEARS : AVERAGE # OF CRASHES PER YEAR (**A**) :

CRASH RATE CALCULATION :

$$\text{RATE} = \frac{(A * 1,000,000)}{(V * 365)}$$

Comments : The District 3 crash rate is 0.89 for signalized intersection

Project Title & Date: 487 Park Avenue TIAS Worcester, MA

INTERSECTION CRASH RATE WORKSHEET

TOWN : Worcester

COUNT DATE : 3/20/2024

UNSIGNALIZED :

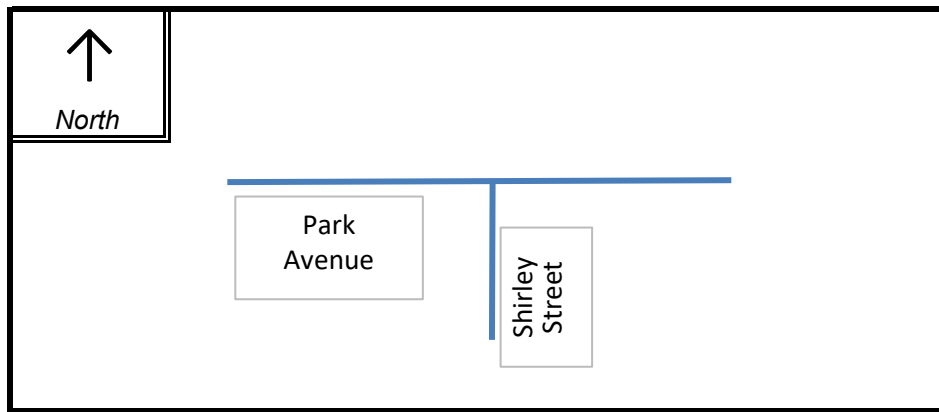
SIGNALIZED :

~ INTERSECTION DATA ~

MAJOR STREET : Park Avenue (Route 12)

MINOR STREET(S) : Shirley Street

**INTERSECTION
DIAGRAM**



PEAK HOUR VOLUMES

APPROACH :	1	2	3	4	5	Total Peak Hourly Approach Volume
DIRECTION :	NB	SB	EB	WB		
PEAK HOURLY VOLUMES (PM) :	28	20	649	918		1,615

" K " FACTOR : INTERSECTION ADT (**V**) = TOTAL DAILY APPROACH VOLUME :

TOTAL # OF CRASHES : # OF YEARS : AVERAGE # OF CRASHES PER YEAR (**A**) :

CRASH RATE CALCULATION :

RATE =

$$\frac{(A * 1,000,000)}{(V * 365)}$$

Comments : The District 3 crash rate is 0.61 for unsignalized intersection

Project Title & Date: 487 Park Avenue TIAS Worcester, MA

INTERSECTION CRASH RATE WORKSHEET

TOWN : Worcester

COUNT DATE : 3/20/2024

UNSIGNALIZED :

SIGNALIZED :

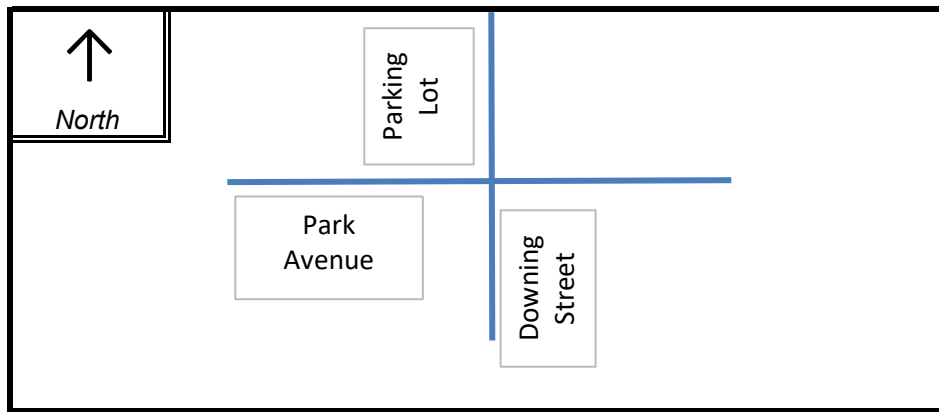
~ INTERSECTION DATA ~

MAJOR STREET : Park Avenue (Route 12)

MINOR STREET(S) : Downing Street

Parking Lot

**INTERSECTION
DIAGRAM**



PEAK HOUR VOLUMES

APPROACH :	1	2	3	4	5	Total Peak Hourly Approach Volume
DIRECTION :	NB	SB	EB	WB		
PEAK HOURLY VOLUMES (PM) :	63	2	668	949		1,682

" K " FACTOR : INTERSECTION ADT (**V**) = TOTAL DAILY APPROACH VOLUME :

TOTAL # OF CRASHES : # OF YEARS : AVERAGE # OF CRASHES PER YEAR (**A**) :

CRASH RATE CALCULATION : RATE = $\frac{(A * 1,000,000)}{(V * 365)}$

Comments : The District 3 crash rate is 0.89 for signalized intersection

Project Title & Date: 487 Park Avenue TIAS Worcester, MA

TRIP GENERATION CALCULATIONS

Query

Filter

DATA SOURCE:

Trip Generation Manual, 11th Ed

SEARCH BY LAND USE CODE:

221



LAND USE GROUP:

(200-299) Residential

LAND USE :

221 - Multifamily Housing (Mid-Rise)

LAND USE SUBCATEGORY:

Not Close to Rail Transit

SETTING/LOCATION:

General Urban/Suburban

INDEPENDENT VARIABLE (IV):

Dwelling Units

TIME PERIOD:

Weekday, Peak Hour of Adjacent Street Traffic

TRIP TYPE:

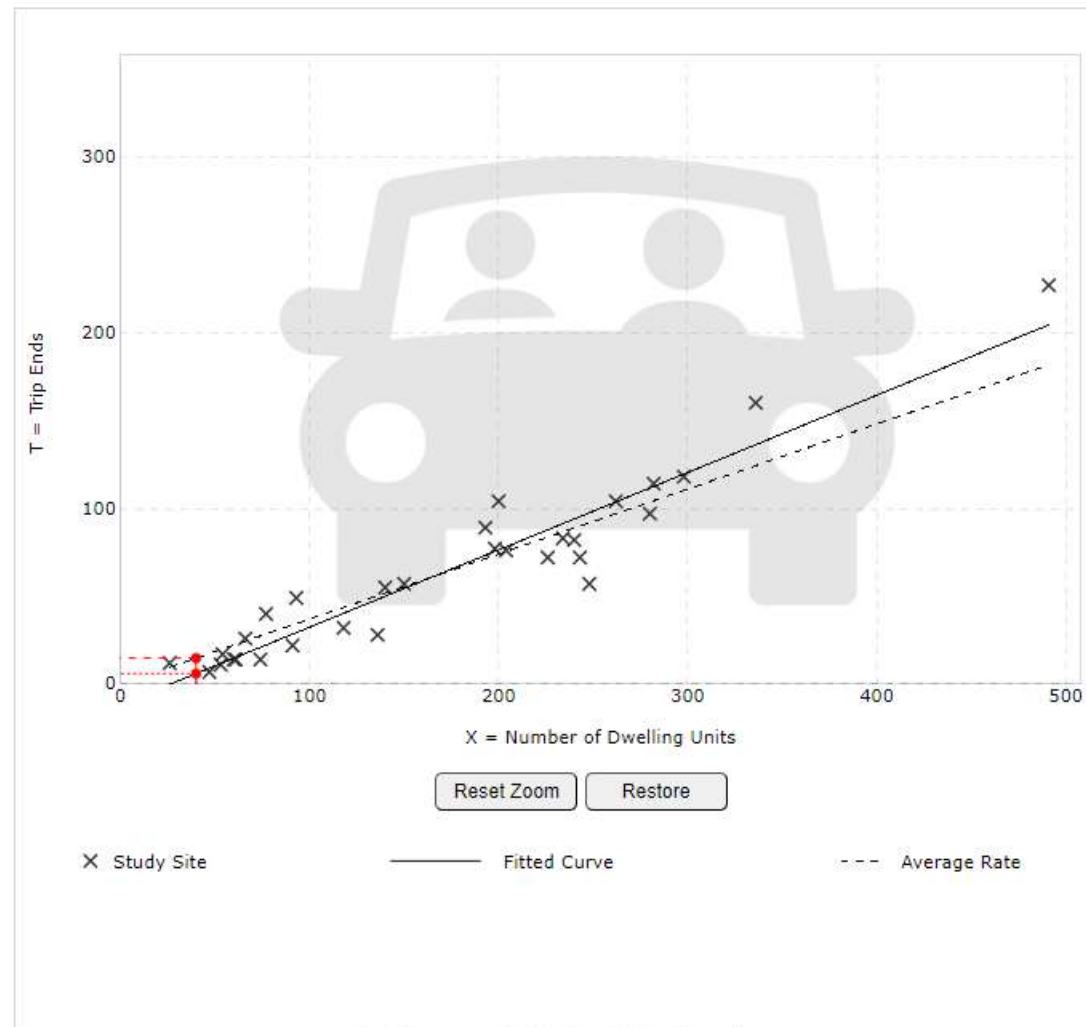
Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:

40

Calculate

Data Plot and Equation



Use the mouse wheel to Zoom Out or Zoom In.
Hover the mouse pointer on data points to view X and T values.

DATA STATISTICS

Land Use:

Multifamily Housing (Mid-Rise) - Not Close to Rail Transit (221) [Click for Description and Data Plots](#)

Independent Variable:

Dwelling Units

Time Period:

Weekday

Peak Hour of Adjacent Street Traffic

One Hour Between 7 and 9 a.m.

Setting/Location:

General Urban/Suburban

Trip Type:

Vehicle

Number of Studies:

30

Avg. Num. of Dwelling Units:

173

Average Rate:

0.37

Range of Rates:

0.15 - 0.53

Standard Deviation:

0.09

Fitted Curve Equation:

 $T = 0.44(X) - 11.61$ R^2 :

0.91

Directional Distribution:

23% entering, 77% exiting

Calculated Trip Ends:

Average Rate: 15 (Total), 3 (Entry), 12 (Exit)

Fitted Curve: 6 (Total), 1 (Entry), 5 (Exit)

Query

Filter

Data Plot and Equation

DATA SOURCE:

Trip Generation Manual, 11th Ed

SEARCH BY LAND USE CODE:

221



LAND USE GROUP:

(200-299) Residential

LAND USE :

221 - Multifamily Housing (Mid-Rise)

LAND USE SUBCATEGORY:

Not Close to Rail Transit

SETTING/LOCATION:

General Urban/Suburban

INDEPENDENT VARIABLE (IV):

Dwelling Units

TIME PERIOD:

Weekday

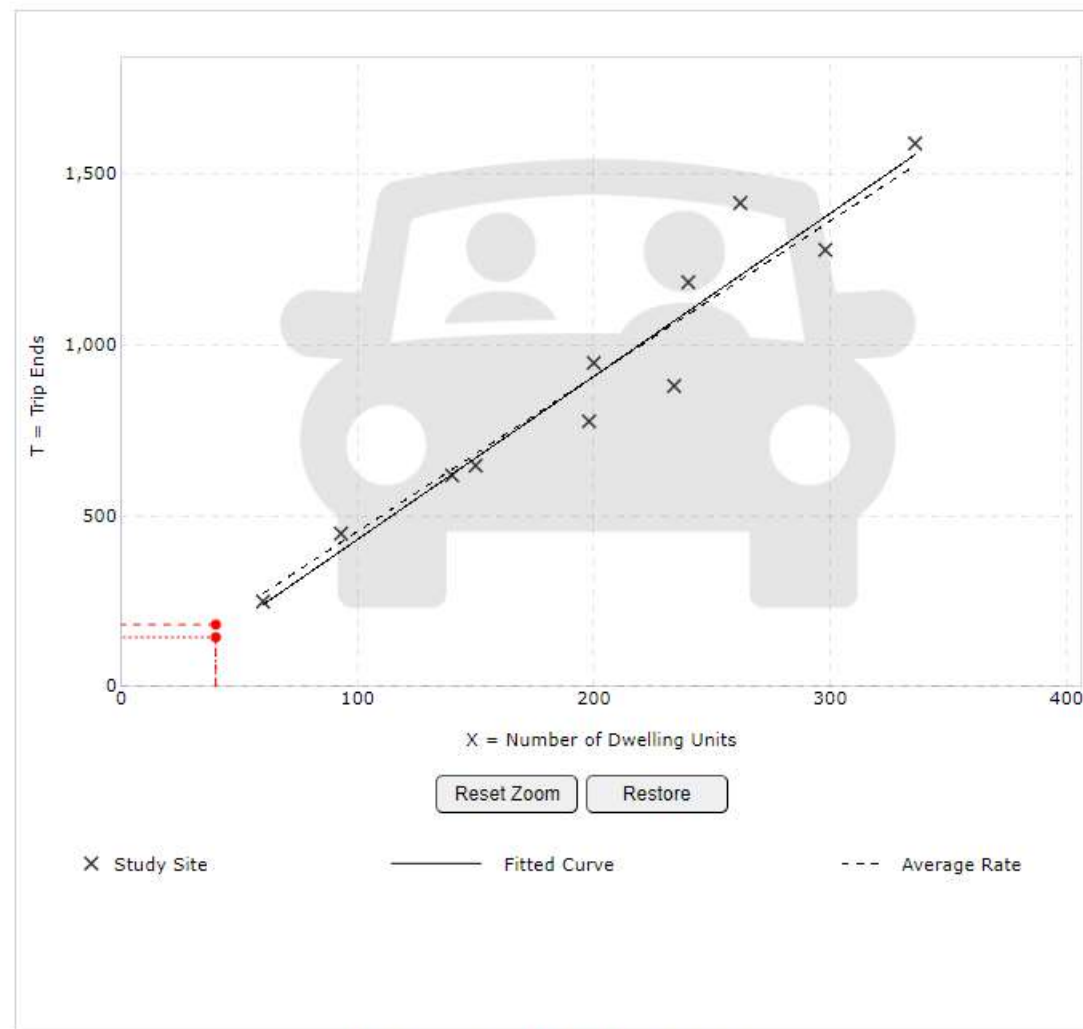
TRIP TYPE:

Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:

40

Calculate



Use the mouse wheel to Zoom Out or Zoom In.
 Hover the mouse pointer on data points to view X and T values.

DATA STATISTICS

Land Use:

Multifamily Housing (Mid-Rise) - Not Close to Rail Transit (221) [Click for Description and Data Plots](#)

Independent Variable:

Dwelling Units

Time Period:

Weekday

Setting/Location:

General Urban/Suburban

Trip Type:

Vehicle

Number of Studies:

11

Avg. Num. of Dwelling Units:

201

Average Rate:

4.54

Range of Rates:

3.76 - 5.40

Standard Deviation:

0.51

Fitted Curve Equation:

 $T = 4.77(X) - 46.46$ R²:

0.93

Directional Distribution:

50% entering, 50% exiting

Calculated Trip Ends:

Average Rate: 182 (Total), 91 (Entry), 91 (Exit)

Fitted Curve: 144 (Total), 72 (Entry), 72 (Exit)

Query

Filter

DATA SOURCE:

Trip Generation Manual, 11th Ed

SEARCH BY LAND USE CODE:

221



LAND USE GROUP:

(200-299) Residential

LAND USE :

221 - Multifamily Housing (Mid-Rise)

LAND USE SUBCATEGORY:

Not Close to Rail Transit

SETTING/LOCATION:

General Urban/Suburban

INDEPENDENT VARIABLE (IV):

Dwelling Units

TIME PERIOD:

Weekday, Peak Hour of Adjacent Street Traffic

TRIP TYPE:

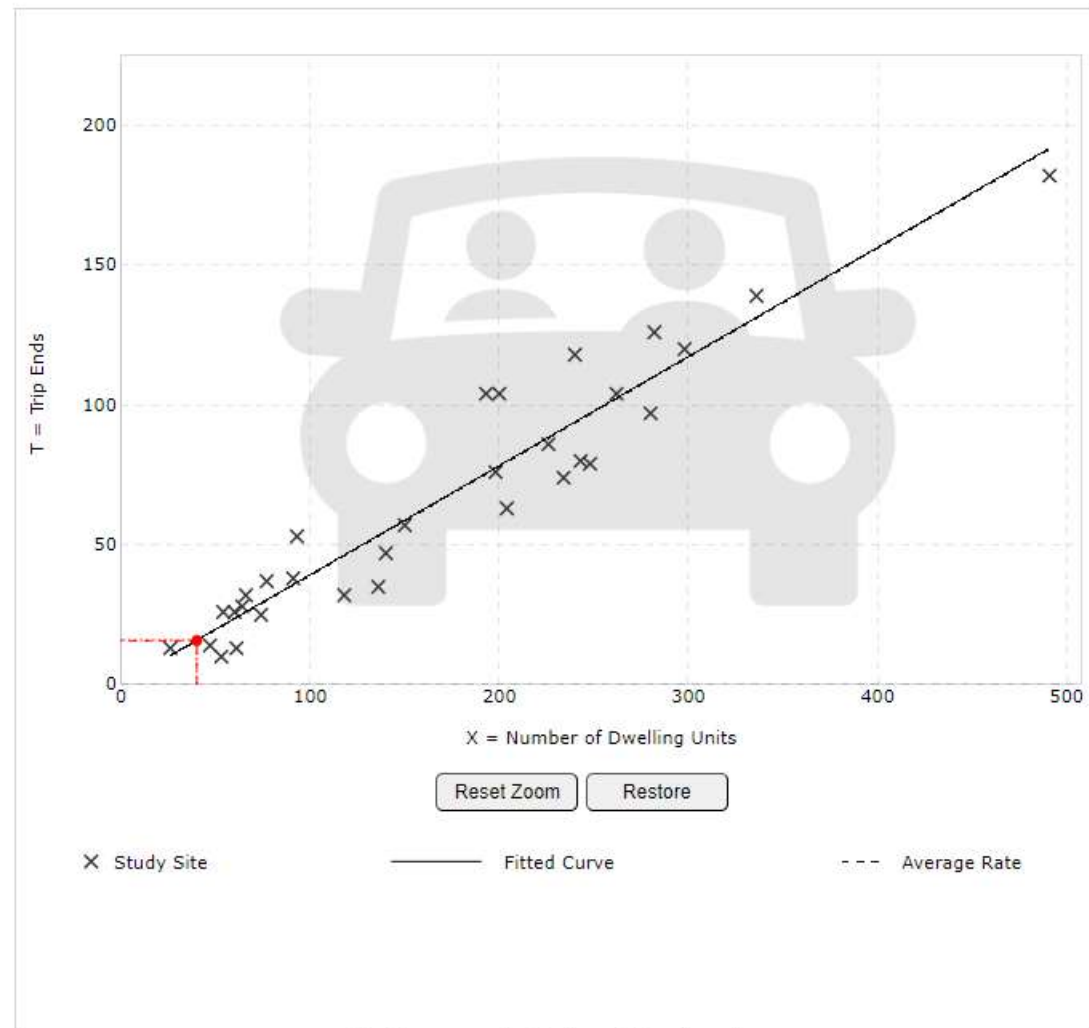
Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:

40

Calculate

Data Plot and Equation



Use the mouse wheel to Zoom Out or Zoom In.
 Hover the mouse pointer on data points to view X and T values.

DATA STATISTICS

Land Use:

Multifamily Housing (Mid-Rise) - Not Close to Rail Transit (221) [Click for Description and Data Plots](#)

Independent Variable:

Dwelling Units

Time Period:

Weekday

Peak Hour of Adjacent Street Traffic

One Hour Between 4 and 6 p.m.

Setting/Location:

General Urban/Suburban

Trip Type:

Vehicle

Number of Studies:

31

Avg. Num. of Dwelling Units:

169

Average Rate:

0.39

Range of Rates:

0.19 - 0.57

Standard Deviation:

0.08

Fitted Curve Equation:

 $T = 0.39(X) + 0.34$ R^2 :

0.91

Directional Distribution:

61% entering, 39% exiting

Calculated Trip Ends:

Average Rate: 16 (Total), 10 (Entry), 6 (Exit)

Fitted Curve: 16 (Total), 10 (Entry), 6 (Exit)

Query

Filter

DATA SOURCE:

Trip Generation Manual, 11th Ed

SEARCH BY LAND USE CODE:

936



LAND USE GROUP:

(900-999) Services

LAND USE:

936 - Coffee/Donut Shop without Drive-Thru

LAND USE SUBCATEGORY:

All Sites

SETTING/LOCATION:

General Urban/Suburban

INDEPENDENT VARIABLE (IV):

1000 Sq. Ft. GFA

TIME PERIOD:

Weekday, Peak Hour of Adjacent Street Traffic

TRIP TYPE:

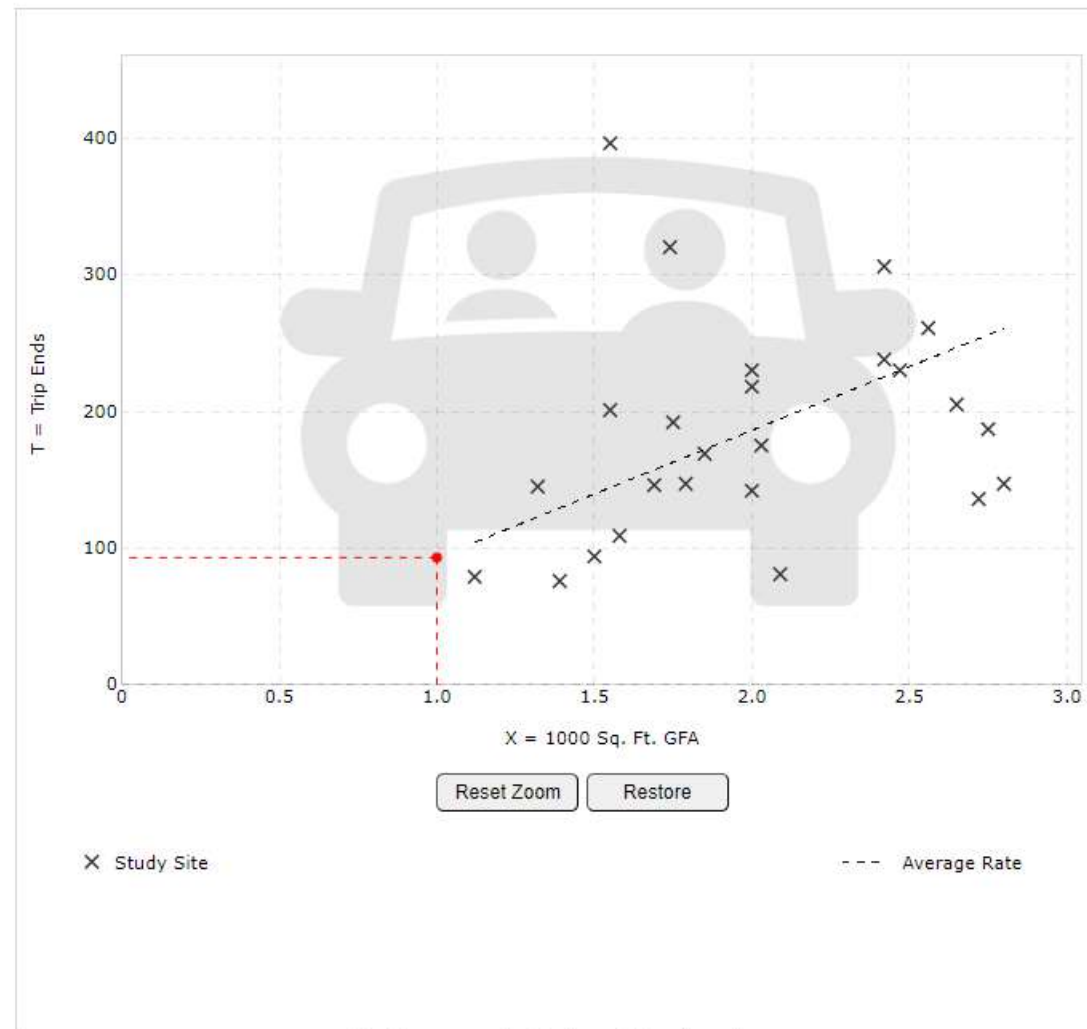
Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:

1

Calculate

Data Plot and Equation



Use the mouse wheel to Zoom Out or Zoom In.
 Hover the mouse pointer on data points to view X and T values.

DATA STATISTICS

Land Use:

Coffee/Donut Shop without Drive-Through Window
 (936) [Click for Description and Data Plots](#)

Independent Variable:

1000 Sq. Ft. GFA

Time Period:

Weekday

Peak Hour of Adjacent Street Traffic

One Hour Between 7 and 9 a.m.

Setting/Location:

General Urban/Suburban

Trip Type:

Vehicle

Number of Studies:

25

Avg. 1000 Sq. Ft. GFA:

2

Average Rate:

93.08

Range of Rates:

38.76 - 255.48

Standard Deviation:

42.71

Fitted Curve Equation:

Not Given

 R^2 :

Directional Distribution:

51% entering, 49% exiting

Calculated Trip Ends:

Average Rate: 93 (Total), 47 (Entry), 46 (Exit)

Query

Filter

DATA SOURCE:

Trip Generation Manual, 11th Ed

SEARCH BY LAND USE CODE:

936



LAND USE GROUP:

(900-999) Services

LAND USE :

936 - Coffee/Donut Shop without Drive-Throu

LAND USE SUBCATEGORY:

All Sites

SETTING/LOCATION:

General Urban/Suburban

INDEPENDENT VARIABLE (IV):

1000 Sq. Ft. GFA

TIME PERIOD:

Weekday, Peak Hour of Adjacent Street Traffic

TRIP TYPE:

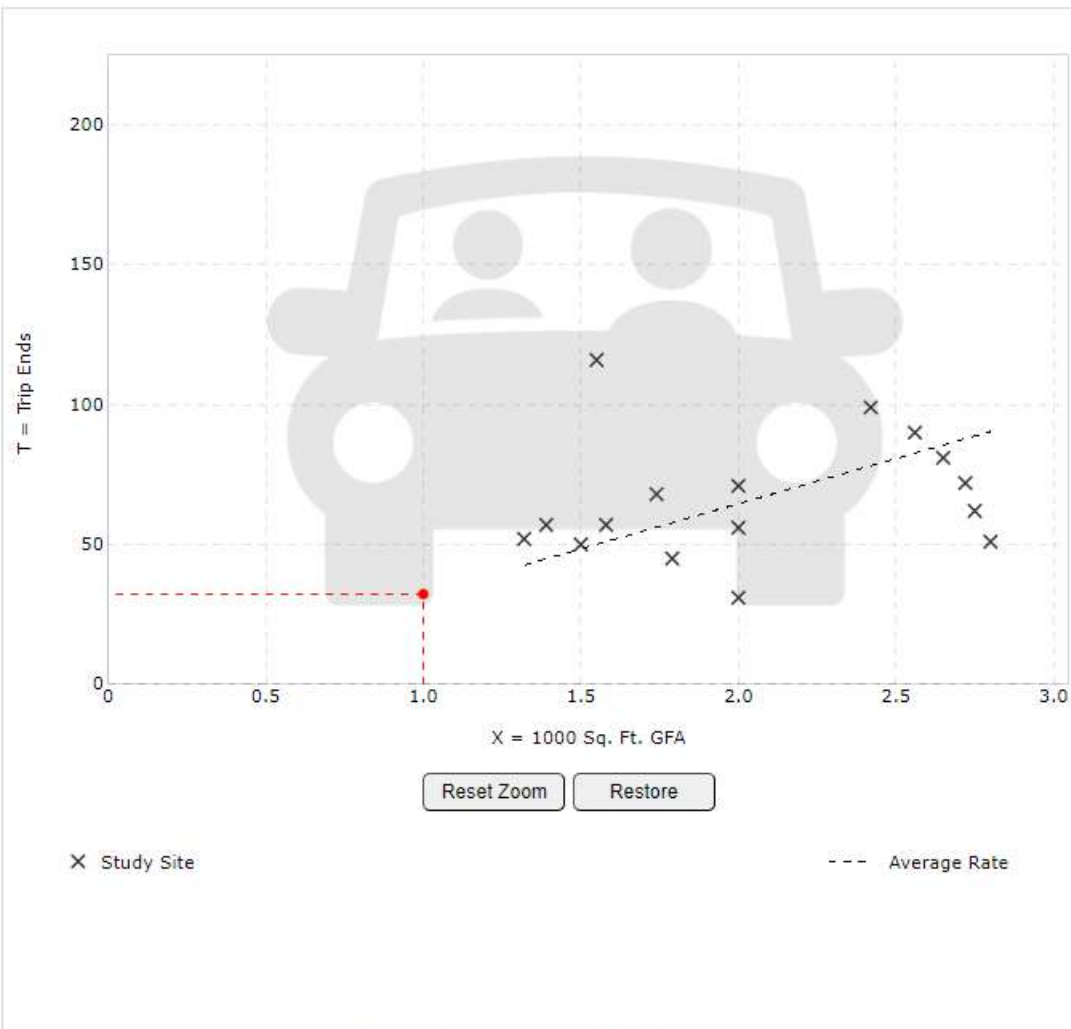
Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:

1

Calculate

Data Plot and Equation



Use the mouse wheel to Zoom Out or Zoom In.
 Hover the mouse pointer on data points to view X and T values.

DATA STATISTICS

Land Use:

Coffee/Donut Shop without Drive-Through Window (936) [Click for Description and Data Plots](#)

Independent Variable:

1000 Sq. Ft. GFA

Time Period:

Weekday

Peak Hour of Adjacent Street Traffic

One Hour Between 4 and 6 p.m.

Setting/Location:

General Urban/Suburban

Trip Type:

Vehicle

Number of Studies:

16

Avg. 1000 Sq. Ft. GFA:

2

Average Rate:

32.29

Range of Rates:

15.50 - 74.84

Standard Deviation:

12.64

Fitted Curve Equation:

Not Given

 R^2 :

Directional Distribution:

50% entering, 50% exiting

Calculated Trip Ends:

Average Rate: 32 (Total), 16 (Entry), 16 (Exit)

Query

Filter

DATA SOURCE:

Trip Generation Manual, 11th Ed

SEARCH BY LAND USE CODE:

943



LAND USE GROUP:

(900-999) Services

LAND USE :

943 - Automobile Parts and Service Center

LAND USE SUBCATEGORY:

All Sites

SETTING/LOCATION:

General Urban/Suburban

INDEPENDENT VARIABLE (IV):

1000 Sq. Ft. GFA

TIME PERIOD:

Weekday, Peak Hour of Adjacent Street Traffic

TRIP TYPE:

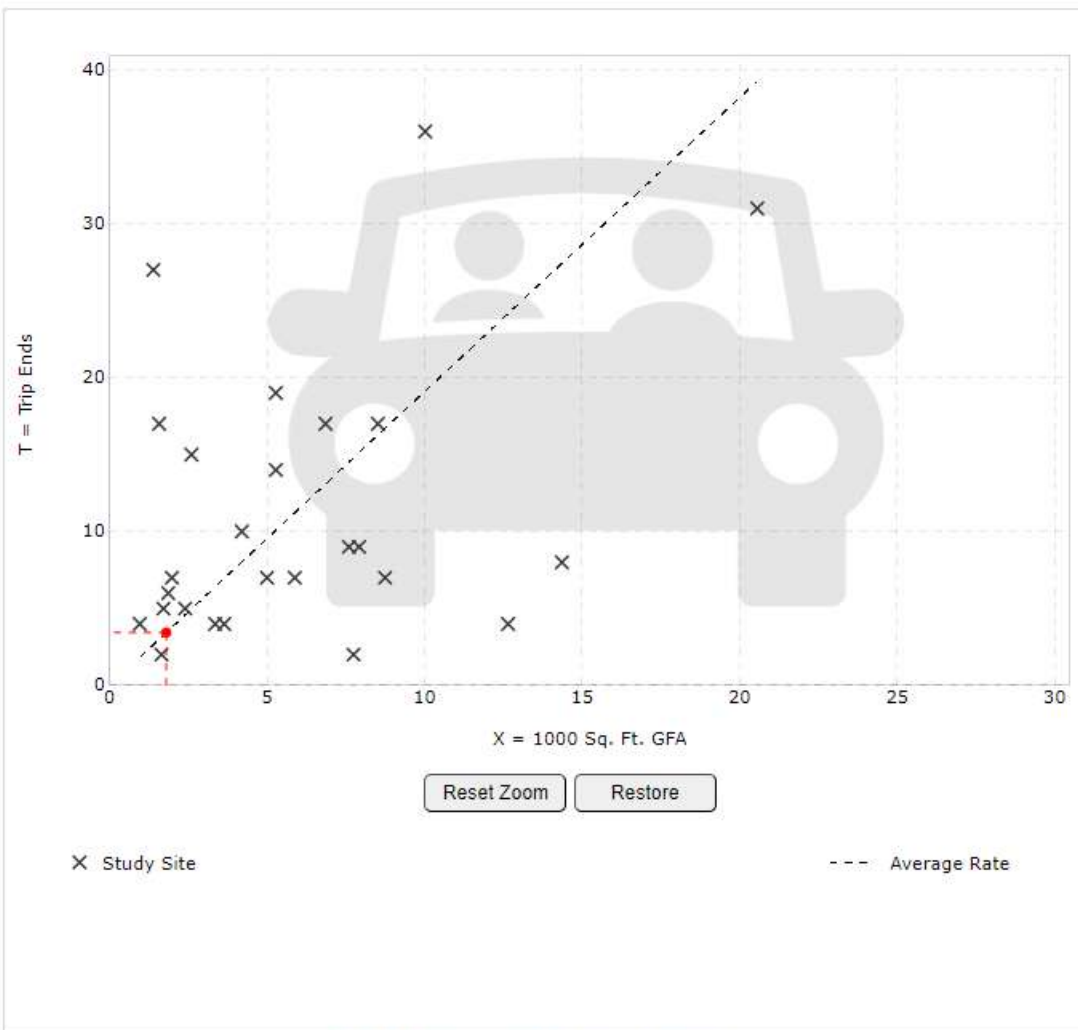
Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:

1.8

Calculate

Data Plot and Equation



Use the mouse wheel to Zoom Out or Zoom In.
Hover the mouse pointer on data points to view X and T values.

DATA STATISTICS

Land Use:

Automobile Parts and Service Center (943) [Click for Description and Data Plots](#)

Independent Variable:

1000 Sq. Ft. GFA

Time Period:

Weekday

Peak Hour of Adjacent Street Traffic
One Hour Between 7 and 9 a.m.

Setting/Location:

General Urban/Suburban

Trip Type:

Vehicle

Number of Studies:

26

Avg. 1000 Sq. Ft. GFA:

6

Average Rate:

1.91

Range of Rates:

0.26 - 19.40

Standard Deviation:

2.28

Fitted Curve Equation:

Not Given

R²:

Directional Distribution:

72% entering, 28% exiting

Calculated Trip Ends:

Average Rate: 3 (Total), 2 (Entry), 1 (Exit)

Query

Filter

Data Plot and Equation

DATA STATISTICS

DATA SOURCE:

Trip Generation Manual, 11th Ed

SEARCH BY LAND USE CODE:

943



LAND USE GROUP:

(900-999) Services

LAND USE :

943 - Automobile Parts and Service Center

LAND USE SUBCATEGORY:

All Sites

SETTING/LOCATION:

General Urban/Suburban

INDEPENDENT VARIABLE (IV):

1000 Sq. Ft. GFA

TIME PERIOD:

Weekday

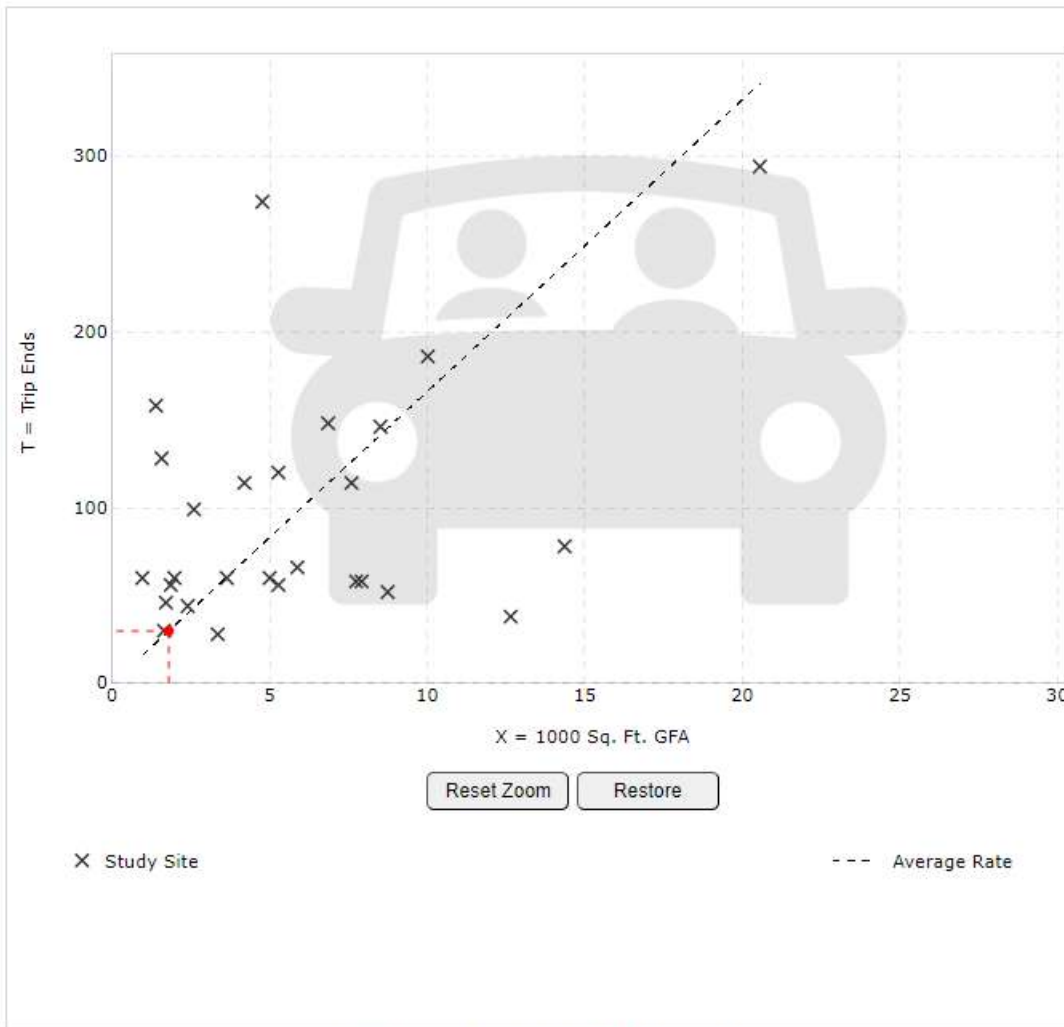
TRIP TYPE:

Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:

1.8

Calculate



Land Use:

Automobile Parts and Service Center (943) [Click for Description and Data Plots](#)

Independent Variable:

1000 Sq. Ft. GFA

Time Period:

Weekday

Setting/Location:

General Urban/Suburban

Trip Type:

Vehicle

Number of Studies:

27

Avg. 1000 Sq. Ft. GFA:

6

Average Rate:

16.60

Range of Rates:

3.00 - 113.51

Standard Deviation:

16.17

Fitted Curve Equation:

Not Given

R²:

Directional Distribution:

50% entering, 50% exiting

Calculated Trip Ends:

Average Rate: 30 (Total), 15 (Entry), 15 (Exit)

Query

Filter

DATA SOURCE:

Trip Generation Manual, 11th Ed

SEARCH BY LAND USE CODE:

943



LAND USE GROUP:

(900-999) Services

LAND USE:

943 - Automobile Parts and Service Center

LAND USE SUBCATEGORY:

All Sites

SETTING/LOCATION:

General Urban/Suburban

INDEPENDENT VARIABLE (IV):

1000 Sq. Ft. GFA

TIME PERIOD:

Weekday, Peak Hour of Adjacent Street Traffic

TRIP TYPE:

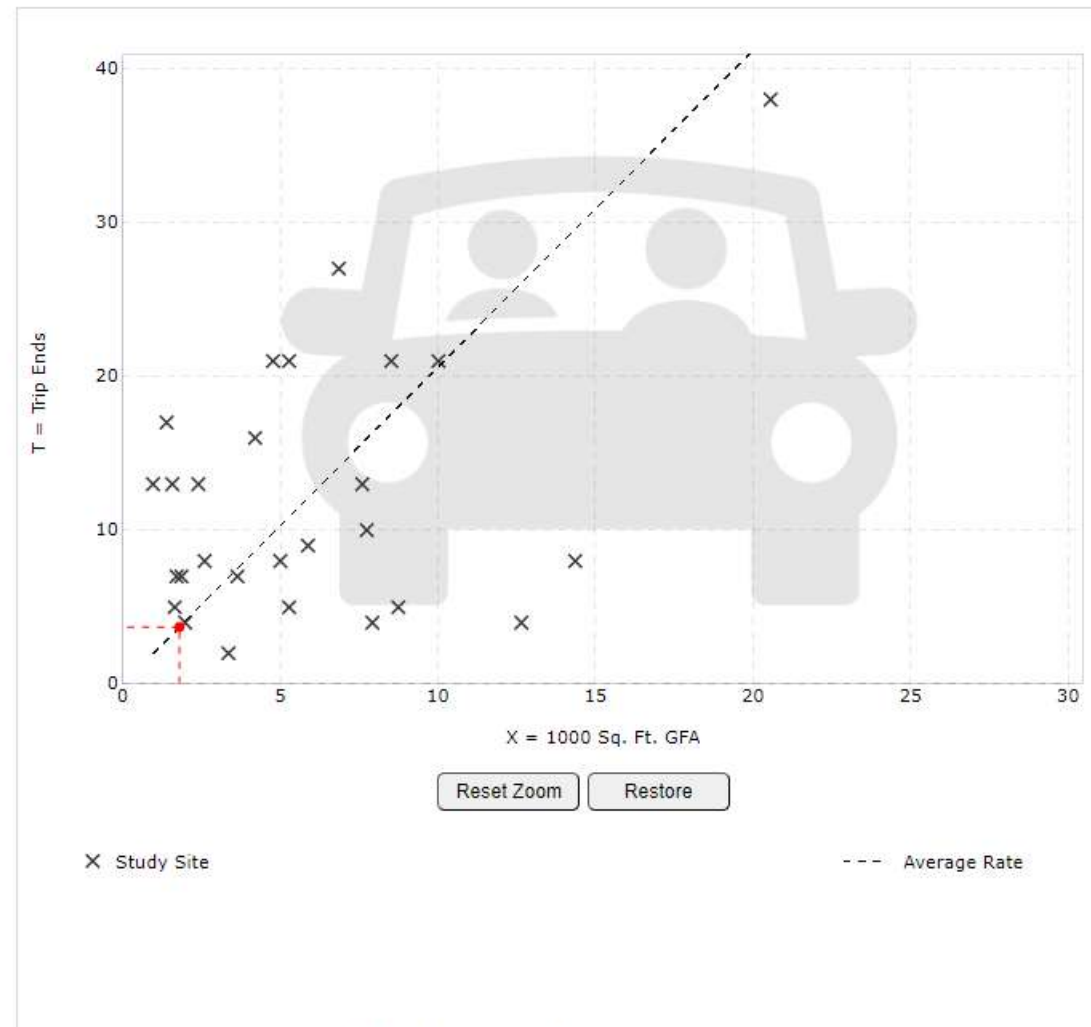
Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:

1.8

Calculate

Data Plot and Equation



DATA STATISTICS

Land Use:

Automobile Parts and Service Center (943) [Click for Description and Data Plots](#)

Independent Variable:

1000 Sq. Ft. GFA

Time Period:

Weekday

Peak Hour of Adjacent Street Traffic
One Hour Between 4 and 6 p.m.

Setting/Location:

General Urban/Suburban

Trip Type:

Vehicle

Number of Studies:

27

Avg. 1000 Sq. Ft. GFA:

6

Average Rate:

2.06

Range of Rates:

0.32 - 13.54

Standard Deviation:

1.97

Fitted Curve Equation:

Not Given

 R^2 :

Directional Distribution:

39% entering, 61% exiting

Calculated Trip Ends:

Average Rate: 4 (Total), 1 (Entry), 3 (Exit)



INTERSECTION CAPACITY ANALYSIS WORKSHEETS



Lanes, Volumes, Timings
1: Downing Street & Park Avenue (Route 12)

Existing Conditions AM



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR	Ø3
Lane Configurations	↑↑			↑↑	↑↑		
Traffic Volume (vph)	839	9	31	497	12	12	
Future Volume (vph)	839	9	31	497	12	12	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	
Lane Width (ft)	11	11	11	11	15	15	
Grade (%)	2%			0%	3%		
Lane Util. Factor	0.95	0.95	0.95	0.95	1.00	1.00	
Ped Bike Factor				1.00	1.00		
Frt	0.998				0.939		
Flt Protected				0.996	0.973		
Satd. Flow (prot)	3317	0	0	3383	1721	0	
Flt Permitted				0.804	0.973		
Satd. Flow (perm)	3317	0	0	2731	1718	0	
Right Turn on Red		Yes				Yes	
Satd. Flow (RTOR)	3				20		
Link Speed (mph)	30			30	30		
Link Distance (ft)	351			179	249		
Travel Time (s)	8.0			4.1	5.7		
Confl. Peds. (#/hr)			7		2		
Peak Hour Factor	0.88	0.56	0.55	0.85	0.50	0.60	
Heavy Vehicles (%)	4%	0%	0%	3%	17%	0%	
Adj. Flow (vph)	953	16	56	585	24	20	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	969	0	0	641	44	0	
Enter Blocked Intersection	No	No	No	No	No	No	
Lane Alignment	Left	Right	Left	Left	Left	Right	
Median Width(ft)	0			0	15		
Link Offset(ft)	0			0	0		
Crosswalk Width(ft)	16			16	16		
Two way Left Turn Lane							
Headway Factor	1.06	1.06	1.04	1.04	0.90	0.90	
Turning Speed (mph)		9	15		15	9	
Number of Detectors	2		1	2	1		
Detector Template	Thru		Left	Thru	Left		
Leading Detector (ft)	100		20	100	20		
Trailing Detector (ft)	0		0	0	0		
Detector 1 Position(ft)	0		0	0	0		
Detector 1 Size(ft)	6		20	6	20		
Detector 1 Type	Cl+Ex		Cl+Ex	Cl+Ex	Cl+Ex		
Detector 1 Channel							
Detector 1 Extend (s)	0.0		0.0	0.0	0.0		
Detector 1 Queue (s)	0.0		0.0	0.0	0.0		
Detector 1 Delay (s)	0.0		0.0	0.0	0.0		
Detector 2 Position(ft)	94			94			
Detector 2 Size(ft)	6			6			
Detector 2 Type	Cl+Ex			Cl+Ex			
Detector 2 Channel							
Detector 2 Extend (s)	0.0			0.0			
Turn Type	NA		Perm	NA	Perm		

Lanes, Volumes, Timings
1: Downing Street & Park Avenue (Route 12)

Existing Conditions AM

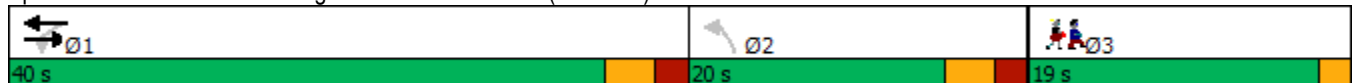


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR	Ø3
Protected Phases	1			1			3
Permitted Phases			1		2		
Detector Phase	1		1	1	2		
Switch Phase							
Minimum Initial (s)	8.0		8.0	8.0	6.0		5.0
Minimum Split (s)	23.0		23.0	23.0	11.0		19.0
Total Split (s)	40.0		40.0	40.0	20.0		19.0
Total Split (%)	50.6%		50.6%	50.6%	25.3%		24%
Maximum Green (s)	35.0		35.0	35.0	15.0		17.0
Yellow Time (s)	3.0		3.0	3.0	3.0		2.0
All-Red Time (s)	2.0		2.0	2.0	2.0		0.0
Lost Time Adjust (s)	0.0			0.0	0.0		
Total Lost Time (s)	5.0			5.0	5.0		
Lead/Lag	Lead		Lead	Lead	Lag		
Lead-Lag Optimize?	Yes		Yes	Yes	Yes		
Vehicle Extension (s)	3.0		3.0	3.0	3.0		3.0
Recall Mode	Max		Max	Max	None		Ped
Walk Time (s)							7.0
Flash Dont Walk (s)							11.0
Pedestrian Calls (#/hr)							9
Act Effct Green (s)	35.3			35.3	6.9		
Actuated g/C Ratio	0.52			0.52	0.10		
v/c Ratio	0.56			0.45	0.23		
Control Delay	13.5			12.5	22.8		
Queue Delay	0.0			0.0	0.0		
Total Delay	13.5			12.5	22.8		
LOS	B			B	C		
Approach Delay	13.5			12.5	22.8		
Approach LOS	B			B	C		

Intersection Summary

Area Type:	Other
Cycle Length:	79
Actuated Cycle Length:	67.5
Natural Cycle:	55
Control Type:	Semi Act-Uncoord
Maximum v/c Ratio:	0.56
Intersection Signal Delay:	13.3
Intersection LOS:	B
Intersection Capacity Utilization:	50.3%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 1: Downing Street & Park Avenue (Route 12)



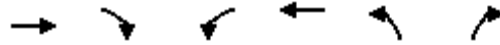
1: Downing Street & Park Avenue (Route 12)



Lane Group	EBT	WBT	NBL
Lane Group Flow (vph)	969	641	44
v/c Ratio	0.56	0.45	0.23
Control Delay	13.5	12.5	22.8
Queue Delay	0.0	0.0	0.0
Total Delay	13.5	12.5	22.8
Queue Length 50th (ft)	151	93	10
Queue Length 95th (ft)	215	133	17
Internal Link Dist (ft)	271	99	169
Turn Bay Length (ft)			
Base Capacity (vph)	1736	1428	400
Starvation Cap Reductn	0	0	0
Spillback Cap Reductn	0	0	0
Storage Cap Reductn	0	0	0
Reduced v/c Ratio	0.56	0.45	0.11
Intersection Summary			

HCM Signalized Intersection Capacity Analysis
 1: Downing Street & Park Avenue (Route 12)

Existing Conditions AM



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↑↑	
Traffic Volume (vph)	839	9	31	497	12	12
Future Volume (vph)	839	9	31	497	12	12
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	11	11	11	11	15	15
Grade (%)	2%			0%	3%	
Total Lost time (s)	5.0			5.0	5.0	
Lane Util. Factor	0.95			0.95	1.00	
Frbp, ped/bikes	1.00			1.00	1.00	
Flpb, ped/bikes	1.00			1.00	0.99	
Frt	1.00			1.00	0.94	
Flt Protected	1.00			1.00	0.97	
Satd. Flow (prot)	3316			3381	1712	
Flt Permitted	1.00			0.80	0.97	
Satd. Flow (perm)	3316			2730	1712	
Peak-hour factor, PHF	0.88	0.56	0.55	0.85	0.50	0.60
Adj. Flow (vph)	953	16	56	585	24	20
RTOR Reduction (vph)	1	0	0	0	19	0
Lane Group Flow (vph)	968	0	0	641	25	0
Confl. Peds. (#/hr)			7		2	
Heavy Vehicles (%)	4%	0%	0%	3%	17%	0%
Turn Type	NA		Perm	NA	Perm	
Protected Phases	1			1		
Permitted Phases			1		2	
Actuated Green, G (s)	35.3			35.3	4.2	
Effective Green, g (s)	35.3			35.3	4.2	
Actuated g/C Ratio	0.51			0.51	0.06	
Clearance Time (s)	5.0			5.0	5.0	
Vehicle Extension (s)	3.0			3.0	3.0	
Lane Grp Cap (vph)	1679			1382	103	
v/s Ratio Prot	c0.29					
v/s Ratio Perm				0.23	c0.01	
v/c Ratio	0.58			0.46	0.24	
Uniform Delay, d1	12.0			11.1	31.2	
Progression Factor	1.00			1.00	1.00	
Incremental Delay, d2	1.4			1.1	1.2	
Delay (s)	13.4			12.2	32.5	
Level of Service	B			B	C	
Approach Delay (s)	13.4			12.2	32.5	
Approach LOS	B			B	C	

Intersection Summary

HCM 2000 Control Delay	13.5	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.37		
Actuated Cycle Length (s)	69.7	Sum of lost time (s)	12.0
Intersection Capacity Utilization	50.3%	ICU Level of Service	A
Analysis Period (min)	15		

c Critical Lane Group

Lanes, Volumes, Timings
3: Maywood Street & Park Avenue (Route 12)

Existing Conditions AM



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕			↕↕			↕			↕	
Traffic Volume (vph)	15	810	30	23	371	39	42	66	45	67	56	16
Future Volume (vph)	15	810	30	23	371	39	42	66	45	67	56	16
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	11	11	11	11	11	11	16	16	16	16	16	16
Grade (%)		2%			2%			2%			2%	
Lane Util. Factor	0.95	0.95	0.95	0.95	0.95	0.95	1.00	1.00	1.00	1.00	1.00	1.00
Ped Bike Factor		1.00			1.00			1.00			1.00	
Frt		0.992			0.984			0.961			0.975	
Flt Protected		0.999			0.997			0.988			0.979	
Satd. Flow (prot)	0	3351	0	0	3246	0	0	1996	0	0	1961	0
Flt Permitted		0.932			0.839			0.837			0.673	
Satd. Flow (perm)	0	3126	0	0	2731	0	0	1688	0	0	1346	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		8			19			23			12	
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		189			269			185			216	
Travel Time (s)		4.3			6.1			4.2			4.9	
Confl. Peds. (#/hr)	1			8			6			3		
Peak Hour Factor	0.63	0.94	0.63	0.72	0.82	0.70	0.88	0.75	0.80	0.88	0.82	0.50
Heavy Vehicles (%)	13%	2%	0%	4%	4%	8%	2%	2%	0%	6%	3%	0%
Adj. Flow (vph)	24	862	48	32	452	56	48	88	56	76	68	32
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	934	0	0	540	0	0	192	0	0	176	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		0			0			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.06	1.06	1.06	1.06	1.06	1.06	0.86	0.86	0.86	0.86	0.86	0.86
Turning Speed (mph)	15		9	15		9	15		9	15		9
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		2			2			4			4	
Permitted Phases	2			2			4			4		
Minimum Split (s)	28.0	28.0		28.0	28.0		13.0	13.0		13.0	13.0	
Total Split (s)	35.0	35.0		35.0	35.0		17.0	17.0		17.0	17.0	
Total Split (%)	46.1%	46.1%		46.1%	46.1%		22.4%	22.4%		22.4%	22.4%	
Maximum Green (s)	30.0	30.0		30.0	30.0		12.0	12.0		12.0	12.0	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		5.0			5.0			5.0			5.0	
Lead/Lag							Lag	Lag		Lag	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Walk Time (s)												
Flash Dont Walk (s)												
Pedestrian Calls (#/hr)												
Act Effect Green (s)		30.0			30.0			12.0			12.0	

Lane Group	Ø3
Lane Configurations	
Traffic Volume (vph)	
Future Volume (vph)	
Ideal Flow (vphpl)	
Lane Width (ft)	
Grade (%)	
Lane Util. Factor	
Ped Bike Factor	
Frt	
Flt Protected	
Satd. Flow (prot)	
Flt Permitted	
Satd. Flow (perm)	
Right Turn on Red	
Satd. Flow (RTOR)	
Link Speed (mph)	
Link Distance (ft)	
Travel Time (s)	
Confl. Peds. (#/hr)	
Peak Hour Factor	
Heavy Vehicles (%)	
Adj. Flow (vph)	
Shared Lane Traffic (%)	
Lane Group Flow (vph)	
Enter Blocked Intersection	
Lane Alignment	
Median Width(ft)	
Link Offset(ft)	
Crosswalk Width(ft)	
Two way Left Turn Lane	
Headway Factor	
Turning Speed (mph)	
Turn Type	
Protected Phases	3
Permitted Phases	
Minimum Split (s)	24.0
Total Split (s)	24.0
Total Split (%)	32%
Maximum Green (s)	21.0
Yellow Time (s)	2.0
All-Red Time (s)	1.0
Lost Time Adjust (s)	
Total Lost Time (s)	
Lead/Lag	Lead
Lead-Lag Optimize?	Yes
Walk Time (s)	7.0
Flash Dont Walk (s)	16.0
Pedestrian Calls (#/hr)	20
Act Effct Green (s)	

Lanes, Volumes, Timings
 3: Maywood Street & Park Avenue (Route 12)

Existing Conditions AM

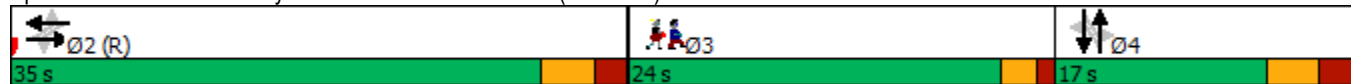


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Actuated g/C Ratio		0.39			0.39			0.16			0.16	
v/c Ratio		0.75			0.50			0.67			0.79	
Control Delay		24.4			18.6			39.9			55.9	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		24.4			18.6			39.9			55.9	
LOS		C			B			D			E	
Approach Delay		24.4			18.6			39.9			55.9	
Approach LOS		C			B			D			E	

Intersection Summary

Area Type:	Other
Cycle Length:	76
Actuated Cycle Length:	76
Offset:	0 (0%), Referenced to phase 2:EBWB and 6:, Start of Green
Natural Cycle:	65
Control Type:	Pretimed
Maximum v/c Ratio:	0.79
Intersection Signal Delay:	27.3
Intersection LOS:	C
Intersection Capacity Utilization	55.6%
ICU Level of Service	B
Analysis Period (min)	15

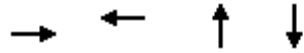
Splits and Phases: 3: Maywood Street & Park Avenue (Route 12)



Lane Group	Ø3
Actuated g/C Ratio	
v/c Ratio	
Control Delay	
Queue Delay	
Total Delay	
LOS	
Approach Delay	
Approach LOS	
Intersection Summary	

Queues
 3: Maywood Street & Park Avenue (Route 12)

Existing Conditions AM



Lane Group	EBT	WBT	NBT	SBT
Lane Group Flow (vph)	934	540	192	176
v/c Ratio	0.75	0.50	0.67	0.79
Control Delay	24.4	18.6	39.9	55.9
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	24.4	18.6	39.9	55.9
Queue Length 50th (ft)	193	94	76	75
Queue Length 95th (ft)	264	122	113	#152
Internal Link Dist (ft)	109	189	105	136
Turn Bay Length (ft)				
Base Capacity (vph)	1238	1089	285	222
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.75	0.50	0.67	0.79

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

HCM Signalized Intersection Capacity Analysis

3: Maywood Street & Park Avenue (Route 12)

Existing Conditions AM



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕			↕↕			↕↕			↕↕	
Traffic Volume (vph)	15	810	30	23	371	39	42	66	45	67	56	16
Future Volume (vph)	15	810	30	23	371	39	42	66	45	67	56	16
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	11	11	11	11	11	11	16	16	16	16	16	16
Grade (%)		2%			2%			2%			2%	
Total Lost time (s)		5.0			5.0			5.0			5.0	
Lane Util. Factor		0.95			0.95			1.00			1.00	
Frbp, ped/bikes		1.00			1.00			1.00			1.00	
Flpb, ped/bikes		1.00			1.00			1.00			1.00	
Frt		0.99			0.98			0.96			0.98	
Flt Protected		1.00			1.00			0.99			0.98	
Satd. Flow (prot)		3351			3247			1991			1959	
Flt Permitted		0.93			0.84			0.84			0.67	
Satd. Flow (perm)		3127			2733			1687			1348	
Peak-hour factor, PHF	0.63	0.94	0.63	0.72	0.82	0.70	0.88	0.75	0.80	0.88	0.82	0.50
Adj. Flow (vph)	24	862	48	32	452	56	48	88	56	76	68	32
RTOR Reduction (vph)	0	5	0	0	12	0	0	19	0	0	10	0
Lane Group Flow (vph)	0	929	0	0	529	0	0	173	0	0	166	0
Confl. Peds. (#/hr)	1			8			6			3		
Heavy Vehicles (%)	13%	2%	0%	4%	4%	8%	2%	2%	0%	6%	3%	0%
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		2			2			4			4	
Permitted Phases	2			2			4			4		
Actuated Green, G (s)		30.0			30.0			12.0			12.0	
Effective Green, g (s)		30.0			30.0			12.0			12.0	
Actuated g/C Ratio		0.39			0.39			0.16			0.16	
Clearance Time (s)		5.0			5.0			5.0			5.0	
Lane Grp Cap (vph)		1234			1078			266			212	
v/s Ratio Prot												
v/s Ratio Perm		c0.30			0.19			0.10			c0.12	
v/c Ratio		0.75			0.49			0.65			0.78	
Uniform Delay, d1		19.8			17.3			30.0			30.7	
Progression Factor		1.00			1.00			1.00			1.00	
Incremental Delay, d2		4.3			1.6			11.7			24.4	
Delay (s)		24.1			18.9			41.7			55.2	
Level of Service		C			B			D			E	
Approach Delay (s)		24.1			18.9			41.7			55.2	
Approach LOS		C			B			D			E	
Intersection Summary												
HCM 2000 Control Delay		27.4			HCM 2000 Level of Service			C				
HCM 2000 Volume to Capacity ratio		0.51										
Actuated Cycle Length (s)		76.0			Sum of lost time (s)			13.0				
Intersection Capacity Utilization		55.6%			ICU Level of Service			B				
Analysis Period (min)		15										
c	Critical Lane Group											

Intersection												
Int Delay, s/veh	0											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔			↔				
Traffic Vol, veh/h	2	867	1	0	479	1	0	0	0	0	0	0
Future Vol, veh/h	2	867	1	0	479	1	0	0	0	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	10	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	2	-	-	2	-	-	0	-	-	0	-
Peak Hour Factor	50	87	25	25	100	25	25	25	25	25	25	25
Heavy Vehicles, %	0	4	0	0	3	0	0	0	0	0	0	0
Mvmt Flow	4	997	4	0	479	4	0	0	0	0	0	0

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	483	0	0	1001	0	0	1257	1490	501
Stage 1	-	-	-	-	-	-	1007	1007	-
Stage 2	-	-	-	-	-	-	250	483	-
Critical Hdwy	4.1	-	-	4.1	-	-	6.8	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	5.8	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	5.8	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3
Pot Cap-1 Maneuver	1090	-	-	700	-	-	166	125	521
Stage 1	-	-	-	-	-	-	318	321	-
Stage 2	-	-	-	-	-	-	774	556	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1090	-	-	700	-	-	163	0	521
Mov Cap-2 Maneuver	-	-	-	-	-	-	163	0	-
Stage 1	-	-	-	-	-	-	315	0	-
Stage 2	-	-	-	-	-	-	767	0	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0	0
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR
Capacity (veh/h)	-	1090	-	-	700	-	-
HCM Lane V/C Ratio	-	0.004	-	-	-	-	-
HCM Control Delay (s)	0	8.3	0	-	0	-	-
HCM Lane LOS	A	A	A	-	A	-	-
HCM 95th %tile Q(veh)	-	0	-	-	0	-	-

Intersection						
Int Delay, s/veh	0.4					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↑	
Traffic Vol, veh/h	944	0	8	444	3	5
Future Vol, veh/h	944	0	8	444	3	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	2	-	-	2	3	-
Peak Hour Factor	90	25	67	89	38	63
Heavy Vehicles, %	3	0	0	4	33	0
Mvmt Flow	1049	0	12	499	8	8

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	1049	0	1323
Stage 1	-	-	-	-	1049
Stage 2	-	-	-	-	274
Critical Hdwy	-	-	4.1	-	8.06
Critical Hdwy Stg 1	-	-	-	-	7.06
Critical Hdwy Stg 2	-	-	-	-	7.06
Follow-up Hdwy	-	-	2.2	-	3.83
Pot Cap-1 Maneuver	-	-	671	-	91
Stage 1	-	-	-	-	199
Stage 2	-	-	-	-	633
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	671	-	89
Mov Cap-2 Maneuver	-	-	-	-	89
Stage 1	-	-	-	-	199
Stage 2	-	-	-	-	617

Approach	EB	WB	NB
HCM Control Delay, s	0	0.3	31.8
HCM LOS			D

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	150	-	-	671	-
HCM Lane V/C Ratio	0.106	-	-	0.018	-
HCM Control Delay (s)	31.8	-	-	10.5	0.1
HCM Lane LOS	D	-	-	B	A
HCM 95th %tile Q(veh)	0.3	-	-	0.1	-

Intersection												
Int Delay, s/veh	1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	0	0	0	0	3	0	9	1	1	18	0
Future Vol, veh/h	0	0	0	0	0	3	0	9	1	1	18	0
Conflicting Peds, #/hr	2	0	0	2	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	3	-	-	3	-
Peak Hour Factor	25	25	38	92	92	92	25	75	25	25	56	25
Heavy Vehicles, %	0	0	0	2	2	2	0	0	0	0	0	0
Mvmt Flow	0	0	0	0	0	3	0	12	4	4	32	0

Major/Minor	Minor2		Minor1			Major1		Major2				
Conflicting Flow All	58	56	34	56	54	16	32	0	0	16	0	0
Stage 1	40	40	-	14	14	-	-	-	-	-	-	-
Stage 2	18	16	-	42	40	-	-	-	-	-	-	-
Critical Hdwy	7.1	6.5	6.2	7.12	6.52	6.22	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.1	5.5	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.1	5.5	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.518	4.018	3.318	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	944	839	1045	941	837	1063	1593	-	-	1615	-	-
Stage 1	980	866	-	1006	884	-	-	-	-	-	-	-
Stage 2	1006	886	-	972	862	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	936	836	1043	936	834	1060	1593	-	-	1615	-	-
Mov Cap-2 Maneuver	936	836	-	936	834	-	-	-	-	-	-	-
Stage 1	980	863	-	1006	884	-	-	-	-	-	-	-
Stage 2	1001	886	-	967	859	-	-	-	-	-	-	-

Approach	EB		WB			NB		SB		
HCM Control Delay, s	0		8.4			0		0.8		
HCM LOS	A		A							

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1593	-	-	-	1060	1615	-	-
HCM Lane V/C Ratio	-	-	-	-	0.003	0.002	-	-
HCM Control Delay (s)	0	-	-	0	8.4	7.2	0	-
HCM Lane LOS	A	-	-	A	A	A	A	-
HCM 95th %tile Q(veh)	0	-	-	-	0	0	-	-

Lanes, Volumes, Timings
1: Downing Street & Park Avenue (Route 12)

Existing Conditions PM



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR	Ø3
Lane Configurations	↑↑			↑↑	↑↑		
Traffic Volume (vph)	652	16	24	888	37	25	
Future Volume (vph)	652	16	24	888	37	25	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	
Lane Width (ft)	11	11	11	11	15	15	
Grade (%)	2%			0%	3%		
Lane Util. Factor	0.95	0.95	0.95	0.95	1.00	1.00	
Ped Bike Factor				1.00	0.98		
Frt	0.995				0.943		
Flt Protected				0.999	0.972		
Satd. Flow (prot)	3372	0	0	3452	1844	0	
Flt Permitted				0.928	0.972		
Satd. Flow (perm)	3372	0	0	3207	1825	0	
Right Turn on Red		Yes				Yes	
Satd. Flow (RTOR)	5				32		
Link Speed (mph)	30			30	30		
Link Distance (ft)	351			179	249		
Travel Time (s)	8.0			4.1	5.7		
Confl. Peds. (#/hr)			11		11		
Confl. Bikes (#/hr)						2	
Peak Hour Factor	0.91	0.67	1.00	0.90	0.84	0.78	
Heavy Vehicles (%)	2%	0%	0%	1%	3%	0%	
Adj. Flow (vph)	716	24	24	987	44	32	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	740	0	0	1011	76	0	
Enter Blocked Intersection	No	No	No	No	No	No	
Lane Alignment	Left	Right	Left	Left	Left	Right	
Median Width(ft)	0			0	15		
Link Offset(ft)	0			0	0		
Crosswalk Width(ft)	16			16	16		
Two way Left Turn Lane							
Headway Factor	1.06	1.06	1.04	1.04	0.90	0.90	
Turning Speed (mph)		9	15		15	9	
Number of Detectors	2		1	2	1		
Detector Template	Thru		Left	Thru	Left		
Leading Detector (ft)	100		20	100	20		
Trailing Detector (ft)	0		0	0	0		
Detector 1 Position(ft)	0		0	0	0		
Detector 1 Size(ft)	6		20	6	20		
Detector 1 Type	Cl+Ex		Cl+Ex	Cl+Ex	Cl+Ex		
Detector 1 Channel							
Detector 1 Extend (s)	0.0		0.0	0.0	0.0		
Detector 1 Queue (s)	0.0		0.0	0.0	0.0		
Detector 1 Delay (s)	0.0		0.0	0.0	0.0		
Detector 2 Position(ft)	94			94			
Detector 2 Size(ft)	6			6			
Detector 2 Type	Cl+Ex			Cl+Ex			
Detector 2 Channel							
Detector 2 Extend (s)	0.0			0.0			

Lanes, Volumes, Timings
 1: Downing Street & Park Avenue (Route 12)

Existing Conditions PM

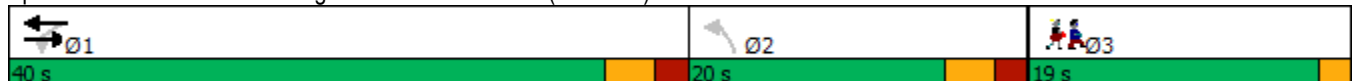


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR	Ø3
Turn Type	NA		Perm	NA	Perm		
Protected Phases	1			1			3
Permitted Phases			1		2		
Detector Phase	1		1	1	2		
Switch Phase							
Minimum Initial (s)	8.0		8.0	8.0	6.0		5.0
Minimum Split (s)	23.0		23.0	23.0	11.0		19.0
Total Split (s)	40.0		40.0	40.0	20.0		19.0
Total Split (%)	50.6%		50.6%	50.6%	25.3%		24%
Maximum Green (s)	35.0		35.0	35.0	15.0		17.0
Yellow Time (s)	3.0		3.0	3.0	3.0		2.0
All-Red Time (s)	2.0		2.0	2.0	2.0		0.0
Lost Time Adjust (s)	0.0			0.0	0.0		
Total Lost Time (s)	5.0			5.0	5.0		
Lead/Lag	Lead		Lead	Lead	Lag		
Lead-Lag Optimize?	Yes		Yes	Yes	Yes		
Vehicle Extension (s)	3.0		3.0	3.0	3.0		3.0
Recall Mode	Max		Max	Max	None		Ped
Walk Time (s)							7.0
Flash Dont Walk (s)							11.0
Pedestrian Calls (#/hr)							27
Act Effct Green (s)	35.2			35.2	7.5		
Actuated g/C Ratio	0.50			0.50	0.11		
v/c Ratio	0.44			0.63	0.34		
Control Delay	13.0			16.0	24.1		
Queue Delay	0.0			0.0	0.0		
Total Delay	13.0			16.0	24.1		
LOS	B			B	C		
Approach Delay	13.0			16.0	24.1		
Approach LOS	B			B	C		

Intersection Summary

Area Type:	Other
Cycle Length:	79
Actuated Cycle Length:	70.3
Natural Cycle:	60
Control Type:	Semi Act-Uncoord
Maximum v/c Ratio:	0.63
Intersection Signal Delay:	15.1
Intersection LOS:	B
Intersection Capacity Utilization:	55.1%
ICU Level of Service:	B
Analysis Period (min):	15

Splits and Phases: 1: Downing Street & Park Avenue (Route 12)



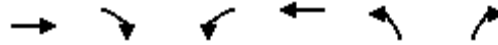
1: Downing Street & Park Avenue (Route 12)



Lane Group	EBT	WBT	NBL
Lane Group Flow (vph)	740	1011	76
v/c Ratio	0.44	0.63	0.34
Control Delay	13.0	16.0	24.1
Queue Delay	0.0	0.0	0.0
Total Delay	13.0	16.0	24.1
Queue Length 50th (ft)	106	167	18
Queue Length 95th (ft)	163	252	50
Internal Link Dist (ft)	271	99	169
Turn Bay Length (ft)			
Base Capacity (vph)	1691	1605	416
Starvation Cap Reductn	0	0	0
Spillback Cap Reductn	0	0	0
Storage Cap Reductn	0	0	0
Reduced v/c Ratio	0.44	0.63	0.18
Intersection Summary			

HCM Signalized Intersection Capacity Analysis
 1: Downing Street & Park Avenue (Route 12)

Existing Conditions PM



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↑↑	
Traffic Volume (vph)	652	16	24	888	37	25
Future Volume (vph)	652	16	24	888	37	25
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	11	11	11	11	15	15
Grade (%)	2%			0%	3%	
Total Lost time (s)	5.0			5.0	5.0	
Lane Util. Factor	0.95			0.95	1.00	
Frbp, ped/bikes	1.00			1.00	0.99	
Flpb, ped/bikes	1.00			1.00	0.98	
Frt	1.00			1.00	0.94	
Flt Protected	1.00			1.00	0.97	
Satd. Flow (prot)	3373			3452	1800	
Flt Permitted	1.00			0.93	0.97	
Satd. Flow (perm)	3373			3206	1800	
Peak-hour factor, PHF	0.91	0.67	1.00	0.90	0.84	0.78
Adj. Flow (vph)	716	24	24	987	44	32
RTOR Reduction (vph)	3	0	0	0	29	0
Lane Group Flow (vph)	737	0	0	1011	47	0
Confl. Peds. (#/hr)			11		11	
Confl. Bikes (#/hr)						2
Heavy Vehicles (%)	2%	0%	0%	1%	3%	0%
Turn Type	NA		Perm	NA	Perm	
Protected Phases	1			1		
Permitted Phases			1		2	
Actuated Green, G (s)	35.2			35.2	6.1	
Effective Green, g (s)	35.2			35.2	6.1	
Actuated g/C Ratio	0.49			0.49	0.09	
Clearance Time (s)	5.0			5.0	5.0	
Vehicle Extension (s)	3.0			3.0	3.0	
Lane Grp Cap (vph)	1662			1580	153	
v/s Ratio Prot	0.22					
v/s Ratio Perm				c0.32	c0.03	
v/c Ratio	0.44			0.64	0.31	
Uniform Delay, d1	11.7			13.4	30.7	
Progression Factor	1.00			1.00	1.00	
Incremental Delay, d2	0.9			2.0	1.1	
Delay (s)	12.6			15.4	31.8	
Level of Service	B			B	C	
Approach Delay (s)	12.6			15.4	31.8	
Approach LOS	B			B	C	

Intersection Summary

HCM 2000 Control Delay	15.0	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.41		
Actuated Cycle Length (s)	71.4	Sum of lost time (s)	12.0
Intersection Capacity Utilization	55.1%	ICU Level of Service	B
Analysis Period (min)	15		

c Critical Lane Group

Lanes, Volumes, Timings
3: Maywood Street & Park Avenue (Route 12)

Existing Conditions PM



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕			↕↕			↕			↕	
Traffic Volume (vph)	9	537	28	56	770	40	61	58	63	44	44	10
Future Volume (vph)	9	537	28	56	770	40	61	58	63	44	44	10
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	11	11	11	11	11	11	16	16	16	16	16	16
Grade (%)		2%			2%			2%			2%	
Lane Util. Factor	0.95	0.95	0.95	0.95	0.95	0.95	1.00	1.00	1.00	1.00	1.00	1.00
Ped Bike Factor		1.00			1.00			1.00			0.99	
Frt		0.991			0.991			0.956			0.983	
Flt Protected		0.999			0.997			0.981			0.976	
Satd. Flow (prot)	0	3359	0	0	3058	0	0	1953	0	0	1938	0
Flt Permitted		0.923			0.831			0.832			0.659	
Satd. Flow (perm)	0	3103	0	0	2548	0	0	1650	0	0	1304	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		10			10			27			8	
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		189			269			185			216	
Travel Time (s)		4.3			6.1			4.2			4.9	
Confl. Peds. (#/hr)	3			14			7			8		
Confl. Bikes (#/hr)												2
Peak Hour Factor	0.56	0.85	0.70	0.88	0.91	0.71	0.73	0.91	0.89	0.73	0.92	0.63
Heavy Vehicles (%)	0%	2%	0%	0%	13%	4%	3%	2%	2%	11%	0%	0%
Adj. Flow (vph)	16	632	40	64	846	56	84	64	71	60	48	16
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	688	0	0	966	0	0	219	0	0	124	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		0			0			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.06	1.06	1.06	1.06	1.06	1.06	0.86	0.86	0.86	0.86	0.86	0.86
Turning Speed (mph)	15		9	15		9	15		9	15		9
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		2			2			4			4	
Permitted Phases	2			2			4			4		
Minimum Split (s)	28.0	28.0		28.0	28.0		13.0	13.0		13.0	13.0	
Total Split (s)	35.0	35.0		35.0	35.0		17.0	17.0		17.0	17.0	
Total Split (%)	46.1%	46.1%		46.1%	46.1%		22.4%	22.4%		22.4%	22.4%	
Maximum Green (s)	30.0	30.0		30.0	30.0		12.0	12.0		12.0	12.0	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		5.0			5.0			5.0			5.0	
Lead/Lag							Lag	Lag		Lag	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Walk Time (s)												
Flash Dont Walk (s)												
Pedestrian Calls (#/hr)												

Lane Group	Ø3
Lane Configurations	
Traffic Volume (vph)	
Future Volume (vph)	
Ideal Flow (vphpl)	
Lane Width (ft)	
Grade (%)	
Lane Util. Factor	
Ped Bike Factor	
Frt	
Flt Protected	
Satd. Flow (prot)	
Flt Permitted	
Satd. Flow (perm)	
Right Turn on Red	
Satd. Flow (RTOR)	
Link Speed (mph)	
Link Distance (ft)	
Travel Time (s)	
Confl. Peds. (#/hr)	
Confl. Bikes (#/hr)	
Peak Hour Factor	
Heavy Vehicles (%)	
Adj. Flow (vph)	
Shared Lane Traffic (%)	
Lane Group Flow (vph)	
Enter Blocked Intersection	
Lane Alignment	
Median Width(ft)	
Link Offset(ft)	
Crosswalk Width(ft)	
Two way Left Turn Lane	
Headway Factor	
Turning Speed (mph)	
Turn Type	
Protected Phases	3
Permitted Phases	
Minimum Split (s)	24.0
Total Split (s)	24.0
Total Split (%)	32%
Maximum Green (s)	21.0
Yellow Time (s)	2.0
All-Red Time (s)	1.0
Lost Time Adjust (s)	
Total Lost Time (s)	
Lead/Lag	Lead
Lead-Lag Optimize?	Yes
Walk Time (s)	7.0
Flash Dont Walk (s)	16.0
Pedestrian Calls (#/hr)	32

Lanes, Volumes, Timings
 3: Maywood Street & Park Avenue (Route 12)

Existing Conditions PM

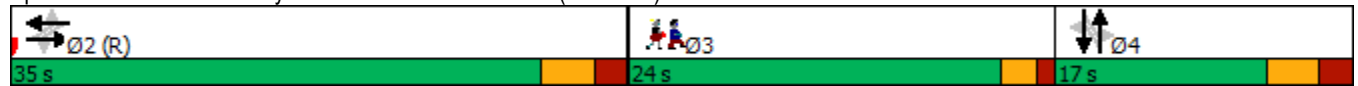


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Act Effct Green (s)		30.0			30.0			12.0			12.0	
Actuated g/C Ratio		0.39			0.39			0.16			0.16	
v/c Ratio		0.56			0.96			0.77			0.58	
Control Delay		19.7			43.4			47.3			40.3	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		19.7			43.4			47.3			40.3	
LOS		B			D			D			D	
Approach Delay		19.7			43.4			47.3			40.3	
Approach LOS		B			D			D			D	

Intersection Summary

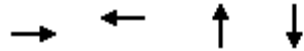
Area Type:	Other
Cycle Length:	76
Actuated Cycle Length:	76
Offset:	0 (0%), Referenced to phase 2:EBWB and 6:, Start of Green
Natural Cycle:	75
Control Type:	Pretimed
Maximum v/c Ratio:	0.96
Intersection Signal Delay:	35.5
Intersection LOS:	D
Intersection Capacity Utilization	64.6%
ICU Level of Service	C
Analysis Period (min)	15

Splits and Phases: 3: Maywood Street & Park Avenue (Route 12)



Lane Group	Ø3
Act Effct Green (s)	
Actuated g/C Ratio	
v/c Ratio	
Control Delay	
Queue Delay	
Total Delay	
LOS	
Approach Delay	
Approach LOS	
Intersection Summary	

3: Maywood Street & Park Avenue (Route 12)



Lane Group	EBT	WBT	NBT	SBT
Lane Group Flow (vph)	688	966	219	124
v/c Ratio	0.56	0.96	0.77	0.58
Control Delay	19.7	43.4	47.3	40.3
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	19.7	43.4	47.3	40.3
Queue Length 50th (ft)	127	224	88	51
Queue Length 95th (ft)	165	#358	#196	#116
Internal Link Dist (ft)	109	189	105	136
Turn Bay Length (ft)				
Base Capacity (vph)	1230	1011	283	212
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.56	0.96	0.77	0.58

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.

HCM Signalized Intersection Capacity Analysis

3: Maywood Street & Park Avenue (Route 12)

Existing Conditions PM



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations		↔↔			↔↔			↔			↔		
Traffic Volume (vph)	9	537	28	56	770	40	61	58	63	44	44	10	
Future Volume (vph)	9	537	28	56	770	40	61	58	63	44	44	10	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Lane Width	11	11	11	11	11	11	16	16	16	16	16	16	
Grade (%)		2%			2%			2%			2%		
Total Lost time (s)		5.0			5.0			5.0			5.0		
Lane Util. Factor		0.95			0.95			1.00			1.00		
Frpb, ped/bikes		1.00			1.00			1.00			1.00		
Flpb, ped/bikes		1.00			1.00			1.00			1.00		
Fr _t		0.99			0.99			0.96			0.98		
Fl _t Protected		1.00			1.00			0.98			0.98		
Satd. Flow (prot)		3359			3057			1946			1931		
Fl _t Permitted		0.92			0.83			0.83			0.66		
Satd. Flow (perm)		3103			2549			1650			1304		
Peak-hour factor, PHF	0.56	0.85	0.70	0.88	0.91	0.71	0.73	0.91	0.89	0.73	0.92	0.63	
Adj. Flow (vph)	16	632	40	64	846	56	84	64	71	60	48	16	
RTOR Reduction (vph)	0	6	0	0	6	0	0	23	0	0	7	0	
Lane Group Flow (vph)	0	682	0	0	960	0	0	196	0	0	117	0	
Confl. Peds. (#/hr)	3			14			7			8			
Confl. Bikes (#/hr)												2	
Heavy Vehicles (%)	0%	2%	0%	0%	13%	4%	3%	2%	2%	11%	0%	0%	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA		
Protected Phases		2			2			4			4		
Permitted Phases	2			2			4			4			
Actuated Green, G (s)		30.0			30.0			12.0			12.0		
Effective Green, g (s)		30.0			30.0			12.0			12.0		
Actuated g/C Ratio		0.39			0.39			0.16			0.16		
Clearance Time (s)		5.0			5.0			5.0			5.0		
Lane Grp Cap (vph)		1224			1006			260			205		
v/s Ratio Prot													
v/s Ratio Perm		0.22			c0.38			c0.12			0.09		
v/c Ratio		0.56			0.95			0.75			0.57		
Uniform Delay, d1		17.8			22.3			30.6			29.6		
Progression Factor		1.00			1.00			1.00			1.00		
Incremental Delay, d2		1.8			19.3			18.3			11.1		
Delay (s)		19.7			41.6			48.9			40.7		
Level of Service		B			D			D			D		
Approach Delay (s)		19.7			41.6			48.9			40.7		
Approach LOS		B			D			D			D		
Intersection Summary													
HCM 2000 Control Delay			34.8									HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio			0.60										
Actuated Cycle Length (s)			76.0									Sum of lost time (s)	13.0
Intersection Capacity Utilization			64.6%									ICU Level of Service	C
Analysis Period (min)			15										

c Critical Lane Group

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔			↔				
Traffic Vol, veh/h	9	647	0	0	783	7	0	0	2	0	0	0
Future Vol, veh/h	9	647	0	0	783	7	0	0	2	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	10	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	2	-	-	2	-	-	0	-	-	0	-
Peak Hour Factor	56	90	25	25	92	58	25	25	50	25	25	25
Heavy Vehicles, %	0	2	0	0	1	0	0	0	0	0	0	0
Mvmt Flow	16	719	0	0	851	12	0	0	4	0	0	0

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	863	0	0	719	0	0	1187	1614	360
Stage 1	-	-	-	-	-	-	751	751	-
Stage 2	-	-	-	-	-	-	436	863	-
Critical Hdwy	4.1	-	-	4.1	-	-	6.8	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	5.8	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	5.8	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3
Pot Cap-1 Maneuver	788	-	-	892	-	-	184	105	642
Stage 1	-	-	-	-	-	-	432	421	-
Stage 2	-	-	-	-	-	-	625	374	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	788	-	-	892	-	-	176	0	642
Mov Cap-2 Maneuver	-	-	-	-	-	-	176	0	-
Stage 1	-	-	-	-	-	-	417	0	-
Stage 2	-	-	-	-	-	-	619	0	-

Approach	EB	WB	NB
HCM Control Delay, s	0.4	0	10.6
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR
Capacity (veh/h)	642	788	-	-	892	-	-
HCM Lane V/C Ratio	0.006	0.02	-	-	-	-	-
HCM Control Delay (s)	10.6	9.7	0.2	-	0	-	-
HCM Lane LOS	B	A	A	-	A	-	-
HCM 95th %tile Q(veh)	0	0.1	-	-	0	-	-

Intersection						
Int Delay, s/veh	0.9					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↑	
Traffic Vol, veh/h	643	6	24	907	4	24
Future Vol, veh/h	643	6	24	907	4	24
Conflicting Peds, #/hr	0	0	0	0	13	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	2	-	-	2	3	-
Peak Hour Factor	92	50	67	91	33	86
Heavy Vehicles, %	2	17	0	1	25	0
Mvmt Flow	699	12	36	997	12	28

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	711	0	1289
Stage 1	-	-	-	-	705
Stage 2	-	-	-	-	584
Critical Hdwy	-	-	4.1	-	7.9
Critical Hdwy Stg 1	-	-	-	-	6.9
Critical Hdwy Stg 2	-	-	-	-	6.9
Follow-up Hdwy	-	-	2.2	-	3.75
Pot Cap-1 Maneuver	-	-	898	-	103
Stage 1	-	-	-	-	351
Stage 2	-	-	-	-	418
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	898	-	93
Mov Cap-2 Maneuver	-	-	-	-	93
Stage 1	-	-	-	-	351
Stage 2	-	-	-	-	376

Approach	EB	WB	NB
HCM Control Delay, s	0	0.7	24
HCM LOS			C

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	229	-	-	898	-
HCM Lane V/C Ratio	0.175	-	-	0.04	-
HCM Control Delay (s)	24	-	-	9.2	0.4
HCM Lane LOS	C	-	-	A	A
HCM 95th %tile Q(veh)	0.6	-	-	0.1	-

Intersection												
Int Delay, s/veh	2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	1	0	4	2	0	0	0	25	2	5	26	2
Future Vol, veh/h	1	0	4	2	0	0	0	25	2	5	26	2
Conflicting Peds, #/hr	1	0	0	6	0	0	1	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	3	-	-	3	-
Peak Hour Factor	25	25	38	25	25	100	25	69	25	63	50	50
Heavy Vehicles, %	0	0	0	0	0	0	0	4	0	0	4	0
Mvmt Flow	4	0	11	8	0	0	0	36	8	8	52	4

Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	112	115	61	122	113	41	57	0	0	44	0	0
Stage 1	71	71	-	40	40	-	-	-	-	-	-	-
Stage 2	41	44	-	82	73	-	-	-	-	-	-	-
Critical Hdwy	7.1	6.5	6.2	7.1	6.5	6.2	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.1	5.5	-	6.1	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.1	5.5	-	6.1	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	870	779	1010	858	781	1036	1560	-	-	1577	-	-
Stage 1	944	840	-	980	866	-	-	-	-	-	-	-
Stage 2	979	862	-	931	838	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	865	774	1002	840	776	1035	1559	-	-	1577	-	-
Mov Cap-2 Maneuver	865	774	-	840	776	-	-	-	-	-	-	-
Stage 1	943	835	-	980	866	-	-	-	-	-	-	-
Stage 2	978	862	-	910	833	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	8.8	9.3	0	0.9
HCM LOS	A	A		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1559	-	-	960	840	1577	-
HCM Lane V/C Ratio	-	-	-	0.015	0.01	0.005	-
HCM Control Delay (s)	0	-	-	8.8	9.3	7.3	0
HCM Lane LOS	A	-	-	A	A	A	A
HCM 95th %tile Q(veh)	0	-	-	0	0	0	-

Lanes, Volumes, Timings
1: Downing Street & Park Avenue (Route 12)

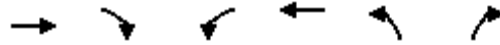
No-Build Conditions AM



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR	Ø3
Lane Configurations	↑↑			↑↑	↑↑		
Traffic Volume (vph)	931	10	34	552	13	13	
Future Volume (vph)	931	10	34	552	13	13	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	
Lane Width (ft)	11	11	11	11	15	15	
Grade (%)	2%			0%	3%		
Lane Util. Factor	0.95	0.95	0.95	0.95	1.00	1.00	
Ped Bike Factor				1.00	1.00		
Frt	0.998				0.932		
Flt Protected				0.997	0.976		
Satd. Flow (prot)	3317	0	0	3384	1726	0	
Flt Permitted				0.857	0.976		
Satd. Flow (perm)	3317	0	0	2908	1723	0	
Right Turn on Red		Yes				Yes	
Satd. Flow (RTOR)	2				14		
Link Speed (mph)	30			30	30		
Link Distance (ft)	351			179	249		
Travel Time (s)	8.0			4.1	5.7		
Confl. Peds. (#/hr)			7		2		
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	
Heavy Vehicles (%)	4%	0%	0%	3%	17%	0%	
Adj. Flow (vph)	1012	11	37	600	14	14	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	1023	0	0	637	28	0	
Enter Blocked Intersection	No	No	No	No	No	No	
Lane Alignment	Left	Right	Left	Left	Left	Right	
Median Width(ft)	0			0	15		
Link Offset(ft)	0			0	0		
Crosswalk Width(ft)	16			16	16		
Two way Left Turn Lane							
Headway Factor	1.06	1.06	1.04	1.04	0.90	0.90	
Turning Speed (mph)		9	15		15	9	
Number of Detectors	2		1	2	1		
Detector Template	Thru		Left	Thru	Left		
Leading Detector (ft)	100		20	100	20		
Trailing Detector (ft)	0		0	0	0		
Detector 1 Position(ft)	0		0	0	0		
Detector 1 Size(ft)	6		20	6	20		
Detector 1 Type	Cl+Ex		Cl+Ex	Cl+Ex	Cl+Ex		
Detector 1 Channel							
Detector 1 Extend (s)	0.0		0.0	0.0	0.0		
Detector 1 Queue (s)	0.0		0.0	0.0	0.0		
Detector 1 Delay (s)	0.0		0.0	0.0	0.0		
Detector 2 Position(ft)	94			94			
Detector 2 Size(ft)	6			6			
Detector 2 Type	Cl+Ex			Cl+Ex			
Detector 2 Channel							
Detector 2 Extend (s)	0.0			0.0			
Turn Type	NA		Perm	NA	Perm		

Lanes, Volumes, Timings
1: Downing Street & Park Avenue (Route 12)

No-Build Conditions AM

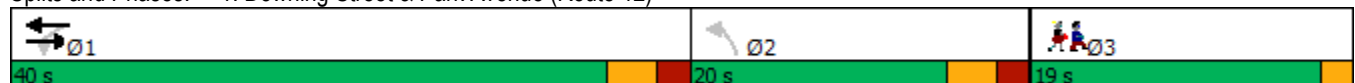


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR	Ø3
Protected Phases	1			1			3
Permitted Phases			1		2		
Detector Phase	1		1	1	2		
Switch Phase							
Minimum Initial (s)	8.0		8.0	8.0	6.0		5.0
Minimum Split (s)	23.0		23.0	23.0	11.0		19.0
Total Split (s)	40.0		40.0	40.0	20.0		19.0
Total Split (%)	50.6%		50.6%	50.6%	25.3%		24%
Maximum Green (s)	35.0		35.0	35.0	15.0		17.0
Yellow Time (s)	3.0		3.0	3.0	3.0		2.0
All-Red Time (s)	2.0		2.0	2.0	2.0		0.0
Lost Time Adjust (s)	0.0			0.0	0.0		
Total Lost Time (s)	5.0			5.0	5.0		
Lead/Lag	Lead		Lead	Lead	Lag		
Lead-Lag Optimize?	Yes		Yes	Yes	Yes		
Vehicle Extension (s)	3.0		3.0	3.0	3.0		3.0
Recall Mode	Max		Max	Max	None		Ped
Walk Time (s)							7.0
Flash Dont Walk (s)							11.0
Pedestrian Calls (#/hr)							9
Act Effct Green (s)	35.3			35.3	6.5		
Actuated g/C Ratio	0.54			0.54	0.10		
v/c Ratio	0.57			0.40	0.15		
Control Delay	12.4			10.7	22.2		
Queue Delay	0.0			0.0	0.0		
Total Delay	12.4			10.7	22.2		
LOS	B			B	C		
Approach Delay	12.4			10.7	22.2		
Approach LOS	B			B	C		

Intersection Summary

Area Type:	Other
Cycle Length:	79
Actuated Cycle Length:	64.9
Natural Cycle:	55
Control Type:	Semi Act-Uncoord
Maximum v/c Ratio:	0.57
Intersection Signal Delay:	11.9
Intersection LOS:	B
Intersection Capacity Utilization	54.1%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 1: Downing Street & Park Avenue (Route 12)



1: Downing Street & Park Avenue (Route 12)



Lane Group	EBT	WBT	NBL
Lane Group Flow (vph)	1023	637	28
v/c Ratio	0.57	0.40	0.15
Control Delay	12.4	10.7	22.2
Queue Delay	0.0	0.0	0.0
Total Delay	12.4	10.7	22.2
Queue Length 50th (ft)	102	56	5
Queue Length 95th (ft)	235	136	28
Internal Link Dist (ft)	271	99	169
Turn Bay Length (ft)			
Base Capacity (vph)	1803	1580	412
Starvation Cap Reductn	0	0	0
Spillback Cap Reductn	0	0	0
Storage Cap Reductn	0	0	0
Reduced v/c Ratio	0.57	0.40	0.07
Intersection Summary			

HCM Signalized Intersection Capacity Analysis

1: Downing Street & Park Avenue (Route 12)

No-Build Conditions AM



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↑↑	
Traffic Volume (vph)	931	10	34	552	13	13
Future Volume (vph)	931	10	34	552	13	13
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	11	11	11	11	15	15
Grade (%)	2%			0%	3%	
Total Lost time (s)	5.0			5.0	5.0	
Lane Util. Factor	0.95			0.95	1.00	
Frbp, ped/bikes	1.00			1.00	1.00	
Flpb, ped/bikes	1.00			1.00	0.99	
Frt	1.00			1.00	0.93	
Flt Protected	1.00			1.00	0.98	
Satd. Flow (prot)	3318			3384	1713	
Flt Permitted	1.00			0.86	0.98	
Satd. Flow (perm)	3318			2907	1713	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	1012	11	37	600	14	14
RTOR Reduction (vph)	1	0	0	0	13	0
Lane Group Flow (vph)	1022	0	0	637	15	0
Confl. Peds. (#/hr)			7		2	
Heavy Vehicles (%)	4%	0%	0%	3%	17%	0%
Turn Type	NA		Perm	NA	Perm	
Protected Phases	1			1		
Permitted Phases			1		2	
Actuated Green, G (s)	35.3			35.3	2.6	
Effective Green, g (s)	35.3			35.3	2.6	
Actuated g/C Ratio	0.52			0.52	0.04	
Clearance Time (s)	5.0			5.0	5.0	
Vehicle Extension (s)	3.0			3.0	3.0	
Lane Grp Cap (vph)	1719			1506	65	
v/s Ratio Prot	c0.31					
v/s Ratio Perm				0.22	c0.01	
v/c Ratio	0.59			0.42	0.22	
Uniform Delay, d1	11.4			10.1	31.8	
Progression Factor	1.00			1.00	1.00	
Incremental Delay, d2	1.5			0.9	1.7	
Delay (s)	12.9			11.0	33.5	
Level of Service	B			B	C	
Approach Delay (s)	12.9			11.0	33.5	
Approach LOS	B			B	C	

Intersection Summary

HCM 2000 Control Delay	12.5	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.38		
Actuated Cycle Length (s)	68.1	Sum of lost time (s)	12.0
Intersection Capacity Utilization	54.1%	ICU Level of Service	A
Analysis Period (min)	15		

c Critical Lane Group

Lanes, Volumes, Timings
3: Maywood Street & Park Avenue (Route 12)

No-Build Conditions AM



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕			↕↕			↕				↕
Traffic Volume (vph)	17	899	33	26	412	43	47	73	50	74	62	18
Future Volume (vph)	17	899	33	26	412	43	47	73	50	74	62	18
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	11	11	11	11	11	11	16	16	16	16	16	16
Grade (%)		2%			2%			2%			2%	
Lane Util. Factor	0.95	0.95	0.95	0.95	0.95	0.95	1.00	1.00	1.00	1.00	1.00	1.00
Ped Bike Factor		1.00			1.00			1.00			1.00	
Frt		0.995			0.987			0.960			0.984	
Flt Protected		0.999			0.997			0.986			0.977	
Satd. Flow (prot)	0	3363	0	0	3258	0	0	1990	0	0	1969	0
Flt Permitted		0.942			0.823			0.837			0.673	
Satd. Flow (perm)	0	3171	0	0	2689	0	0	1686	0	0	1354	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		6			16			23			8	
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		189			269			185			216	
Travel Time (s)		4.3			6.1			4.2			4.9	
Confl. Peds. (#/hr)	1			8			6			3		
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Heavy Vehicles (%)	13%	2%	0%	4%	4%	8%	2%	2%	0%	6%	3%	0%
Adj. Flow (vph)	18	977	36	28	448	47	51	79	54	80	67	20
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	1031	0	0	523	0	0	184	0	0	167	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		0			0			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.06	1.06	1.06	1.06	1.06	1.06	0.86	0.86	0.86	0.86	0.86	0.86
Turning Speed (mph)	15		9	15		9	15		9	15		9
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		2			2			4			4	
Permitted Phases	2			2			4			4		
Minimum Split (s)	28.0	28.0		28.0	28.0		13.0	13.0		13.0	13.0	
Total Split (s)	35.0	35.0		35.0	35.0		17.0	17.0		17.0	17.0	
Total Split (%)	46.1%	46.1%		46.1%	46.1%		22.4%	22.4%		22.4%	22.4%	
Maximum Green (s)	30.0	30.0		30.0	30.0		12.0	12.0		12.0	12.0	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		5.0			5.0			5.0			5.0	
Lead/Lag							Lag	Lag		Lag	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Walk Time (s)												
Flash Dont Walk (s)												
Pedestrian Calls (#/hr)												
Act Effct Green (s)		30.0			30.0			12.0			12.0	

Lane Group	Ø3
Lane Configurations	
Traffic Volume (vph)	
Future Volume (vph)	
Ideal Flow (vphpl)	
Lane Width (ft)	
Grade (%)	
Lane Util. Factor	
Ped Bike Factor	
Frt	
Flt Protected	
Satd. Flow (prot)	
Flt Permitted	
Satd. Flow (perm)	
Right Turn on Red	
Satd. Flow (RTOR)	
Link Speed (mph)	
Link Distance (ft)	
Travel Time (s)	
Confl. Peds. (#/hr)	
Peak Hour Factor	
Heavy Vehicles (%)	
Adj. Flow (vph)	
Shared Lane Traffic (%)	
Lane Group Flow (vph)	
Enter Blocked Intersection	
Lane Alignment	
Median Width(ft)	
Link Offset(ft)	
Crosswalk Width(ft)	
Two way Left Turn Lane	
Headway Factor	
Turning Speed (mph)	
Turn Type	
Protected Phases	3
Permitted Phases	
Minimum Split (s)	24.0
Total Split (s)	24.0
Total Split (%)	32%
Maximum Green (s)	21.0
Yellow Time (s)	2.0
All-Red Time (s)	1.0
Lost Time Adjust (s)	
Total Lost Time (s)	
Lead/Lag	Lead
Lead-Lag Optimize?	Yes
Walk Time (s)	7.0
Flash Dont Walk (s)	16.0
Pedestrian Calls (#/hr)	20
Act Effct Green (s)	

Lanes, Volumes, Timings
 3: Maywood Street & Park Avenue (Route 12)

No-Build Conditions AM

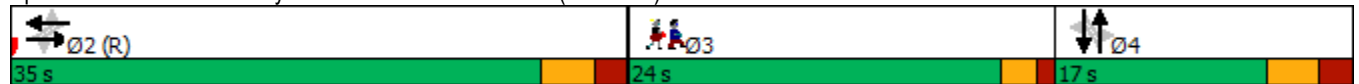


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Actuated g/C Ratio		0.39			0.39			0.16				0.16
v/c Ratio		0.82			0.49			0.65				0.76
Control Delay		27.3			18.6			38.1				53.1
Queue Delay		0.0			0.0			0.0				0.0
Total Delay		27.3			18.6			38.1				53.1
LOS		C			B			D				D
Approach Delay		27.3			18.6			38.1				53.1
Approach LOS		C			B			D				D

Intersection Summary

Area Type:	Other
Cycle Length:	76
Actuated Cycle Length:	76
Offset:	0 (0%), Referenced to phase 2:EBWB and 6:, Start of Green
Natural Cycle:	70
Control Type:	Pretimed
Maximum v/c Ratio:	0.82
Intersection Signal Delay:	28.2
Intersection LOS:	C
Intersection Capacity Utilization	60.9%
ICU Level of Service	B
Analysis Period (min)	15

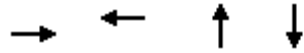
Splits and Phases: 3: Maywood Street & Park Avenue (Route 12)



Lane Group	Ø3
Actuated g/C Ratio	
v/c Ratio	
Control Delay	
Queue Delay	
Total Delay	
LOS	
Approach Delay	
Approach LOS	
Intersection Summary	

Queues
3: Maywood Street & Park Avenue (Route 12)

No-Build Conditions AM



Lane Group	EBT	WBT	NBT	SBT
Lane Group Flow (vph)	1031	523	184	167
v/c Ratio	0.82	0.49	0.65	0.76
Control Delay	27.3	18.6	38.1	53.1
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	27.3	18.6	38.1	53.1
Queue Length 50th (ft)	222	91	72	73
Queue Length 95th (ft)	#303	135	#152	#170
Internal Link Dist (ft)	109	189	105	136
Turn Bay Length (ft)				
Base Capacity (vph)	1255	1071	285	220
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.82	0.49	0.65	0.76

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.

HCM Signalized Intersection Capacity Analysis

3: Maywood Street & Park Avenue (Route 12)

No-Build Conditions AM



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations		↕↕			↕↕			↕			↕		
Traffic Volume (vph)	17	899	33	26	412	43	47	73	50	74	62	18	
Future Volume (vph)	17	899	33	26	412	43	47	73	50	74	62	18	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Lane Width	11	11	11	11	11	11	16	16	16	16	16	16	
Grade (%)		2%			2%			2%			2%		
Total Lost time (s)		5.0			5.0			5.0			5.0		
Lane Util. Factor		0.95			0.95			1.00			1.00		
Frbp, ped/bikes		1.00			1.00			1.00			1.00		
Flpb, ped/bikes		1.00			1.00			1.00			1.00		
Frt		0.99			0.99			0.96			0.98		
Flt Protected		1.00			1.00			0.99			0.98		
Satd. Flow (prot)		3362			3257			1987			1965		
Flt Permitted		0.94			0.82			0.84			0.67		
Satd. Flow (perm)		3168			2688			1687			1354		
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	
Adj. Flow (vph)	18	977	36	28	448	47	51	79	54	80	67	20	
RTOR Reduction (vph)	0	4	0	0	10	0	0	19	0	0	7	0	
Lane Group Flow (vph)	0	1027	0	0	513	0	0	165	0	0	160	0	
Confl. Peds. (#/hr)	1			8			6			3			
Heavy Vehicles (%)	13%	2%	0%	4%	4%	8%	2%	2%	0%	6%	3%	0%	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA		
Protected Phases		2			2			4			4		
Permitted Phases	2			2			4			4			
Actuated Green, G (s)		30.0			30.0			12.0			12.0		
Effective Green, g (s)		30.0			30.0			12.0			12.0		
Actuated g/C Ratio		0.39			0.39			0.16			0.16		
Clearance Time (s)		5.0			5.0			5.0			5.0		
Lane Grp Cap (vph)		1250			1061			266			213		
v/s Ratio Prot													
v/s Ratio Perm		c0.32			0.19			0.10			c0.12		
v/c Ratio		0.82			0.48			0.62			0.75		
Uniform Delay, d1		20.6			17.2			29.9			30.6		
Progression Factor		1.00			1.00			1.00			1.00		
Incremental Delay, d2		6.2			1.6			10.4			21.5		
Delay (s)		26.8			18.8			40.2			52.1		
Level of Service		C			B			D			D		
Approach Delay (s)		26.8			18.8			40.2			52.1		
Approach LOS		C			B			D			D		
Intersection Summary													
HCM 2000 Control Delay			28.1									HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio			0.53										
Actuated Cycle Length (s)			76.0									Sum of lost time (s)	13.0
Intersection Capacity Utilization			60.9%									ICU Level of Service	B
Analysis Period (min)			15										
c	Critical Lane Group												

Intersection												
Int Delay, s/veh	0											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔			↔				
Traffic Vol, veh/h	2	962	1	0	532	1	0	0	0	0	0	0
Future Vol, veh/h	2	962	1	0	532	1	0	0	0	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	10	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	2	-	-	2	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	4	0	0	3	0	0	0	0	0	0	0
Mvmt Flow	2	1046	1	0	578	1	0	0	0	0	0	0

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	579	0	0	1047	0	0	1350	1630	524
Stage 1	-	-	-	-	-	-	1051	1051	-
Stage 2	-	-	-	-	-	-	299	579	-
Critical Hdwy	4.1	-	-	4.1	-	-	6.8	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	5.8	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	5.8	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3
Pot Cap-1 Maneuver	1005	-	-	672	-	-	144	103	503
Stage 1	-	-	-	-	-	-	302	306	-
Stage 2	-	-	-	-	-	-	732	504	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1005	-	-	672	-	-	142	0	503
Mov Cap-2 Maneuver	-	-	-	-	-	-	142	0	-
Stage 1	-	-	-	-	-	-	300	0	-
Stage 2	-	-	-	-	-	-	725	0	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0	0
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR
Capacity (veh/h)	-	1005	-	-	672	-	-
HCM Lane V/C Ratio	-	0.002	-	-	-	-	-
HCM Control Delay (s)	0	8.6	0	-	0	-	-
HCM Lane LOS	A	A	A	-	A	-	-
HCM 95th %tile Q(veh)	-	0	-	-	0	-	-

Intersection						
Int Delay, s/veh	0.3					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↑	
Traffic Vol, veh/h	1048	0	9	493	3	6
Future Vol, veh/h	1048	0	9	493	3	6
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	2	-	-	2	3	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	3	0	0	4	33	0
Mvmt Flow	1139	0	10	536	3	7

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	1139	0	1427
Stage 1	-	-	-	-	1139
Stage 2	-	-	-	-	288
Critical Hdwy	-	-	4.1	-	8.06
Critical Hdwy Stg 1	-	-	-	-	7.06
Critical Hdwy Stg 2	-	-	-	-	7.06
Follow-up Hdwy	-	-	2.2	-	3.83
Pot Cap-1 Maneuver	-	-	621	-	75
Stage 1	-	-	-	-	174
Stage 2	-	-	-	-	620
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	621	-	73
Mov Cap-2 Maneuver	-	-	-	-	73
Stage 1	-	-	-	-	174
Stage 2	-	-	-	-	606

Approach	EB	WB	NB
HCM Control Delay, s	0	0.3	28.2
HCM LOS			D

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	165	-	-	621	-
HCM Lane V/C Ratio	0.059	-	-	0.016	-
HCM Control Delay (s)	28.2	-	-	10.9	0.1
HCM Lane LOS	D	-	-	B	A
HCM 95th %tile Q(veh)	0.2	-	-	0	-

Intersection												
Int Delay, s/veh	0.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	0	0	0	0	3	0	10	1	1	20	0
Future Vol, veh/h	0	0	0	0	0	3	0	10	1	1	20	0
Conflicting Peds, #/hr	2	0	0	2	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	3	-	-	3	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	2	2	2	0	0	0	0	0	0
Mvmt Flow	0	0	0	0	0	3	0	11	1	1	22	0

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	39	36	24	38	36	14	22	0	0	12	0	0
Stage 1	24	24	-	12	12	-	-	-	-	-	-	-
Stage 2	15	12	-	26	24	-	-	-	-	-	-	-
Critical Hdwy	7.1	6.5	6.2	7.12	6.52	6.22	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.1	5.5	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.1	5.5	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.518	4.018	3.318	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	971	860	1058	967	856	1066	1607	-	-	1620	-	-
Stage 1	999	879	-	1009	886	-	-	-	-	-	-	-
Stage 2	1010	890	-	992	875	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	965	859	1055	964	855	1063	1607	-	-	1620	-	-
Mov Cap-2 Maneuver	965	859	-	964	855	-	-	-	-	-	-	-
Stage 1	999	878	-	1009	886	-	-	-	-	-	-	-
Stage 2	1005	890	-	989	874	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0	8.4	0	0.3
HCM LOS	A	A		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1607	-	-	-	1063	1620	-
HCM Lane V/C Ratio	-	-	-	-	0.003	0.001	-
HCM Control Delay (s)	0	-	-	0	8.4	7.2	0
HCM Lane LOS	A	-	-	A	A	A	A
HCM 95th %tile Q(veh)	0	-	-	-	0	0	-

Lanes, Volumes, Timings
1: Downing Street & Park Avenue (Route 12)

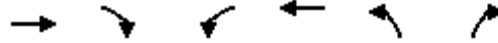
No-Build Conditions PM



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR	Ø3
Lane Configurations	↑↑			↑↑	↑↑		
Traffic Volume (vph)	724	18	27	986	41	28	
Future Volume (vph)	724	18	27	986	41	28	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	
Lane Width (ft)	11	11	11	11	15	15	
Grade (%)	2%			0%	3%		
Lane Util. Factor	0.95	0.95	0.95	0.95	1.00	1.00	
Ped Bike Factor				1.00	0.98		
Frt	0.996				0.946		
Flt Protected				0.999	0.971		
Satd. Flow (prot)	3375	0	0	3453	1847	0	
Flt Permitted				0.919	0.971		
Satd. Flow (perm)	3375	0	0	3176	1828	0	
Right Turn on Red		Yes				Yes	
Satd. Flow (RTOR)	4				30		
Link Speed (mph)	30			30	30		
Link Distance (ft)	351			179	249		
Travel Time (s)	8.0			4.1	5.7		
Confl. Peds. (#/hr)			11		11		
Confl. Bikes (#/hr)						2	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	
Heavy Vehicles (%)	2%	0%	0%	1%	3%	0%	
Adj. Flow (vph)	787	20	29	1072	45	30	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	807	0	0	1101	75	0	
Enter Blocked Intersection	No	No	No	No	No	No	
Lane Alignment	Left	Right	Left	Left	Left	Right	
Median Width(ft)	0			0	15		
Link Offset(ft)	0			0	0		
Crosswalk Width(ft)	16			16	16		
Two way Left Turn Lane							
Headway Factor	1.06	1.06	1.04	1.04	0.90	0.90	
Turning Speed (mph)		9	15		15	9	
Number of Detectors	2		1	2	1		
Detector Template	Thru		Left	Thru	Left		
Leading Detector (ft)	100		20	100	20		
Trailing Detector (ft)	0		0	0	0		
Detector 1 Position(ft)	0		0	0	0		
Detector 1 Size(ft)	6		20	6	20		
Detector 1 Type	Cl+Ex		Cl+Ex	Cl+Ex	Cl+Ex		
Detector 1 Channel							
Detector 1 Extend (s)	0.0		0.0	0.0	0.0		
Detector 1 Queue (s)	0.0		0.0	0.0	0.0		
Detector 1 Delay (s)	0.0		0.0	0.0	0.0		
Detector 2 Position(ft)	94			94			
Detector 2 Size(ft)	6			6			
Detector 2 Type	Cl+Ex			Cl+Ex			
Detector 2 Channel							
Detector 2 Extend (s)	0.0			0.0			

Lanes, Volumes, Timings
 1: Downing Street & Park Avenue (Route 12)

No-Build Conditions PM

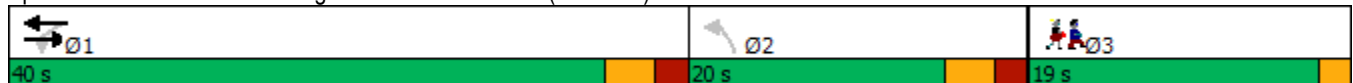


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR	Ø3
Turn Type	NA		Perm	NA	Perm		
Protected Phases	1			1			3
Permitted Phases			1		2		
Detector Phase	1		1	1	2		
Switch Phase							
Minimum Initial (s)	8.0		8.0	8.0	6.0		5.0
Minimum Split (s)	23.0		23.0	23.0	11.0		19.0
Total Split (s)	40.0		40.0	40.0	20.0		19.0
Total Split (%)	50.6%		50.6%	50.6%	25.3%		24%
Maximum Green (s)	35.0		35.0	35.0	15.0		17.0
Yellow Time (s)	3.0		3.0	3.0	3.0		2.0
All-Red Time (s)	2.0		2.0	2.0	2.0		0.0
Lost Time Adjust (s)	0.0			0.0	0.0		
Total Lost Time (s)	5.0			5.0	5.0		
Lead/Lag	Lead		Lead	Lead	Lag		
Lead-Lag Optimize?	Yes		Yes	Yes	Yes		
Vehicle Extension (s)	3.0		3.0	3.0	3.0		3.0
Recall Mode	Max		Max	Max	None		Ped
Walk Time (s)							7.0
Flash Dont Walk (s)							11.0
Pedestrian Calls (#/hr)							9
Act Effct Green (s)	35.2			35.2	7.5		
Actuated g/C Ratio	0.50			0.50	0.11		
v/c Ratio	0.48			0.69	0.34		
Control Delay	13.5			17.3	24.6		
Queue Delay	0.0			0.0	0.0		
Total Delay	13.5			17.3	24.6		
LOS	B			B	C		
Approach Delay	13.5			17.3	24.6		
Approach LOS	B			B	C		

Intersection Summary

Area Type: Other
 Cycle Length: 79
 Actuated Cycle Length: 70.4
 Natural Cycle: 60
 Control Type: Semi Act-Uncoord
 Maximum v/c Ratio: 0.69
 Intersection Signal Delay: 16.1
 Intersection LOS: B
 Intersection Capacity Utilization 60.0%
 ICU Level of Service B
 Analysis Period (min) 15

Splits and Phases: 1: Downing Street & Park Avenue (Route 12)



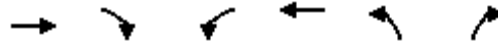
1: Downing Street & Park Avenue (Route 12)



Lane Group	EBT	WBT	NBL
Lane Group Flow (vph)	807	1101	75
v/c Ratio	0.48	0.69	0.34
Control Delay	13.5	17.3	24.6
Queue Delay	0.0	0.0	0.0
Total Delay	13.5	17.3	24.6
Queue Length 50th (ft)	120	192	19
Queue Length 95th (ft)	182	286	56
Internal Link Dist (ft)	271	99	169
Turn Bay Length (ft)			
Base Capacity (vph)	1691	1589	415
Starvation Cap Reductn	0	0	0
Spillback Cap Reductn	0	0	0
Storage Cap Reductn	0	0	0
Reduced v/c Ratio	0.48	0.69	0.18
Intersection Summary			

HCM Signalized Intersection Capacity Analysis
 1: Downing Street & Park Avenue (Route 12)

No-Build Conditions PM



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↑↑	
Traffic Volume (vph)	724	18	27	986	41	28
Future Volume (vph)	724	18	27	986	41	28
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	11	11	11	11	15	15
Grade (%)	2%			0%	3%	
Total Lost time (s)	5.0			5.0	5.0	
Lane Util. Factor	0.95			0.95	1.00	
Frbp, ped/bikes	1.00			1.00	0.99	
Flpb, ped/bikes	1.00			1.00	0.98	
Frt	1.00			1.00	0.95	
Flt Protected	1.00			1.00	0.97	
Satd. Flow (prot)	3376			3451	1802	
Flt Permitted	1.00			0.92	0.97	
Satd. Flow (perm)	3376			3174	1802	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	787	20	29	1072	45	30
RTOR Reduction (vph)	2	0	0	0	27	0
Lane Group Flow (vph)	805	0	0	1101	48	0
Confl. Peds. (#/hr)			11		11	
Confl. Bikes (#/hr)						2
Heavy Vehicles (%)	2%	0%	0%	1%	3%	0%
Turn Type	NA		Perm	NA	Perm	
Protected Phases	1			1		
Permitted Phases			1		2	
Actuated Green, G (s)	35.2			35.2	6.1	
Effective Green, g (s)	35.2			35.2	6.1	
Actuated g/C Ratio	0.49			0.49	0.09	
Clearance Time (s)	5.0			5.0	5.0	
Vehicle Extension (s)	3.0			3.0	3.0	
Lane Grp Cap (vph)	1664			1564	153	
v/s Ratio Prot	0.24					
v/s Ratio Perm				c0.35	c0.03	
v/c Ratio	0.48			0.70	0.31	
Uniform Delay, d1	12.1			14.1	30.7	
Progression Factor	1.00			1.00	1.00	
Incremental Delay, d2	1.0			2.7	1.2	
Delay (s)	13.1			16.7	31.8	
Level of Service	B			B	C	
Approach Delay (s)	13.1			16.7	31.8	
Approach LOS	B			B	C	

Intersection Summary

HCM 2000 Control Delay	15.8	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.45		
Actuated Cycle Length (s)	71.4	Sum of lost time (s)	12.0
Intersection Capacity Utilization	60.0%	ICU Level of Service	B
Analysis Period (min)	15		

c Critical Lane Group

Lanes, Volumes, Timings
3: Maywood Street & Park Avenue (Route 12)

No-Build Conditions PM



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔↔			↔↔			↔			↔	
Traffic Volume (vph)	10	596	31	62	855	44	68	64	70	49	49	11
Future Volume (vph)	10	596	31	62	855	44	68	64	70	49	49	11
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	11	11	11	11	11	11	16	16	16	16	16	16
Grade (%)		2%			2%			2%			2%	
Lane Util. Factor	0.95	0.95	0.95	0.95	0.95	0.95	1.00	1.00	1.00	1.00	1.00	1.00
Ped Bike Factor		1.00			1.00			1.00			1.00	
Frt		0.993			0.993			0.953			0.986	
Flt Protected		0.999			0.997			0.983			0.978	
Satd. Flow (prot)	0	3364	0	0	3061	0	0	1951	0	0	1956	0
Flt Permitted		0.934			0.824			0.854			0.654	
Satd. Flow (perm)	0	3145	0	0	2529	0	0	1690	0	0	1304	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		8			7			30			6	
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		189			269			185			216	
Travel Time (s)		4.3			6.1			4.2			4.9	
Confl. Peds. (#/hr)	3			14			7			8		
Confl. Bikes (#/hr)												2
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Heavy Vehicles (%)	0%	2%	0%	0%	13%	4%	3%	2%	2%	11%	0%	0%
Adj. Flow (vph)	11	648	34	67	929	48	74	70	76	53	53	12
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	693	0	0	1044	0	0	220	0	0	118	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		0			0			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.06	1.06	1.06	1.06	1.06	1.06	0.86	0.86	0.86	0.86	0.86	0.86
Turning Speed (mph)	15		9	15		9	15		9	15		9
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		2			2			4			4	
Permitted Phases	2			2			4			4		
Minimum Split (s)	28.0	28.0		28.0	28.0		13.0	13.0		13.0	13.0	
Total Split (s)	35.0	35.0		35.0	35.0		17.0	17.0		17.0	17.0	
Total Split (%)	46.1%	46.1%		46.1%	46.1%		22.4%	22.4%		22.4%	22.4%	
Maximum Green (s)	30.0	30.0		30.0	30.0		12.0	12.0		12.0	12.0	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		5.0			5.0			5.0			5.0	
Lead/Lag							Lag	Lag		Lag	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Walk Time (s)												
Flash Dont Walk (s)												
Pedestrian Calls (#/hr)												

Lane Group	Ø3
Lane Configurations	
Traffic Volume (vph)	
Future Volume (vph)	
Ideal Flow (vphpl)	
Lane Width (ft)	
Grade (%)	
Lane Util. Factor	
Ped Bike Factor	
Frt	
Flt Protected	
Satd. Flow (prot)	
Flt Permitted	
Satd. Flow (perm)	
Right Turn on Red	
Satd. Flow (RTOR)	
Link Speed (mph)	
Link Distance (ft)	
Travel Time (s)	
Confl. Peds. (#/hr)	
Confl. Bikes (#/hr)	
Peak Hour Factor	
Heavy Vehicles (%)	
Adj. Flow (vph)	
Shared Lane Traffic (%)	
Lane Group Flow (vph)	
Enter Blocked Intersection	
Lane Alignment	
Median Width(ft)	
Link Offset(ft)	
Crosswalk Width(ft)	
Two way Left Turn Lane	
Headway Factor	
Turning Speed (mph)	
Turn Type	
Protected Phases	3
Permitted Phases	
Minimum Split (s)	24.0
Total Split (s)	24.0
Total Split (%)	32%
Maximum Green (s)	21.0
Yellow Time (s)	2.0
All-Red Time (s)	1.0
Lost Time Adjust (s)	
Total Lost Time (s)	
Lead/Lag	Lead
Lead-Lag Optimize?	Yes
Walk Time (s)	7.0
Flash Dont Walk (s)	16.0
Pedestrian Calls (#/hr)	20

Lanes, Volumes, Timings
 3: Maywood Street & Park Avenue (Route 12)

No-Build Conditions PM

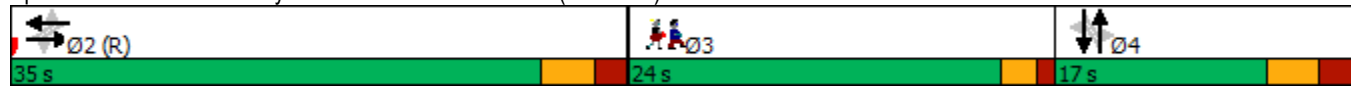


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Act Effct Green (s)		30.0			30.0			12.0			12.0	
Actuated g/C Ratio		0.39			0.39			0.16			0.16	
v/c Ratio		0.56			1.04			0.75			0.56	
Control Delay		19.7			64.8			44.6			39.6	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		19.7			64.8			44.6			39.6	
LOS		B			E			D			D	
Approach Delay		19.7			64.8			44.6			39.6	
Approach LOS		B			E			D			D	

Intersection Summary

Area Type:	Other
Cycle Length:	76
Actuated Cycle Length:	76
Offset:	0 (0%), Referenced to phase 2:EBWB and 6:, Start of Green
Natural Cycle:	80
Control Type:	Pretimed
Maximum v/c Ratio:	1.04
Intersection Signal Delay:	46.2
Intersection LOS:	D
Intersection Capacity Utilization	70.4%
ICU Level of Service	C
Analysis Period (min)	15

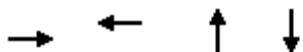
Splits and Phases: 3: Maywood Street & Park Avenue (Route 12)



Lane Group	Ø3
Act Effct Green (s)	
Actuated g/C Ratio	
v/c Ratio	
Control Delay	
Queue Delay	
Total Delay	
LOS	
Approach Delay	
Approach LOS	
Intersection Summary	

Queues
3: Maywood Street & Park Avenue (Route 12)

No-Build Conditions PM



Lane Group	EBT	WBT	NBT	SBT
Lane Group Flow (vph)	693	1044	220	118
v/c Ratio	0.56	1.04	0.75	0.56
Control Delay	19.7	64.8	44.6	39.6
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	19.7	64.8	44.6	39.6
Queue Length 50th (ft)	128	~285	87	49
Queue Length 95th (ft)	180	#406	#191	#106
Internal Link Dist (ft)	109	189	105	136
Turn Bay Length (ft)				
Base Capacity (vph)	1246	1002	292	210
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.56	1.04	0.75	0.56

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.

HCM Signalized Intersection Capacity Analysis

3: Maywood Street & Park Avenue (Route 12)

No-Build Conditions PM



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations		↕↕			↕↕			↕			↕		
Traffic Volume (vph)	10	596	31	62	855	44	68	64	70	49	49	11	
Future Volume (vph)	10	596	31	62	855	44	68	64	70	49	49	11	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Lane Width	11	11	11	11	11	11	16	16	16	16	16	16	
Grade (%)		2%			2%			2%			2%		
Total Lost time (s)		5.0			5.0			5.0			5.0		
Lane Util. Factor		0.95			0.95			1.00			1.00		
Frbp, ped/bikes		1.00			1.00			1.00			1.00		
Flpb, ped/bikes		1.00			1.00			1.00			1.00		
Frt		0.99			0.99			0.95			0.99		
Flt Protected		1.00			1.00			0.98			0.98		
Satd. Flow (prot)		3364			3059			1947			1950		
Flt Permitted		0.93			0.82			0.85			0.65		
Satd. Flow (perm)		3143			2530			1690			1303		
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	
Adj. Flow (vph)	11	648	34	67	929	48	74	70	76	53	53	12	
RTOR Reduction (vph)	0	5	0	0	4	0	0	25	0	0	5	0	
Lane Group Flow (vph)	0	688	0	0	1040	0	0	195	0	0	113	0	
Confl. Peds. (#/hr)	3			14			7			8			
Confl. Bikes (#/hr)												2	
Heavy Vehicles (%)	0%	2%	0%	0%	13%	4%	3%	2%	2%	11%	0%	0%	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA		
Protected Phases		2			2			4			4		
Permitted Phases	2			2			4			4			
Actuated Green, G (s)		30.0			30.0			12.0			12.0		
Effective Green, g (s)		30.0			30.0			12.0			12.0		
Actuated g/C Ratio		0.39			0.39			0.16			0.16		
Clearance Time (s)		5.0			5.0			5.0			5.0		
Lane Grp Cap (vph)		1240			998			266			205		
v/s Ratio Prot													
v/s Ratio Perm		0.22			0.41			0.12			0.09		
v/c Ratio		0.55			1.04			0.73			0.55		
Uniform Delay, d1		17.8			23.0			30.5			29.5		
Progression Factor		1.00			1.00			1.00			1.00		
Incremental Delay, d2		1.8			40.0			16.3			10.3		
Delay (s)		19.6			63.0			46.8			39.8		
Level of Service		B			E			D			D		
Approach Delay (s)		19.6			63.0			46.8			39.8		
Approach LOS		B			E			D			D		
Intersection Summary													
HCM 2000 Control Delay			45.5									HCM 2000 Level of Service	D
HCM 2000 Volume to Capacity ratio			0.63										
Actuated Cycle Length (s)			76.0									Sum of lost time (s)	13.0
Intersection Capacity Utilization			70.4%									ICU Level of Service	C
Analysis Period (min)			15										

c Critical Lane Group

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔			↔				
Traffic Vol, veh/h	10	718	0	0	869	8	0	0	2	0	0	0
Future Vol, veh/h	10	718	0	0	869	8	0	0	2	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	10	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	2	-	-	2	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	2	0	0	1	0	0	0	0	0	0	0
Mvmt Flow	11	780	0	0	945	9	0	0	2	0	0	0

Major/Minor	Major1		Major2			Minor1			
Conflicting Flow All	954	0	0	780	0	0	1285	1756	390
Stage 1	-	-	-	-	-	-	802	802	-
Stage 2	-	-	-	-	-	-	483	954	-
Critical Hdwy	4.1	-	-	4.1	-	-	6.8	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	5.8	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	5.8	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3
Pot Cap-1 Maneuver	729	-	-	846	-	-	159	86	614
Stage 1	-	-	-	-	-	-	407	399	-
Stage 2	-	-	-	-	-	-	592	340	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	729	-	-	846	-	-	153	0	614
Mov Cap-2 Maneuver	-	-	-	-	-	-	153	0	-
Stage 1	-	-	-	-	-	-	396	0	-
Stage 2	-	-	-	-	-	-	587	0	-

Approach	EB	WB	NB
HCM Control Delay, s	0.2	0	10.9
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR
Capacity (veh/h)	614	729	-	-	846	-	-
HCM Lane V/C Ratio	0.004	0.015	-	-	-	-	-
HCM Control Delay (s)	10.9	10	0.1	-	0	-	-
HCM Lane LOS	B	B	A	-	A	-	-
HCM 95th %tile Q(veh)	0	0	-	-	0	-	-

Intersection						
Int Delay, s/veh	0.7					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↑	
Traffic Vol, veh/h	714	7	27	1007	4	27
Future Vol, veh/h	714	7	27	1007	4	27
Conflicting Peds, #/hr	0	0	0	0	13	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	2	-	-	2	3	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	17	0	1	25	0
Mvmt Flow	776	8	29	1095	4	29

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	784	0	1399
Stage 1	-	-	-	-	780
Stage 2	-	-	-	-	619
Critical Hdwy	-	-	4.1	-	7.9
Critical Hdwy Stg 1	-	-	-	-	6.9
Critical Hdwy Stg 2	-	-	-	-	6.9
Follow-up Hdwy	-	-	2.2	-	3.75
Pot Cap-1 Maneuver	-	-	843	-	85
Stage 1	-	-	-	-	314
Stage 2	-	-	-	-	398
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	843	-	77
Mov Cap-2 Maneuver	-	-	-	-	77
Stage 1	-	-	-	-	314
Stage 2	-	-	-	-	359

Approach	EB	WB	NB
HCM Control Delay, s	0	0.6	17.7
HCM LOS			C

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	318	-	-	843	-
HCM Lane V/C Ratio	0.106	-	-	0.035	-
HCM Control Delay (s)	17.7	-	-	9.4	0.4
HCM Lane LOS	C	-	-	A	A
HCM 95th %tile Q(veh)	0.4	-	-	0.1	-

Intersection

Int Delay, s/veh 1.4

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	1	0	4	2	0	0	0	28	2	6	29	2
Future Vol, veh/h	1	0	4	2	0	0	0	28	2	6	29	2
Conflicting Peds, #/hr	1	0	0	6	0	0	1	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	3	-	-	3	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	0	0	0	0	4	0	0	4	0
Mvmt Flow	1	0	4	2	0	0	0	30	2	7	32	2

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	80	80	40	86	80	32	35	0	0	32	0	0
Stage 1	48	48	-	31	31	-	-	-	-	-	-	-
Stage 2	32	32	-	55	49	-	-	-	-	-	-	-
Critical Hdwy	7.1	6.5	6.2	7.1	6.5	6.2	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.1	5.5	-	6.1	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.1	5.5	-	6.1	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	913	814	1037	905	814	1048	1589	-	-	1593	-	-
Stage 1	971	859	-	991	873	-	-	-	-	-	-	-
Stage 2	990	872	-	962	858	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	908	810	1029	892	810	1047	1587	-	-	1593	-	-
Mov Cap-2 Maneuver	908	810	-	892	810	-	-	-	-	-	-	-
Stage 1	970	855	-	991	873	-	-	-	-	-	-	-
Stage 2	989	872	-	947	854	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	8.6		9		0		1.2	
HCM LOS	A		A					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1587	-	-	1002	892	1593	-	-
HCM Lane V/C Ratio	-	-	-	0.005	0.002	0.004	-	-
HCM Control Delay (s)	0	-	-	8.6	9	7.3	0	-
HCM Lane LOS	A	-	-	A	A	A	A	-
HCM 95th %tile Q(veh)	0	-	-	0	0	0	-	-

Lanes, Volumes, Timings
1: Downing Street & Park Avenue (Route 12)

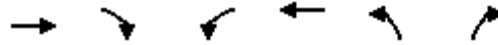
Build Conditions AM



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR	Ø3
Lane Configurations	↑↑			↑↑	↑↑		
Traffic Volume (vph)	962	10	34	570	13	13	
Future Volume (vph)	962	10	34	570	13	13	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	
Lane Width (ft)	11	11	11	11	15	15	
Grade (%)	2%			0%	3%		
Lane Util. Factor	0.95	0.95	0.95	0.95	1.00	1.00	
Ped Bike Factor				1.00	1.00		
Frt	0.998				0.932		
Flt Protected				0.997	0.976		
Satd. Flow (prot)	3317	0	0	3383	1726	0	
Flt Permitted				0.855	0.976		
Satd. Flow (perm)	3317	0	0	2901	1723	0	
Right Turn on Red		Yes				Yes	
Satd. Flow (RTOR)	2				14		
Link Speed (mph)	30			30	30		
Link Distance (ft)	351			179	249		
Travel Time (s)	8.0			4.1	5.7		
Confl. Peds. (#/hr)			7		2		
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	
Heavy Vehicles (%)	4%	0%	0%	3%	17%	0%	
Adj. Flow (vph)	1046	11	37	620	14	14	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	1057	0	0	657	28	0	
Enter Blocked Intersection	No	No	No	No	No	No	
Lane Alignment	Left	Right	Left	Left	Left	Right	
Median Width(ft)	0			0	15		
Link Offset(ft)	0			0	0		
Crosswalk Width(ft)	16			16	16		
Two way Left Turn Lane							
Headway Factor	1.06	1.06	1.04	1.04	0.90	0.90	
Turning Speed (mph)		9	15		15	9	
Number of Detectors	2		1	2	1		
Detector Template	Thru		Left	Thru	Left		
Leading Detector (ft)	100		20	100	20		
Trailing Detector (ft)	0		0	0	0		
Detector 1 Position(ft)	0		0	0	0		
Detector 1 Size(ft)	6		20	6	20		
Detector 1 Type	Cl+Ex		Cl+Ex	Cl+Ex	Cl+Ex		
Detector 1 Channel							
Detector 1 Extend (s)	0.0		0.0	0.0	0.0		
Detector 1 Queue (s)	0.0		0.0	0.0	0.0		
Detector 1 Delay (s)	0.0		0.0	0.0	0.0		
Detector 2 Position(ft)	94			94			
Detector 2 Size(ft)	6			6			
Detector 2 Type	Cl+Ex			Cl+Ex			
Detector 2 Channel							
Detector 2 Extend (s)	0.0			0.0			
Turn Type	NA		Perm	NA	Perm		

Lanes, Volumes, Timings
1: Downing Street & Park Avenue (Route 12)

Build Conditions AM

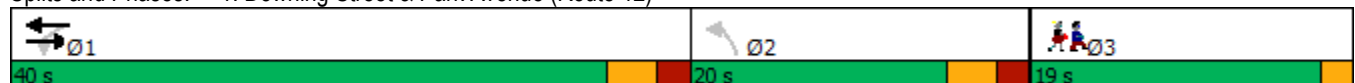


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR	Ø3
Protected Phases	1			1			3
Permitted Phases			1		2		
Detector Phase	1		1	1	2		
Switch Phase							
Minimum Initial (s)	8.0		8.0	8.0	6.0		5.0
Minimum Split (s)	23.0		23.0	23.0	11.0		19.0
Total Split (s)	40.0		40.0	40.0	20.0		19.0
Total Split (%)	50.6%		50.6%	50.6%	25.3%		24%
Maximum Green (s)	35.0		35.0	35.0	15.0		17.0
Yellow Time (s)	3.0		3.0	3.0	3.0		2.0
All-Red Time (s)	2.0		2.0	2.0	2.0		0.0
Lost Time Adjust (s)	0.0			0.0	0.0		
Total Lost Time (s)	5.0			5.0	5.0		
Lead/Lag	Lead		Lead	Lead	Lag		
Lead-Lag Optimize?	Yes		Yes	Yes	Yes		
Vehicle Extension (s)	3.0		3.0	3.0	3.0		3.0
Recall Mode	Max		Max	Max	None		Ped
Walk Time (s)							7.0
Flash Dont Walk (s)							11.0
Pedestrian Calls (#/hr)							9
Act Effct Green (s)	35.3			35.3	6.5		
Actuated g/C Ratio	0.54			0.54	0.10		
v/c Ratio	0.59			0.42	0.15		
Control Delay	12.7			10.8	22.2		
Queue Delay	0.0			0.0	0.0		
Total Delay	12.7			10.8	22.2		
LOS	B			B	C		
Approach Delay	12.7			10.8	22.2		
Approach LOS	B			B	C		

Intersection Summary

Area Type:	Other
Cycle Length:	79
Actuated Cycle Length:	64.9
Natural Cycle:	60
Control Type:	Semi Act-Uncoord
Maximum v/c Ratio:	0.59
Intersection Signal Delay:	12.1
Intersection LOS:	B
Intersection Capacity Utilization	54.5%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 1: Downing Street & Park Avenue (Route 12)



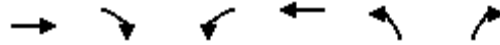
1: Downing Street & Park Avenue (Route 12)



Lane Group	EBT	WBT	NBL
Lane Group Flow (vph)	1057	657	28
v/c Ratio	0.59	0.42	0.15
Control Delay	12.7	10.8	22.2
Queue Delay	0.0	0.0	0.0
Total Delay	12.7	10.8	22.2
Queue Length 50th (ft)	107	59	5
Queue Length 95th (ft)	246	142	28
Internal Link Dist (ft)	271	99	169
Turn Bay Length (ft)			
Base Capacity (vph)	1803	1576	412
Starvation Cap Reductn	0	0	0
Spillback Cap Reductn	0	0	0
Storage Cap Reductn	0	0	0
Reduced v/c Ratio	0.59	0.42	0.07
Intersection Summary			

HCM Signalized Intersection Capacity Analysis
 1: Downing Street & Park Avenue (Route 12)

Build Conditions AM



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↑↑	
Traffic Volume (vph)	962	10	34	570	13	13
Future Volume (vph)	962	10	34	570	13	13
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	11	11	11	11	15	15
Grade (%)	2%			0%	3%	
Total Lost time (s)	5.0			5.0	5.0	
Lane Util. Factor	0.95			0.95	1.00	
Frbp, ped/bikes	1.00			1.00	1.00	
Flpb, ped/bikes	1.00			1.00	0.99	
Frt	1.00			1.00	0.93	
Flt Protected	1.00			1.00	0.98	
Satd. Flow (prot)	3318			3384	1713	
Flt Permitted	1.00			0.86	0.98	
Satd. Flow (perm)	3318			2902	1713	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	1046	11	37	620	14	14
RTOR Reduction (vph)	1	0	0	0	13	0
Lane Group Flow (vph)	1056	0	0	657	15	0
Confl. Peds. (#/hr)			7		2	
Heavy Vehicles (%)	4%	0%	0%	3%	17%	0%
Turn Type	NA		Perm	NA	Perm	
Protected Phases	1			1		
Permitted Phases			1		2	
Actuated Green, G (s)	35.3			35.3	2.6	
Effective Green, g (s)	35.3			35.3	2.6	
Actuated g/C Ratio	0.52			0.52	0.04	
Clearance Time (s)	5.0			5.0	5.0	
Vehicle Extension (s)	3.0			3.0	3.0	
Lane Grp Cap (vph)	1719			1504	65	
v/s Ratio Prot	c0.32					
v/s Ratio Perm				0.23	c0.01	
v/c Ratio	0.61			0.44	0.22	
Uniform Delay, d1	11.6			10.2	31.8	
Progression Factor	1.00			1.00	1.00	
Incremental Delay, d2	1.7			0.9	1.7	
Delay (s)	13.2			11.1	33.5	
Level of Service	B			B	C	
Approach Delay (s)	13.2			11.1	33.5	
Approach LOS	B			B	C	

Intersection Summary

HCM 2000 Control Delay	12.8	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.40		
Actuated Cycle Length (s)	68.1	Sum of lost time (s)	12.0
Intersection Capacity Utilization	54.5%	ICU Level of Service	A
Analysis Period (min)	15		

c Critical Lane Group

Lanes, Volumes, Timings
3: Maywood Street & Park Avenue (Route 12)

Build Conditions AM



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕			↕↕			↕			↕	
Traffic Volume (vph)	17	925	33	32	428	48	47	73	54	78	62	18
Future Volume (vph)	17	925	33	32	428	48	47	73	54	78	62	18
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	11	11	11	11	11	11	16	16	16	16	16	16
Grade (%)		2%			2%			2%			2%	
Lane Util. Factor	0.95	0.95	0.95	0.95	0.95	0.95	1.00	1.00	1.00	1.00	1.00	1.00
Ped Bike Factor		1.00			1.00			1.00			1.00	
Frt		0.995			0.986			0.958			0.984	
Flt Protected		0.999			0.997			0.987			0.976	
Satd. Flow (prot)	0	3363	0	0	3254	0	0	1988	0	0	1966	0
Flt Permitted		0.941			0.772			0.840			0.655	
Satd. Flow (perm)	0	3168	0	0	2519	0	0	1689	0	0	1317	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		5			17			26			7	
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		189			269			185			216	
Travel Time (s)		4.3			6.1			4.2			4.9	
Confl. Peds. (#/hr)	1			8			6			3		
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Heavy Vehicles (%)	13%	2%	0%	4%	4%	8%	2%	2%	0%	6%	3%	0%
Adj. Flow (vph)	18	1005	36	35	465	52	51	79	59	85	67	20
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	1059	0	0	552	0	0	189	0	0	172	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		0			0			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.06	1.06	1.06	1.06	1.06	1.06	0.86	0.86	0.86	0.86	0.86	0.86
Turning Speed (mph)	15		9	15		9	15		9	15		9
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		2			2			4			4	
Permitted Phases	2			2			4			4		
Minimum Split (s)	28.0	28.0		28.0	28.0		13.0	13.0		13.0	13.0	
Total Split (s)	35.0	35.0		35.0	35.0		17.0	17.0		17.0	17.0	
Total Split (%)	46.1%	46.1%		46.1%	46.1%		22.4%	22.4%		22.4%	22.4%	
Maximum Green (s)	30.0	30.0		30.0	30.0		12.0	12.0		12.0	12.0	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		5.0			5.0			5.0			5.0	
Lead/Lag							Lag	Lag		Lag	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Walk Time (s)												
Flash Dont Walk (s)												
Pedestrian Calls (#/hr)												
Act Effect Green (s)		30.0			30.0			12.0			12.0	

Lane Group	Ø3
Lane Configurations	
Traffic Volume (vph)	
Future Volume (vph)	
Ideal Flow (vphpl)	
Lane Width (ft)	
Grade (%)	
Lane Util. Factor	
Ped Bike Factor	
Frt	
Flt Protected	
Satd. Flow (prot)	
Flt Permitted	
Satd. Flow (perm)	
Right Turn on Red	
Satd. Flow (RTOR)	
Link Speed (mph)	
Link Distance (ft)	
Travel Time (s)	
Confl. Peds. (#/hr)	
Peak Hour Factor	
Heavy Vehicles (%)	
Adj. Flow (vph)	
Shared Lane Traffic (%)	
Lane Group Flow (vph)	
Enter Blocked Intersection	
Lane Alignment	
Median Width(ft)	
Link Offset(ft)	
Crosswalk Width(ft)	
Two way Left Turn Lane	
Headway Factor	
Turning Speed (mph)	
Turn Type	
Protected Phases	3
Permitted Phases	
Minimum Split (s)	24.0
Total Split (s)	24.0
Total Split (%)	32%
Maximum Green (s)	21.0
Yellow Time (s)	2.0
All-Red Time (s)	1.0
Lost Time Adjust (s)	
Total Lost Time (s)	
Lead/Lag	Lead
Lead-Lag Optimize?	Yes
Walk Time (s)	7.0
Flash Dont Walk (s)	16.0
Pedestrian Calls (#/hr)	20
Act Effct Green (s)	

Lanes, Volumes, Timings
 3: Maywood Street & Park Avenue (Route 12)

Build Conditions AM

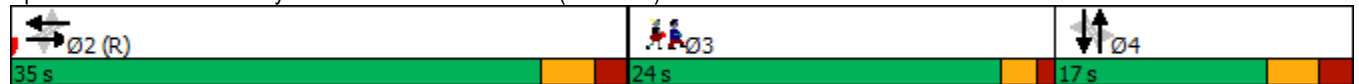


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Actuated g/C Ratio		0.39			0.39			0.16			0.16	
v/c Ratio		0.85			0.55			0.66			0.81	
Control Delay		28.8			19.7			38.2			59.6	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		28.8			19.7			38.2			59.6	
LOS		C			B			D			E	
Approach Delay		28.8			19.7			38.2			59.6	
Approach LOS		C			B			D			E	

Intersection Summary

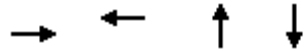
Area Type:	Other
Cycle Length:	76
Actuated Cycle Length:	76
Offset:	0 (0%), Referenced to phase 2:EBWB and 6:, Start of Green
Natural Cycle:	70
Control Type:	Pretimed
Maximum v/c Ratio:	0.85
Intersection Signal Delay:	29.8
Intersection LOS:	C
Intersection Capacity Utilization	62.5%
ICU Level of Service	B
Analysis Period (min)	15

Splits and Phases: 3: Maywood Street & Park Avenue (Route 12)



Lane Group	Ø3
Actuated g/C Ratio	
v/c Ratio	
Control Delay	
Queue Delay	
Total Delay	
LOS	
Approach Delay	
Approach LOS	
Intersection Summary	

3: Maywood Street & Park Avenue (Route 12)



Lane Group	EBT	WBT	NBT	SBT
Lane Group Flow (vph)	1059	552	189	172
v/c Ratio	0.85	0.55	0.66	0.81
Control Delay	28.8	19.7	38.2	59.6
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	28.8	19.7	38.2	59.6
Queue Length 50th (ft)	231	100	73	76
Queue Length 95th (ft)	#328	147	#156	#180
Internal Link Dist (ft)	109	189	105	136
Turn Bay Length (ft)				
Base Capacity (vph)	1253	1004	288	213
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.85	0.55	0.66	0.81

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.

HCM Signalized Intersection Capacity Analysis

3: Maywood Street & Park Avenue (Route 12)

Build Conditions AM



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations		↕↕			↕↕			↕			↕		
Traffic Volume (vph)	17	925	33	32	428	48	47	73	54	78	62	18	
Future Volume (vph)	17	925	33	32	428	48	47	73	54	78	62	18	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Lane Width	11	11	11	11	11	11	16	16	16	16	16	16	
Grade (%)		2%			2%			2%			2%		
Total Lost time (s)		5.0			5.0			5.0			5.0		
Lane Util. Factor		0.95			0.95			1.00			1.00		
Frbp, ped/bikes		1.00			1.00			1.00			1.00		
Flpb, ped/bikes		1.00			1.00			1.00			1.00		
Frt		0.99			0.99			0.96			0.98		
Flt Protected		1.00			1.00			0.99			0.98		
Satd. Flow (prot)		3363			3252			1984			1963		
Flt Permitted		0.94			0.77			0.84			0.66		
Satd. Flow (perm)		3168			2520			1689			1319		
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	
Adj. Flow (vph)	18	1005	36	35	465	52	51	79	59	85	67	20	
RTOR Reduction (vph)	0	3	0	0	10	0	0	22	0	0	6	0	
Lane Group Flow (vph)	0	1056	0	0	542	0	0	167	0	0	166	0	
Confl. Peds. (#/hr)	1			8			6			3			
Heavy Vehicles (%)	13%	2%	0%	4%	4%	8%	2%	2%	0%	6%	3%	0%	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA		
Protected Phases		2			2			4			4		
Permitted Phases	2			2			4			4			
Actuated Green, G (s)		30.0			30.0			12.0			12.0		
Effective Green, g (s)		30.0			30.0			12.0			12.0		
Actuated g/C Ratio		0.39			0.39			0.16			0.16		
Clearance Time (s)		5.0			5.0			5.0			5.0		
Lane Grp Cap (vph)		1250			994			266			208		
v/s Ratio Prot													
v/s Ratio Perm		c0.33			0.21			0.10			c0.13		
v/c Ratio		0.84			0.54			0.63			0.80		
Uniform Delay, d1		20.9			17.7			29.9			30.8		
Progression Factor		1.00			1.00			1.00			1.00		
Incremental Delay, d2		7.1			2.1			10.7			26.5		
Delay (s)		28.0			19.9			40.7			57.4		
Level of Service		C			B			D			E		
Approach Delay (s)		28.0			19.9			40.7			57.4		
Approach LOS		C			B			D			E		
Intersection Summary													
HCM 2000 Control Delay			29.5									HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio			0.55										
Actuated Cycle Length (s)			76.0									Sum of lost time (s)	13.0
Intersection Capacity Utilization			62.5%									ICU Level of Service	B
Analysis Period (min)			15										
c	Critical Lane Group												

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔↔			↔↔			↔				
Traffic Vol, veh/h	2	962	35	9	543	1	19	0	17	0	0	0
Future Vol, veh/h	2	962	35	9	543	1	19	0	17	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	10	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	2	-	-	2	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	4	0	0	3	0	0	0	0	0	0	0
Mvmt Flow	2	1046	38	10	590	1	21	0	18	0	0	0

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	591	0	0	1084	0	0	1394	1680	542
Stage 1	-	-	-	-	-	-	1069	1069	-
Stage 2	-	-	-	-	-	-	325	611	-
Critical Hdwy	4.1	-	-	4.1	-	-	6.8	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	5.8	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	5.8	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3
Pot Cap-1 Maneuver	995	-	-	651	-	-	135	96	490
Stage 1	-	-	-	-	-	-	296	300	-
Stage 2	-	-	-	-	-	-	711	487	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	995	-	-	651	-	-	130	0	490
Mov Cap-2 Maneuver	-	-	-	-	-	-	130	0	-
Stage 1	-	-	-	-	-	-	295	0	-
Stage 2	-	-	-	-	-	-	688	0	-

Approach	EB			WB			NB		
HCM Control Delay, s	0			0.3			27.5		
HCM LOS							D		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR
Capacity (veh/h)	199	995	-	-	651	-	-
HCM Lane V/C Ratio	0.197	0.002	-	-	0.015	-	-
HCM Control Delay (s)	27.5	8.6	0	-	10.6	0.1	-
HCM Lane LOS	D	A	A	-	B	A	-
HCM 95th %tile Q(veh)	0.7	0	-	-	0	-	-

Intersection						
Int Delay, s/veh	0.8					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↑	
Traffic Vol, veh/h	1063	0	18	502	9	22
Future Vol, veh/h	1063	0	18	502	9	22
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	2	-	-	2	3	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	3	0	0	4	33	0
Mvmt Flow	1155	0	20	546	10	24

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	1155	0	1468
Stage 1	-	-	-	-	1155
Stage 2	-	-	-	-	313
Critical Hdwy	-	-	4.1	-	8.06
Critical Hdwy Stg 1	-	-	-	-	7.06
Critical Hdwy Stg 2	-	-	-	-	7.06
Follow-up Hdwy	-	-	2.2	-	3.83
Pot Cap-1 Maneuver	-	-	612	-	69
Stage 1	-	-	-	-	170
Stage 2	-	-	-	-	598
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	612	-	66
Mov Cap-2 Maneuver	-	-	-	-	66
Stage 1	-	-	-	-	170
Stage 2	-	-	-	-	570

Approach	EB	WB	NB
HCM Control Delay, s	0	0.6	31.9
HCM LOS			D

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	167	-	-	612	-
HCM Lane V/C Ratio	0.202	-	-	0.032	-
HCM Control Delay (s)	31.9	-	-	11.1	0.2
HCM Lane LOS	D	-	-	B	A
HCM 95th %tile Q(veh)	0.7	-	-	0.1	-

Intersection												
Int Delay, s/veh	4.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	22	0	1	0	0	3	0	10	1	9	20	1
Future Vol, veh/h	22	0	1	0	0	3	0	10	1	9	20	1
Conflicting Peds, #/hr	2	0	0	2	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	3	-	-	3	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	2	2	2	0	0	0	0	0	0
Mvmt Flow	24	0	1	0	0	3	0	11	1	10	22	1

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	58	55	25	57	55	14	23	0	0	12	0	0
Stage 1	43	43	-	12	12	-	-	-	-	-	-	-
Stage 2	15	12	-	45	43	-	-	-	-	-	-	-
Critical Hdwy	7.1	6.5	6.2	7.12	6.52	6.22	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.1	5.5	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.1	5.5	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.518	4.018	3.318	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	944	840	1057	940	836	1066	1605	-	-	1620	-	-
Stage 1	976	863	-	1009	886	-	-	-	-	-	-	-
Stage 2	1010	890	-	969	859	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	935	835	1054	932	831	1063	1605	-	-	1620	-	-
Mov Cap-2 Maneuver	935	835	-	932	831	-	-	-	-	-	-	-
Stage 1	976	858	-	1009	886	-	-	-	-	-	-	-
Stage 2	1005	890	-	960	854	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	8.9		8.4		0		2.2	
HCM LOS	A		A					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1605	-	-	940	1063	1620	-	-
HCM Lane V/C Ratio	-	-	-	0.027	0.003	0.006	-	-
HCM Control Delay (s)	0	-	-	8.9	8.4	7.2	0	-
HCM Lane LOS	A	-	-	A	A	A	A	-
HCM 95th %tile Q(veh)	0	-	-	0.1	0	0	-	-

Lanes, Volumes, Timings
1: Downing Street & Park Avenue (Route 12)

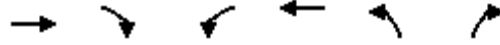
Build Conditions PM



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR	Ø3
Lane Configurations	↑↑			↑↑	↑↑		
Traffic Volume (vph)	735	18	27	1000	41	28	
Future Volume (vph)	735	18	27	1000	41	28	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	
Lane Width (ft)	11	11	11	11	15	15	
Grade (%)	2%			0%	3%		
Lane Util. Factor	0.95	0.95	0.95	0.95	1.00	1.00	
Ped Bike Factor				1.00	0.98		
Frt	0.996				0.946		
Flt Protected				0.999	0.971		
Satd. Flow (prot)	3375	0	0	3453	1847	0	
Flt Permitted				0.918	0.971		
Satd. Flow (perm)	3375	0	0	3172	1828	0	
Right Turn on Red		Yes				Yes	
Satd. Flow (RTOR)	4				30		
Link Speed (mph)	30			30	30		
Link Distance (ft)	351			179	249		
Travel Time (s)	8.0			4.1	5.7		
Confl. Peds. (#/hr)			11		11		
Confl. Bikes (#/hr)						2	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	
Heavy Vehicles (%)	2%	0%	0%	1%	3%	0%	
Adj. Flow (vph)	799	20	29	1087	45	30	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	819	0	0	1116	75	0	
Enter Blocked Intersection	No	No	No	No	No	No	
Lane Alignment	Left	Right	Left	Left	Left	Right	
Median Width(ft)	0			0	15		
Link Offset(ft)	0			0	0		
Crosswalk Width(ft)	16			16	16		
Two way Left Turn Lane							
Headway Factor	1.06	1.06	1.04	1.04	0.90	0.90	
Turning Speed (mph)		9	15		15	9	
Number of Detectors	2		1	2	1		
Detector Template	Thru		Left	Thru	Left		
Leading Detector (ft)	100		20	100	20		
Trailing Detector (ft)	0		0	0	0		
Detector 1 Position(ft)	0		0	0	0		
Detector 1 Size(ft)	6		20	6	20		
Detector 1 Type	Cl+Ex		Cl+Ex	Cl+Ex	Cl+Ex		
Detector 1 Channel							
Detector 1 Extend (s)	0.0		0.0	0.0	0.0		
Detector 1 Queue (s)	0.0		0.0	0.0	0.0		
Detector 1 Delay (s)	0.0		0.0	0.0	0.0		
Detector 2 Position(ft)	94			94			
Detector 2 Size(ft)	6			6			
Detector 2 Type	Cl+Ex			Cl+Ex			
Detector 2 Channel							
Detector 2 Extend (s)	0.0			0.0			

Lanes, Volumes, Timings
 1: Downing Street & Park Avenue (Route 12)

Build Conditions PM

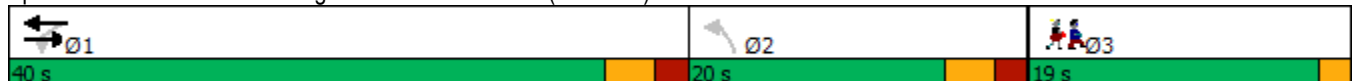


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR	Ø3
Turn Type	NA		Perm	NA	Perm		
Protected Phases	1			1			3
Permitted Phases			1		2		
Detector Phase	1		1	1	2		
Switch Phase							
Minimum Initial (s)	8.0		8.0	8.0	6.0		5.0
Minimum Split (s)	23.0		23.0	23.0	11.0		19.0
Total Split (s)	40.0		40.0	40.0	20.0		19.0
Total Split (%)	50.6%		50.6%	50.6%	25.3%		24%
Maximum Green (s)	35.0		35.0	35.0	15.0		17.0
Yellow Time (s)	3.0		3.0	3.0	3.0		2.0
All-Red Time (s)	2.0		2.0	2.0	2.0		0.0
Lost Time Adjust (s)	0.0			0.0	0.0		
Total Lost Time (s)	5.0			5.0	5.0		
Lead/Lag	Lead		Lead	Lead	Lag		
Lead-Lag Optimize?	Yes		Yes	Yes	Yes		
Vehicle Extension (s)	3.0		3.0	3.0	3.0		3.0
Recall Mode	Max		Max	Max	None		Ped
Walk Time (s)							7.0
Flash Dont Walk (s)							11.0
Pedestrian Calls (#/hr)							9
Act Effct Green (s)	35.2			35.2	7.5		
Actuated g/C Ratio	0.50			0.50	0.11		
v/c Ratio	0.48			0.70	0.34		
Control Delay	13.6			17.6	24.6		
Queue Delay	0.0			0.0	0.0		
Total Delay	13.6			17.6	24.6		
LOS	B			B	C		
Approach Delay	13.6			17.6	24.6		
Approach LOS	B			B	C		

Intersection Summary

Area Type: Other
 Cycle Length: 79
 Actuated Cycle Length: 70.4
 Natural Cycle: 60
 Control Type: Semi Act-Uncoord
 Maximum v/c Ratio: 0.70
 Intersection Signal Delay: 16.2
 Intersection LOS: B
 Intersection Capacity Utilization 60.4%
 ICU Level of Service B
 Analysis Period (min) 15

Splits and Phases: 1: Downing Street & Park Avenue (Route 12)



1: Downing Street & Park Avenue (Route 12)

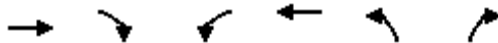


Lane Group	EBT	WBT	NBL
Lane Group Flow (vph)	819	1116	75
v/c Ratio	0.48	0.70	0.34
Control Delay	13.6	17.6	24.6
Queue Delay	0.0	0.0	0.0
Total Delay	13.6	17.6	24.6
Queue Length 50th (ft)	122	196	19
Queue Length 95th (ft)	185	293	56
Internal Link Dist (ft)	271	99	169
Turn Bay Length (ft)			
Base Capacity (vph)	1691	1587	415
Starvation Cap Reductn	0	0	0
Spillback Cap Reductn	0	0	0
Storage Cap Reductn	0	0	0
Reduced v/c Ratio	0.48	0.70	0.18
Intersection Summary			

HCM Signalized Intersection Capacity Analysis

1: Downing Street & Park Avenue (Route 12)

Build Conditions PM



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↑↑	
Traffic Volume (vph)	735	18	27	1000	41	28
Future Volume (vph)	735	18	27	1000	41	28
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	11	11	11	11	15	15
Grade (%)	2%			0%	3%	
Total Lost time (s)	5.0			5.0	5.0	
Lane Util. Factor	0.95			0.95	1.00	
Frbp, ped/bikes	1.00			1.00	0.99	
Flpb, ped/bikes	1.00			1.00	0.98	
Frt	1.00			1.00	0.95	
Flt Protected	1.00			1.00	0.97	
Satd. Flow (prot)	3376			3451	1802	
Flt Permitted	1.00			0.92	0.97	
Satd. Flow (perm)	3376			3174	1802	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	799	20	29	1087	45	30
RTOR Reduction (vph)	2	0	0	0	27	0
Lane Group Flow (vph)	817	0	0	1116	48	0
Confl. Peds. (#/hr)			11		11	
Confl. Bikes (#/hr)						2
Heavy Vehicles (%)	2%	0%	0%	1%	3%	0%
Turn Type	NA		Perm	NA	Perm	
Protected Phases	1			1		
Permitted Phases			1		2	
Actuated Green, G (s)	35.2			35.2	6.1	
Effective Green, g (s)	35.2			35.2	6.1	
Actuated g/C Ratio	0.49			0.49	0.09	
Clearance Time (s)	5.0			5.0	5.0	
Vehicle Extension (s)	3.0			3.0	3.0	
Lane Grp Cap (vph)	1664			1564	153	
v/s Ratio Prot	0.24					
v/s Ratio Perm				c0.35	c0.03	
v/c Ratio	0.49			0.71	0.31	
Uniform Delay, d1	12.1			14.2	30.7	
Progression Factor	1.00			1.00	1.00	
Incremental Delay, d2	1.0			2.8	1.2	
Delay (s)	13.1			17.0	31.8	
Level of Service	B			B	C	
Approach Delay (s)	13.1			17.0	31.8	
Approach LOS	B			B	C	

Intersection Summary

HCM 2000 Control Delay	16.0	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.45		
Actuated Cycle Length (s)	71.4	Sum of lost time (s)	12.0
Intersection Capacity Utilization	60.4%	ICU Level of Service	B
Analysis Period (min)	15		
c Critical Lane Group			

Lanes, Volumes, Timings
3: Maywood Street & Park Avenue (Route 12)

Build Conditions PM



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕			↕↕			↕			↕	
Traffic Volume (vph)	10	605	31	64	865	45	68	64	73	50	49	11
Future Volume (vph)	10	605	31	64	865	45	68	64	73	50	49	11
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	11	11	11	11	11	11	16	16	16	16	16	16
Grade (%)		2%			2%			2%			2%	
Lane Util. Factor	0.95	0.95	0.95	0.95	0.95	0.95	1.00	1.00	1.00	1.00	1.00	1.00
Ped Bike Factor		1.00			1.00			1.00			1.00	
Frt		0.993			0.993			0.952			0.986	
Flt Protected		0.999			0.997			0.984			0.978	
Satd. Flow (prot)	0	3364	0	0	3061	0	0	1951	0	0	1955	0
Flt Permitted		0.933			0.811			0.855			0.645	
Satd. Flow (perm)	0	3142	0	0	2489	0	0	1690	0	0	1285	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		8			8			31			6	
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		189			269			185			216	
Travel Time (s)		4.3			6.1			4.2			4.9	
Confl. Peds. (#/hr)	3			14			7			8		
Confl. Bikes (#/hr)												2
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Heavy Vehicles (%)	0%	2%	0%	0%	13%	4%	3%	2%	2%	11%	0%	0%
Adj. Flow (vph)	11	658	34	70	940	49	74	70	79	54	53	12
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	703	0	0	1059	0	0	223	0	0	119	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		0			0			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.06	1.06	1.06	1.06	1.06	1.06	0.86	0.86	0.86	0.86	0.86	0.86
Turning Speed (mph)	15		9	15		9	15		9	15		9
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		2			2			4			4	
Permitted Phases	2			2			4			4		
Minimum Split (s)	28.0	28.0		28.0	28.0		13.0	13.0		13.0	13.0	
Total Split (s)	35.0	35.0		35.0	35.0		17.0	17.0		17.0	17.0	
Total Split (%)	46.1%	46.1%		46.1%	46.1%		22.4%	22.4%		22.4%	22.4%	
Maximum Green (s)	30.0	30.0		30.0	30.0		12.0	12.0		12.0	12.0	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		5.0			5.0			5.0			5.0	
Lead/Lag							Lag	Lag		Lag	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Walk Time (s)												
Flash Dont Walk (s)												
Pedestrian Calls (#/hr)												

Lane Group	Ø3
Lane Configurations	
Traffic Volume (vph)	
Future Volume (vph)	
Ideal Flow (vphpl)	
Lane Width (ft)	
Grade (%)	
Lane Util. Factor	
Ped Bike Factor	
Frt	
Flt Protected	
Satd. Flow (prot)	
Flt Permitted	
Satd. Flow (perm)	
Right Turn on Red	
Satd. Flow (RTOR)	
Link Speed (mph)	
Link Distance (ft)	
Travel Time (s)	
Confl. Peds. (#/hr)	
Confl. Bikes (#/hr)	
Peak Hour Factor	
Heavy Vehicles (%)	
Adj. Flow (vph)	
Shared Lane Traffic (%)	
Lane Group Flow (vph)	
Enter Blocked Intersection	
Lane Alignment	
Median Width(ft)	
Link Offset(ft)	
Crosswalk Width(ft)	
Two way Left Turn Lane	
Headway Factor	
Turning Speed (mph)	
Turn Type	
Protected Phases	3
Permitted Phases	
Minimum Split (s)	24.0
Total Split (s)	24.0
Total Split (%)	32%
Maximum Green (s)	21.0
Yellow Time (s)	2.0
All-Red Time (s)	1.0
Lost Time Adjust (s)	
Total Lost Time (s)	
Lead/Lag	Lead
Lead-Lag Optimize?	Yes
Walk Time (s)	7.0
Flash Dont Walk (s)	16.0
Pedestrian Calls (#/hr)	20

Lanes, Volumes, Timings
3: Maywood Street & Park Avenue (Route 12)

Build Conditions PM

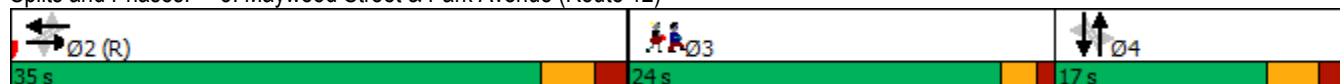


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Act Effct Green (s)		30.0			30.0			12.0			12.0	
Actuated g/C Ratio		0.39			0.39			0.16			0.16	
v/c Ratio		0.56			1.07			0.76			0.57	
Control Delay		19.9			75.1			45.4			40.5	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		19.9			75.1			45.4			40.5	
LOS		B			E			D			D	
Approach Delay		19.9			75.1			45.4			40.5	
Approach LOS		B			E			D			D	

Intersection Summary

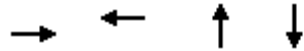
Area Type:	Other
Cycle Length:	76
Actuated Cycle Length:	76
Offset:	0 (0%), Referenced to phase 2:EBWB and 6:, Start of Green
Natural Cycle:	80
Control Type:	Pretimed
Maximum v/c Ratio:	1.07
Intersection Signal Delay:	51.6
Intersection LOS:	D
Intersection Capacity Utilization	71.1%
ICU Level of Service	C
Analysis Period (min)	15

Splits and Phases: 3: Maywood Street & Park Avenue (Route 12)



Lane Group	Ø3
Act Effct Green (s)	
Actuated g/C Ratio	
v/c Ratio	
Control Delay	
Queue Delay	
Total Delay	
LOS	
Approach Delay	
Approach LOS	
Intersection Summary	

3: Maywood Street & Park Avenue (Route 12)



Lane Group	EBT	WBT	NBT	SBT
Lane Group Flow (vph)	703	1059	223	119
v/c Ratio	0.56	1.07	0.76	0.57
Control Delay	19.9	75.1	45.4	40.5
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	19.9	75.1	45.4	40.5
Queue Length 50th (ft)	131	~297	88	50
Queue Length 95th (ft)	183	#418	#195	#113
Internal Link Dist (ft)	109	189	105	136
Turn Bay Length (ft)				
Base Capacity (vph)	1245	987	292	207
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.56	1.07	0.76	0.57

















Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.

HCM Signalized Intersection Capacity Analysis

3: Maywood Street & Park Avenue (Route 12)

Build Conditions PM

													
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations													
Traffic Volume (vph)	10	605	31	64	865	45	68	64	73	50	49	11	
Future Volume (vph)	10	605	31	64	865	45	68	64	73	50	49	11	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Lane Width	11	11	11	11	11	11	16	16	16	16	16	16	
Grade (%)		2%			2%			2%			2%		
Total Lost time (s)		5.0			5.0			5.0			5.0		
Lane Util. Factor		0.95			0.95			1.00			1.00		
Frbp, ped/bikes		1.00			1.00			1.00			1.00		
Flpb, ped/bikes		1.00			1.00			1.00			1.00		
Frt		0.99			0.99			0.95			0.99		
Flt Protected		1.00			1.00			0.98			0.98		
Satd. Flow (prot)		3364			3059			1945			1949		
Flt Permitted		0.93			0.81			0.85			0.65		
Satd. Flow (perm)		3143			2491			1690			1287		
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	
Adj. Flow (vph)	11	658	34	70	940	49	74	70	79	54	53	12	
RTOR Reduction (vph)	0	5	0	0	5	0	0	26	0	0	5	0	
Lane Group Flow (vph)	0	698	0	0	1054	0	0	197	0	0	114	0	
Confl. Peds. (#/hr)	3			14			7			8			
Confl. Bikes (#/hr)												2	
Heavy Vehicles (%)	0%	2%	0%	0%	13%	4%	3%	2%	2%	11%	0%	0%	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA		
Protected Phases		2			2			4			4		
Permitted Phases	2			2			4			4			
Actuated Green, G (s)		30.0			30.0			12.0			12.0		
Effective Green, g (s)		30.0			30.0			12.0			12.0		
Actuated g/C Ratio		0.39			0.39			0.16			0.16		
Clearance Time (s)		5.0			5.0			5.0			5.0		
Lane Grp Cap (vph)		1240			983			266			203		
v/s Ratio Prot													
v/s Ratio Perm		0.22			0.42			0.12			0.09		
v/c Ratio		0.56			1.07			0.74			0.56		
Uniform Delay, d1		17.9			23.0			30.5			29.6		
Progression Factor		1.00			1.00			1.00			1.00		
Incremental Delay, d2		1.9			50.2			16.9			10.8		
Delay (s)		19.8			73.2			47.4			40.3		
Level of Service		B			E			D			D		
Approach Delay (s)		19.8			73.2			47.4			40.3		
Approach LOS		B			E			D			D		
Intersection Summary													
HCM 2000 Control Delay			50.7									HCM 2000 Level of Service	D
HCM 2000 Volume to Capacity ratio			0.65										
Actuated Cycle Length (s)			76.0									Sum of lost time (s)	13.0
Intersection Capacity Utilization			71.1%									ICU Level of Service	C
Analysis Period (min)			15										

c Critical Lane Group

Intersection												
Int Delay, s/veh	0.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔			↔				
Traffic Vol, veh/h	10	718	13	7	872	8	10	0	7	0	0	0
Future Vol, veh/h	10	718	13	7	872	8	10	0	7	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	10	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	2	-	-	2	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	2	0	0	1	0	0	0	0	0	0	0
Mvmt Flow	11	780	14	8	948	9	11	0	8	0	0	0

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	957	0	0	794	0	0	1309	1782	397
Stage 1	-	-	-	-	-	-	809	809	-
Stage 2	-	-	-	-	-	-	500	973	-
Critical Hdwy	4.1	-	-	4.1	-	-	6.8	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	5.8	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	5.8	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3
Pot Cap-1 Maneuver	727	-	-	836	-	-	153	83	608
Stage 1	-	-	-	-	-	-	403	396	-
Stage 2	-	-	-	-	-	-	580	333	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	727	-	-	836	-	-	145	0	608
Mov Cap-2 Maneuver	-	-	-	-	-	-	145	0	-
Stage 1	-	-	-	-	-	-	392	0	-
Stage 2	-	-	-	-	-	-	563	0	-

Approach	EB			WB			NB		
HCM Control Delay, s	0.2			0.2			23.7		
HCM LOS							C		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR
Capacity (veh/h)	211	727	-	-	836	-	-
HCM Lane V/C Ratio	0.088	0.015	-	-	0.009	-	-
HCM Control Delay (s)	23.7	10	0.1	-	9.3	0.1	-
HCM Lane LOS	C	B	A	-	A	A	-
HCM 95th %tile Q(veh)	0.3	0	-	-	0	-	-

Intersection						
Int Delay, s/veh	1.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↑	
Traffic Vol, veh/h	719	7	34	1014	10	33
Future Vol, veh/h	719	7	34	1014	10	33
Conflicting Peds, #/hr	0	0	0	0	13	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	2	-	-	2	3	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	17	0	1	25	0
Mvmt Flow	782	8	37	1102	11	36

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	790	0	1424
Stage 1	-	-	-	-	786
Stage 2	-	-	-	-	638
Critical Hdwy	-	-	4.1	-	7.9
Critical Hdwy Stg 1	-	-	-	-	6.9
Critical Hdwy Stg 2	-	-	-	-	6.9
Follow-up Hdwy	-	-	2.2	-	3.75
Pot Cap-1 Maneuver	-	-	839	-	81
Stage 1	-	-	-	-	312
Stage 2	-	-	-	-	387
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	839	-	71
Mov Cap-2 Maneuver	-	-	-	-	71
Stage 1	-	-	-	-	312
Stage 2	-	-	-	-	339

Approach	EB	WB	NB
HCM Control Delay, s	0	0.8	25.8
HCM LOS			D

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	219	-	-	839	-
HCM Lane V/C Ratio	0.213	-	-	0.044	-
HCM Control Delay (s)	25.8	-	-	9.5	0.5
HCM Lane LOS	D	-	-	A	A
HCM 95th %tile Q(veh)	0.8	-	-	0.1	-

Intersection												
Int Delay, s/veh	2.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	10	0	5	2	0	0	0	28	2	6	29	9
Future Vol, veh/h	10	0	5	2	0	0	0	28	2	6	29	9
Conflicting Peds, #/hr	1	0	0	6	0	0	1	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	3	-	-	3	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	0	0	0	0	4	0	0	4	0
Mvmt Flow	11	0	5	2	0	0	0	30	2	7	32	10

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	84	84	44	91	88	32	43	0	0	32	0	0
Stage 1	52	52	-	31	31	-	-	-	-	-	-	-
Stage 2	32	32	-	60	57	-	-	-	-	-	-	-
Critical Hdwy	7.1	6.5	6.2	7.1	6.5	6.2	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.1	5.5	-	6.1	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.1	5.5	-	6.1	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	908	810	1032	898	806	1048	1579	-	-	1593	-	-
Stage 1	966	856	-	991	873	-	-	-	-	-	-	-
Stage 2	990	872	-	957	851	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	903	805	1024	884	801	1047	1577	-	-	1593	-	-
Mov Cap-2 Maneuver	903	805	-	884	801	-	-	-	-	-	-	-
Stage 1	965	851	-	991	873	-	-	-	-	-	-	-
Stage 2	989	872	-	940	846	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	8.9		9.1		0		1	
HCM LOS	A		A					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1577	-	-	940	884	1593	-	-
HCM Lane V/C Ratio	-	-	-	0.017	0.002	0.004	-	-
HCM Control Delay (s)	0	-	-	8.9	9.1	7.3	0	-
HCM Lane LOS	A	-	-	A	A	A	A	-
HCM 95th %tile Q(veh)	0	-	-	0.1	0	0	-	-

Lanes, Volumes, Timings
3: Maywood Street & Park Avenue (Route 12)

Build Conditions PM

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	10	605	31	64	865	45	68	64	73	50	49	11
Future Volume (vph)	10	605	31	64	865	45	68	64	73	50	49	11
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	11	11	11	11	11	11	16	16	16	16	16	16
Grade (%)		2%			2%			2%			2%	
Lane Util. Factor	0.95	0.95	0.95	0.95	0.95	0.95	1.00	1.00	1.00	1.00	1.00	1.00
Ped Bike Factor		1.00			1.00			1.00			1.00	
Frt		0.993			0.993			0.952			0.986	
Flt Protected		0.999			0.997			0.984			0.978	
Satd. Flow (prot)	0	3364	0	0	3061	0	0	1951	0	0	1955	0
Flt Permitted		0.934			0.817			0.850			0.643	
Satd. Flow (perm)	0	3145	0	0	2508	0	0	1680	0	0	1281	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		8			7			29			6	
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		189			269			185			216	
Travel Time (s)		4.3			6.1			4.2			4.9	
Confl. Peds. (#/hr)	3			14			7			8		
Confl. Bikes (#/hr)												2
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Heavy Vehicles (%)	0%	2%	0%	0%	13%	4%	3%	2%	2%	11%	0%	0%
Adj. Flow (vph)	11	658	34	70	940	49	74	70	79	54	53	12
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	703	0	0	1059	0	0	223	0	0	119	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		0			0			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.06	1.06	1.06	1.06	1.06	1.06	0.86	0.86	0.86	0.86	0.86	0.86
Turning Speed (mph)	15		9	15		9	15		9	15		9
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		2			2			4			4	
Permitted Phases	2			2			4			4		
Minimum Split (s)	28.0	28.0		28.0	28.0		13.0	13.0		13.0	13.0	
Total Split (s)	38.0	38.0		38.0	38.0		18.0	18.0		18.0	18.0	
Total Split (%)	47.5%	47.5%		47.5%	47.5%		22.5%	22.5%		22.5%	22.5%	
Maximum Green (s)	33.0	33.0		33.0	33.0		13.0	13.0		13.0	13.0	
Yellow Time (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		5.0			5.0			5.0			5.0	
Lead/Lag							Lag	Lag		Lag	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Walk Time (s)												
Flash Dont Walk (s)												
Pedestrian Calls (#/hr)												

Lane Group	Ø3
Lane Configurations	
Traffic Volume (vph)	
Future Volume (vph)	
Ideal Flow (vphpl)	
Lane Width (ft)	
Grade (%)	
Lane Util. Factor	
Ped Bike Factor	
Frt	
Flt Protected	
Satd. Flow (prot)	
Flt Permitted	
Satd. Flow (perm)	
Right Turn on Red	
Satd. Flow (RTOR)	
Link Speed (mph)	
Link Distance (ft)	
Travel Time (s)	
Confl. Peds. (#/hr)	
Confl. Bikes (#/hr)	
Peak Hour Factor	
Heavy Vehicles (%)	
Adj. Flow (vph)	
Shared Lane Traffic (%)	
Lane Group Flow (vph)	
Enter Blocked Intersection	
Lane Alignment	
Median Width(ft)	
Link Offset(ft)	
Crosswalk Width(ft)	
Two way Left Turn Lane	
Headway Factor	
Turning Speed (mph)	
Turn Type	
Protected Phases	3
Permitted Phases	
Minimum Split (s)	24.0
Total Split (s)	24.0
Total Split (%)	30%
Maximum Green (s)	21.0
Yellow Time (s)	2.0
All-Red Time (s)	1.0
Lost Time Adjust (s)	
Total Lost Time (s)	
Lead/Lag	Lead
Lead-Lag Optimize?	Yes
Walk Time (s)	7.0
Flash Dont Walk (s)	16.0
Pedestrian Calls (#/hr)	20

Lanes, Volumes, Timings
 3: Maywood Street & Park Avenue (Route 12)

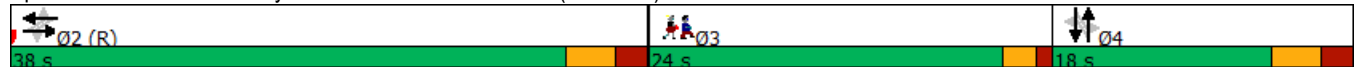
Build Conditions PM

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Act Effect Green (s)		33.0			33.0			13.0			13.0	
Actuated g/C Ratio		0.41			0.41			0.16			0.16	
v/c Ratio		0.54			1.02			0.75			0.56	
Control Delay		19.4			58.3			45.4			40.4	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		19.4			58.3			45.4			40.4	
LOS		B			E			D			D	
Approach Delay		19.4			58.3			45.4			40.4	
Approach LOS		B			E			D			D	

Intersection Summary

Area Type:	Other
Cycle Length:	80
Actuated Cycle Length:	80
Offset:	0 (0%), Referenced to phase 2:EBWB and 6:, Start of Green
Natural Cycle:	80
Control Type:	Pretimed
Maximum v/c Ratio:	1.02
Intersection Signal Delay:	43.0
Intersection LOS:	D
Intersection Capacity Utilization	71.1%
ICU Level of Service	C
Analysis Period (min)	15

Splits and Phases: 3: Maywood Street & Park Avenue (Route 12)



Lane Group	Ø3
Act Effct Green (s)	
Actuated g/C Ratio	
v/c Ratio	
Control Delay	
Queue Delay	
Total Delay	
LOS	
Approach Delay	
Approach LOS	
Intersection Summary	

3: Maywood Street & Park Avenue (Route 12)

	→	←	↑	↓
Lane Group	EBT	WBT	NBT	SBT
Lane Group Flow (vph)	703	1059	223	119
v/c Ratio	0.54	1.02	0.75	0.56
Control Delay	19.4	58.3	45.4	40.4
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	19.4	58.3	45.4	40.4
Queue Length 50th (ft)	133	~283	93	53
Queue Length 95th (ft)	186	#422	#200	#107
Internal Link Dist (ft)	109	189	105	136
Turn Bay Length (ft)				
Base Capacity (vph)	1302	1038	297	213
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.54	1.02	0.75	0.56

















Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.

HCM Signalized Intersection Capacity Analysis

3: Maywood Street & Park Avenue (Route 12)

Build Conditions PM

													
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations													
Traffic Volume (vph)	10	605	31	64	865	45	68	64	73	50	49	11	
Future Volume (vph)	10	605	31	64	865	45	68	64	73	50	49	11	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Lane Width	11	11	11	11	11	11	16	16	16	16	16	16	
Grade (%)		2%			2%			2%			2%		
Total Lost time (s)		5.0			5.0			5.0			5.0		
Lane Util. Factor		0.95			0.95			1.00			1.00		
Frbp, ped/bikes		1.00			1.00			1.00			1.00		
Flpb, ped/bikes		1.00			1.00			1.00			1.00		
Frt		0.99			0.99			0.95			0.99		
Flt Protected		1.00			1.00			0.98			0.98		
Satd. Flow (prot)		3364			3059			1945			1949		
Flt Permitted		0.93			0.82			0.85			0.64		
Satd. Flow (perm)		3143			2507			1681			1282		
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	
Adj. Flow (vph)	11	658	34	70	940	49	74	70	79	54	53	12	
RTOR Reduction (vph)	0	5	0	0	4	0	0	24	0	0	5	0	
Lane Group Flow (vph)	0	698	0	0	1055	0	0	199	0	0	114	0	
Confl. Peds. (#/hr)	3			14			7			8			
Confl. Bikes (#/hr)												2	
Heavy Vehicles (%)	0%	2%	0%	0%	13%	4%	3%	2%	2%	11%	0%	0%	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA		
Protected Phases		2			2			4			4		
Permitted Phases	2			2			4			4			
Actuated Green, G (s)		33.0			33.0			13.0			13.0		
Effective Green, g (s)		33.0			33.0			13.0			13.0		
Actuated g/C Ratio		0.41			0.41			0.16			0.16		
Clearance Time (s)		5.0			5.0			5.0			5.0		
Lane Grp Cap (vph)		1296			1034			273			208		
v/s Ratio Prot													
v/s Ratio Perm		0.22			c0.42			c0.12			0.09		
v/c Ratio		0.54			1.02			0.73			0.55		
Uniform Delay, d1		17.8			23.5			31.8			30.8		
Progression Factor		1.00			1.00			1.00			1.00		
Incremental Delay, d2		1.6			33.2			15.6			10.0		
Delay (s)		19.4			56.7			47.5			40.8		
Level of Service		B			E			D			D		
Approach Delay (s)		19.4			56.7			47.5			40.8		
Approach LOS		B			E			D			D		
Intersection Summary													
HCM 2000 Control Delay			42.3									HCM 2000 Level of Service	D
HCM 2000 Volume to Capacity ratio			0.64										
Actuated Cycle Length (s)			80.0									Sum of lost time (s)	13.0
Intersection Capacity Utilization			71.1%									ICU Level of Service	C
Analysis Period (min)			15										

c Critical Lane Group